

Planning Commission Agenda

Regular Meeting

Tuesday, January 27, 2026, at 1:15 PM

City Council Chambers – City Center South

1001 11th Ave, Greeley, CO 80631

Zoom Webinar Link:

<https://greeleygov.zoom.us/j/88384724221>

NOTICE:

Planning Commission meetings are held on the 2nd and 4th Tuesdays of each month. Meetings are conducted in a hybrid format, with a Zoom webinar in addition to the in-person meeting in Council Chambers.

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Watch Meetings:



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Comment in Real Time:



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Submit Written Comments:



Email comments about any item on the agenda to: cd_admin_team@greeleygov.com



Written comments can be mailed or dropped off at the Planning office at 1100 10th Street, Greeley, CO 80631. Written comments received prior to or during the meeting will be read into the record in real time.

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Planning Commission

January 27, 2026, at 1:15 PM
City Council Chambers – City Center South
1001 11th Avenue, Greeley, CO 80631

Agenda

1. Call to Order
2. Roll Call
3. Approval of the Agenda
4. Approval of the January 13, 2026, Planning Commission Meeting Minutes
5. Staff Report
6. Adjournment

Workshop

7. Westside Area Plan & Form-Based Code



Planning Commission Meeting Minutes

January 13, 2026, at 1:15pm

1. Call to Order

Vice Chair Andersen called the meeting to order at 1:14pm.

2. Roll Call - Present

Vice Chair Louisa Andersen
Commissioner Erik Briscoe
Commissioner Jeff Carlson
Commissioner Brian Franzen
Commissioner Christian Schulte

Absent

Chair Justin Yeater
Commissioner Larry Modlin

3. Approval of the agenda

There were no changes to the agenda and Vice Chair Andersen proceeded with the agenda as published.

4. Approval of the December 09, 2025, Planning Commission Meeting Minutes

Motion by Commissioner Franzen:

He moved to approve the minutes as presented.

Second: Commissioner Briscoe

Vote: Motion passed 5-0

Absent: Chair Yeater and Commissioner Modlin

5. A public hearing regarding a recommendation on an ordinance repealing a portion of Title 24, Chapter 10 of the Greeley Municipal Code relating to the Floodplain Overlay District and amending Title 12 of the Greeley Municipal Code by adding a new Chapter 12 relating to floodplain management.

Brian Hathaway (Water Quality and Regulatory Compliance Manager & Floodplain Administrator) began his presentation by introducing himself and the item as published.

Commissioner Briscoe asked Mr. Hathaway if he could provide a brief overview of the deficiencies that were identified. Mr. Hathaway explained the proposed updates clarify the designation and responsibilities of the Floodplain Administrator and establish a formal variance process that involves the Planning Commission and the Zoning Board of Appeals. Mr. Hathaway shared that the variance language is based on a FEMA model ordinance and aligns with FEMA and CWCB compliance standards. Additional provisions were added, including severability, definitions, and updated terminology, as well as requirements related to base flood elevation, proximate A zones, and limitations on encroachment exceeding half a foot.

Commissioner Franzen asked for clarification on where Zone A areas were and if applicants are required to determine a base flood elevation only for properties located within Zone A, and not for a development as a whole. Mr. Hathaway answered by stating approximate A zones do not have area-specific flood studies, as FEMA has not had the time or resources to delineate a floodway in those locations. Within Greeley, these approximate A zones are limited to the Sand Creek Basin, and the applicant would only need a base flood elevation in this area.

Vice Chair Andersen asked if there were any more questions for staff. Seeing none, Vice Chair Andersen opened the public portion of the meeting at 1:23pm. Seeing none in person or online, Vice Chair Andersen closed the public portion of the meeting at 1:24pm and turned it over to the Commission for Action.

Motion by Commissioner Franzen:

Based on the preceding analysis, the Planning Commission finds that the proposed code amendment to the Greeley Municipal Code is consistent with the Development Code criteria of Section 24-211.b (Items 1 through 4) and therefore recommends that the City Council approves the code amendment to the Greeley Municipal Code as submitted.

Second: Briscoe

Vote: Motion passed 5-0

Absent: Chair Yeater and Commissioner Modlin

6. Staff Report:

Don Threewitt (Interim Director of Community Development) began by noting that there were many upcoming initiatives within the Long-Range Planning Department and turned the floor over to Caleb Jackson (Interim Planning Manager) for the updates.

Mr. Jackson shared that a Planning Commission meeting is scheduled in two weeks, during which consultants will provide updates on the Eastside and Westside Area Plans. Commissioners were also invited to participate in a Westside tour of the Bradburn neighborhood in Westminster on January 26, followed that evening by a public open house for the Westside Area Plan and its form-based code component at the Funplex, which is expected to be well attended.

7. Adjournment:

Vice Chair Andersen adjourned the meeting at 1:26pm.

Workshop

8. Development Code Assessment Report

Mr. Jackson introduced the report as published and began the presentation by reviewing the assessment's goals. He then invited the project consultants to complete the presentation. Jen Gardner & Ries Wilson (Logan Simpson Consultants) spoke to their portion of the presentation.

Commissioner Franzen asked whether the code assessment would be presented again once fully finalized. Mr. Jackson explained that they would incorporate the feedback received today, make revisions, and then follow up either by email or in a packet outlining the adjustments reflected in the final report.

Commissioner Carlson asked what the timeline looks like for the overall process. Mr. Jackson noted that the department will be undertaking substantial code updates this year, with one or two rounds of

revisions expected. An additional phase may be needed in a subsequent year, depending on the budget.

Vice Chair Andersen referenced a previous presentation on “returning to the grid concept” for street and city design, which was discussed at length during a meeting at Aims with slideshows and considerable engagement. She then asked whether any of the information from that well-received presentation is intended to be used. Mr. Jackson noted that this work is linked to the Westside Area Plan. Those consultants will return in two weeks to present the draft Westside plan and draft form-based code—another element of the project that must be aligned with the overall development code and offers an alternative development pattern for consideration.

Vice Chair Andersen also noted that there remains an outstanding punch list from 2021 and 2022 and asked whether those issues will be addressed in the upcoming update. Mr. Jackson explained that the list of outstanding issues has grown since 2021–22, and that all the information has been provided to the project consultants. As a result, those topics should be reflected in the assessment report. He added that if any items are missing from the report, they should be identified so they can be included.

Vice Chair Andersen requested greater focus on water use in landscaping. She also raised questions about fencing types and styles, unclear nonresidential setbacks, and the absence of provisions for commercial vehicle parking in residential areas, including how “commercial” is defined and whether it should be addressed.

Commissioner Briscoe expressed a preference for receiving redline or side-by-side versions of proposed changes, noting that it is difficult and time-intensive for Planning Commission volunteers to identify and assess revisions when only new code is provided.

Mr. Threewitt explained that, as each component is brought forward, they plan to present small chunks first in a work session for discussion before a final recommendation. A redline version will be provided as the review copy, along with a clean proposed ordinance. They also intend to include a summary of changes showing how items have moved or been reorganized from the old code to the new to improve clarity and will also refine how they communicate changes as they proceed.

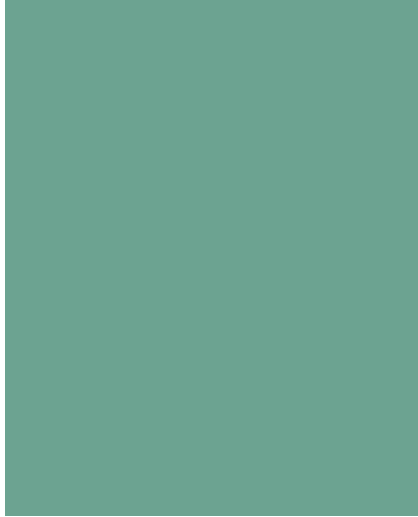
Commissioner Franzen agreed with Commissioner Briscoe’s preference addressed above.

Vice Chair Andersen remarked that understanding the context behind policies is helpful and encouraged the use of examples or stories that explain why a policy was originally created, noting that this context can clarify provisions that might otherwise seem arbitrary.

With no other discussion on this item, Vice Chair Andersen closed the workshop at 1:56pm.

Louisa Andersen – Vice Chair

Don Threewitt – Interim Comm. Development Director



Westside Area Plan

GREELEY, COLORADO



January 2026 DRAFT



ACKNOWLEDGMENTS

The initial drafts of the Westside Area Plan involved the input of over 600 residents, stakeholders, civic institutions, business organizations, and City staff. The Plan authors extend deep gratitude to everyone who lent their insight and to the many more people who will endeavor toward the Plan's implementation.



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Director, Community Development

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Planning Supervisor, Long Range Team

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Planner III, Community Development

PROJECT TEAM



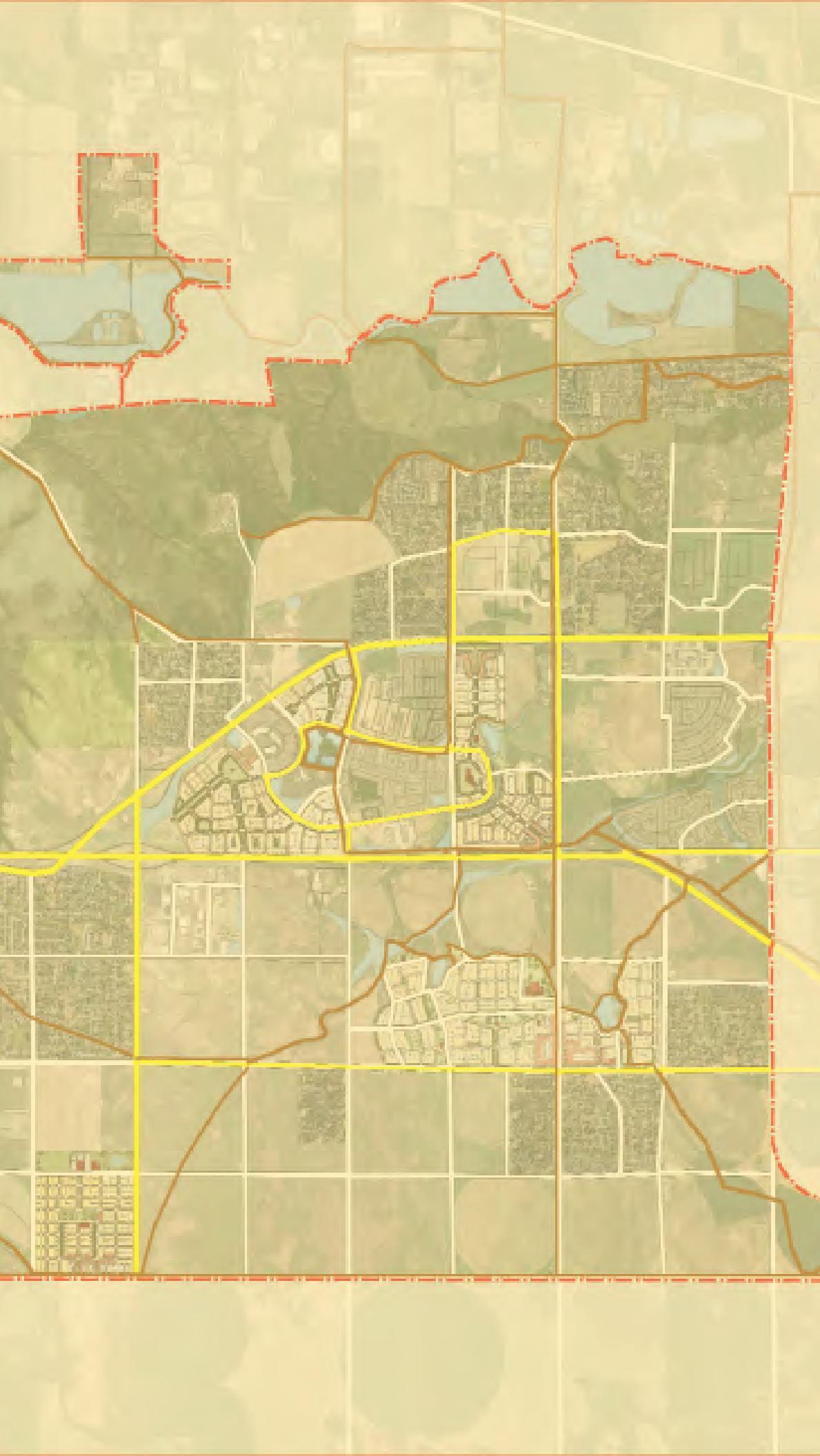
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PLAN SUMMARY

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PLAN OVERVIEW

The Westside Area Plan provides a guiding framework for the City of Greeley, Colorado, at a pivotal moment in its 150+ year history. As one of the fastest-growing communities in one of the fastest-growing states, Greeley faces both significant opportunity and responsibility as it plans for continued growth. Much of that growth is anticipated to occur in Western Greeley, an area that today remains characterized by a mix of working agricultural land, emerging development, and largely separated, auto-oriented patterns typical of suburban sprawl.

With substantial undeveloped land still available, Western Greeley represents a rare opportunity to establish a different model for growth—one that moves beyond conventional development patterns and sets clear expectations for building better. The Plan envisions a future of compact, connected, and walkable neighborhoods organized around a mix of land uses and a broad range of housing types,

sizes, and price points, from attainable housing to executive homes. These complete neighborhoods are intended to support daily needs within close proximity, encourage multimodal mobility, foster community interaction, and enhance long-term economic and environmental sustainability.

The Westside Area Plan establishes a comprehensive, implementable framework to guide this transition. Through an integrated set of goals, policies, and implementation strategies, the Plan translates community aspirations into actionable guidance that aligns with existing City policies while raising the bar for future development. The result is a clear roadmap for shaping high quality, human-scale growth in West Greeley—growth that will help define Greeley’s next chapter and position the area as one of the most attractive and desirable places to live along the Front Range.

EXISTING CONDITIONS SUMMARY

Founded in 1870, the City of Greeley serves as the county seat of Weld County and has long played a central role in northern Colorado’s agricultural and economic history. Today, Greeley is home to approximately 115,000 residents within roughly 50 square miles, and its population is projected to reach approximately 200,000 by 2055. The Western Greeley Subarea—comprising approximately 22 square miles—has been identified as a primary location for accommodating much of this anticipated growth.

From 2010 to 2020, the Greeley Metropolitan Statistical Area experienced population growth of approximately 30 percent, making it the fastest-growing metropolitan area in Colorado. Situated east of Interstate 25 with direct access to US 34 and US 85, Greeley is strategically positioned for growth. The city benefits from prominent views of the Rocky Mountains, including Longs Peak, proximity to the Denver metropolitan area, and access to major educational institutions, such as the University of Northern Colorado (UNC) and Aims Community College.

The Western Greeley Subarea includes a diverse range of landscapes and land uses, including portions of the Cache la Poudre River corridor, the Arroyos del Sol, Areas of Ecological Significance (AES), traditional rural agricultural lands, and existing auto-oriented, sprawling suburban, exurban, industrial, and commercial development. Together, these conditions create both new, appealing opportunities and constraints as development pressure increases. Understanding this context is essential to shaping a coordinated approach to growth—one that responds to market demand while addressing infrastructure, mobility, environmental resources, and long-term community character.



Arroyos del Sol

WESTSIDE MARKET OVERVIEW

As Greeley’s most active growth area, Western Greeley has a range of short- and long-term opportunities to grow its local economy. Housing remains a major economic driver citywide, and Western Greeley continues to experience steady demand due to its growing housing stock, new infrastructure investments, and proximity to major Front Range job centers. While such demand is continuing to rise, much of Western Greeley is defined by auto-oriented sprawl: isolated single-use pods that require residents, employees, and visitors to drive to fulfill their daily needs. This sort of car-dependent planning is destructive to the economy, the environment, public health, and Western Greeley’s sense of community.

Western Greeley’s residents have increasingly been demanding a higher quality of life, and the subarea holds promise for embracing another style of development—traditional neighborhood development—to deliver that vision. As proposed in this Plan, building walkable, mixed-use neighborhoods with a diversity of housing types can help foster an even more desirable Western Greeley that will set a new standard on the Front Range.

New developments must also be reconciled with agricultural operations, which are a defining part of Western Greeley’s economic and geographic landscape. Currently, producers are facing pressures from urban expansion, water limitations, and market fluctuations. Conservation must be a centerpiece of Western Greeley’s rapid growth.

WESTSIDE DEVELOPMENT CAPACITY ANALYSIS

A Development Capacity Analysis conducted as part of this Plan estimates the maximum potential development Western Greeley could accommodate under the Westside Land Use Guidance Map through 2055. Using land use designations from the 2018 Imagine Greeley Comprehensive Plan, the updated map retains the same categories—Neighborhoods, Centers, Open Lands, Natural Areas, and general “Areas”—while refining dwelling unit per acre and maximum square footage standards to better support walkable development rather than Greeley’s prevailing pattern of sprawl.

To model build-out potential, the analysis applies maximum assumptions based on the highest applicable zoning districts: up to six stories with 80 percent lot coverage for Employment, Industrial, and Commercial uses, and up to five stories for mixed-use districts. Because most real-world projects develop at lower intensities, the study also includes more conservative estimates informed by recent development patterns in Western Greeley.

Overall, the analysis confirms that the Westside Land Use Guidance Map provides sufficient capacity to accommodate even optimistic long-term growth projections. Its flexible range of densities and land use types supports both higher-intensity infill and traditional suburban development, allowing the City to adapt to changing market and demographic conditions. Periodic updates to the map will help ensure continued alignment with the City’s long-term vision.

COMMUNITY OUTREACH SUMMARY

At the core of the Westside Area Plan’s process was deep community engagement that welcomed a broad range of Greeley’s constituents to weigh in on how they view Greeley today and how they might like to experience it in the future. Over the course of the project, hundreds of community members contributed to the planning effort. Engagement touchpoints were as follows.

A multi-day in-person community engagement and visioning workshop, or charrette, occurred from July 7 - July 11, 2025. During this week, an evening planning forum drew 200+ residents who participated in a talk by Jeff Speck, author of *Walkable City*, and interactive exercises. While this event was the signature engagement component of the planning process, other activities included:

- Interactive polling via the project website
- Study area tours
- Stakeholder meetings
- Interactive public workshops
- Development review assistance to better understand Western Greeley’s context

The Project Team also conducted focused discussions with developers, property owners, and local businesses. These conversations offered insight into development opportunities, infrastructure needs, and market considerations, ensuring the Plan reflects both community priorities and on-the-ground feasibility.



Jeff Speck presents “Towards a More Walkable Western Greeley”



Display boards presented at the charrette



Residents provide input during an interactive workshop exercise, identifying preferred street types and urban design features for different parts of Western Greeley.

An interactive website for the project was created, entitled **“Speak Up Greeley: Shape the Future of Greeley’s West Plan.”** More than 1,235 participants viewed the project page, with 231 individuals engaging in survey content and 148 submitting responses in both English and Spanish language formats.

MOST VALUED FEATURES

Survey respondents identified the following features as the most valued and most desired for Western Greeley:

- 1 Natural beauty, parks, green spaces, and trails
- 2 Access to amenities, like shopping, retail, groceries, and dining
- 3 Access to healthcare facilities

VISION IN THREE WORDS

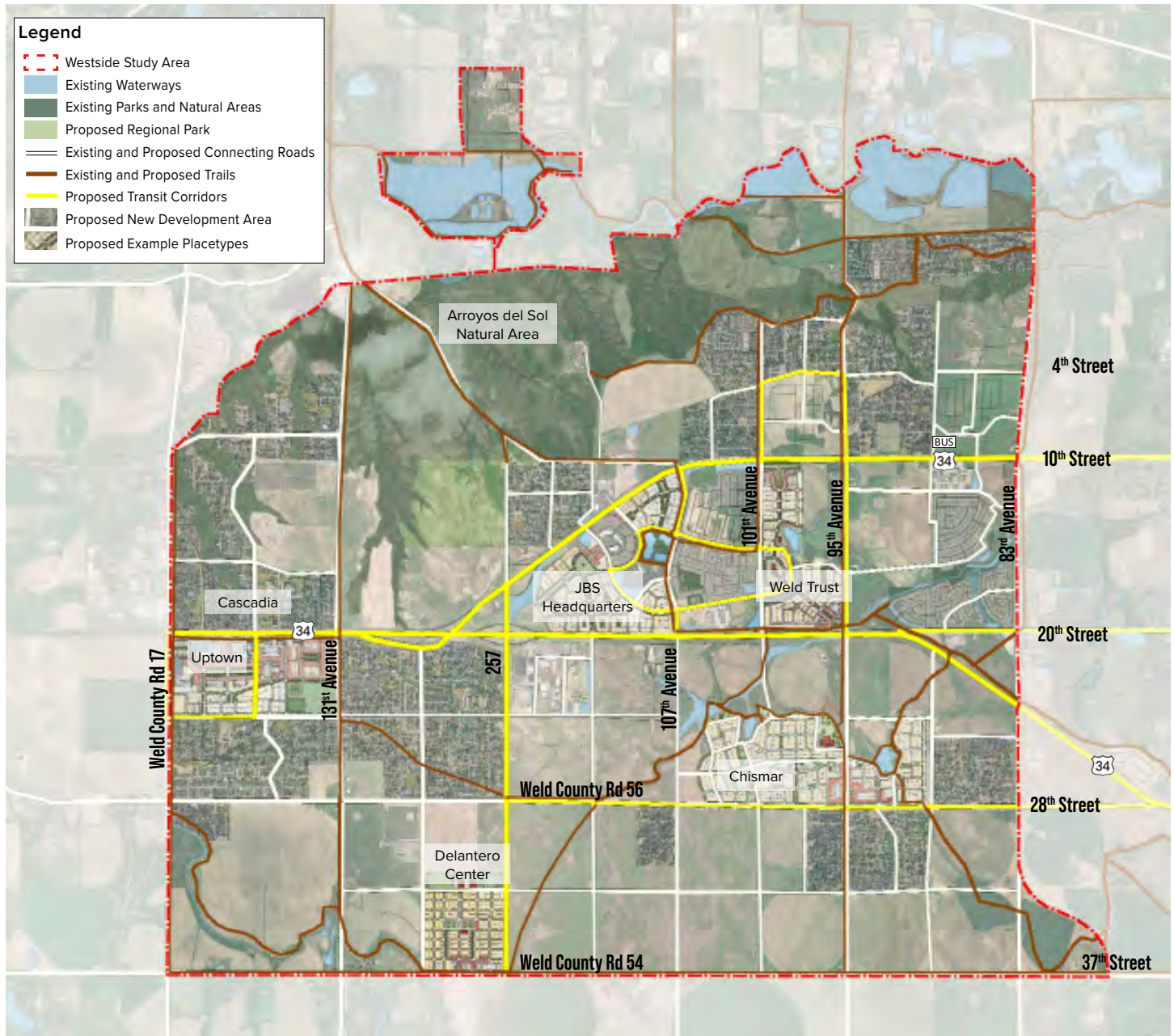
During the July charrette and online, participants were asked to best describe their long-term vision for Western Greeley in three words. The word cloud to the right shows words that were most frequently used. The larger the word, the more times it was repeated.



PHYSICAL FRAMEWORK SUMMARY

Drawing on a thorough review of existing conditions and best practices, the Project Team developed a physical framework to guide public and private investment in Western Greeley. This framework identifies key opportunity areas, mobility corridors, and open space connections that together establish the structure for future growth. The intent is to support a cohesive, connected, and sustainable development pattern that builds upon the community’s natural assets and infrastructure investments.

The physical framework outlines a balanced approach that links neighborhoods, commercial centers, and employment districts through an expanded multimodal network. It emphasizes development, infill, and redevelopment in strategic locations, the protection of agricultural and natural resource areas, and the integration of parks and green corridors that enhance the overall quality of life.



Western Greeley Physical Framework Plan

The framework includes:

- **Activity Nodes** concentrated along key corridors, supporting mixed-use, higher-density, and walkable development.
- **Employment and Innovation Areas** positioned near major transportation routes to encourage economic diversification and support regional job growth.
- **Neighborhood Centers and Housing Opportunities** that promote a range of housing types and affordability levels and maintain connectivity to schools, parks, and community amenities.
- **Greenway and Trail Connections** that link residential areas to open spaces, reinforcing environmental stewardship.
- **Future Mobility Corridors** designed to improve east-west connectivity and reduce congestion, integrating transit and active transportation options.

As the city continues to grow, this framework provides the foundation for coordinated investment and phased implementation over the next decade, ensuring that Western Greeley evolves as a resilient, well-connected, livable, and inclusive community.

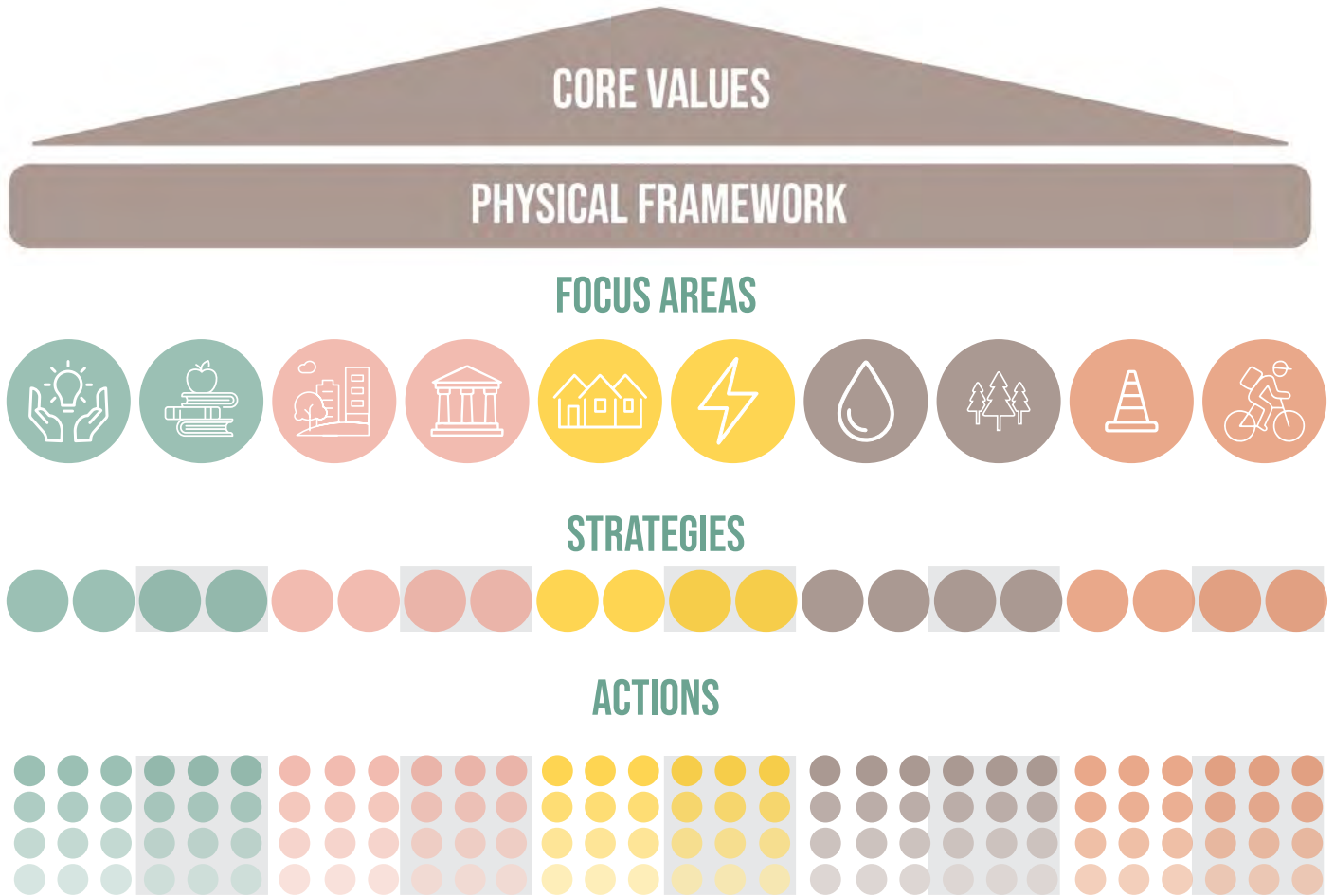


Illustrative Preferred Development Framework for the proposed Western Greeley New Urban Community, showing activity nodes, neighborhoods, employment areas, mobility corridors, and connected open spaces.

ACTION PLAN SUMMARY

The Western Greeley Action Plan offers a coordinated roadmap for managing growth, strengthening community services, and guiding public and private investment over the next decade. Organized around the ten focus areas introduced in the Imagine Greeley Comprehensive Plan—from economic health and housing to transportation, natural resources, and public safety—the Plan translates best practices and findings identified during stakeholder conversations and an existing conditions review into clear, implementable strategies.

The Action Plan outlines priority actions to shape land use and infrastructure decisions, expand mobility and open space networks, diversify housing options, and reinforce the area’s economic and social vitality. Together, these strategies establish a framework that will help the City of Greeley and its partners make informed, phased investments that advance long-term community goals.



CORE VALUES AND VISION SUMMARY

The vision for Western Greeley is rooted in a commitment to creating a community that supports residents of all ages and backgrounds. The Core Values at the heart of this plan (depicted below) emphasize thoughtful growth, strong neighborhoods, environmental stewardship, and a vibrant local economy. They reflect the community’s desire for walkable and well-designed places, access to parks and natural areas, diverse housing choices, safe and efficient mobility options, and high-quality public services.

These values inform the strategies recommended in the Action Plan and are expected to guide long-term decision-making, ensuring that future development enhances quality of life, protects natural resources, celebrates local identity, and strengthens Western Greeley’s role as a complete, inclusive, and enduring community.

CORE VALUES

CONNECTED & ACCESSIBLE

PROSPEROUS & INNOVATIVE

SUSTAINABLE & RESILIENT

WELCOMING & LIVABLE

The Plan organizes its strategies around ten interconnected elements that collectively advance the community’s core values and long-term vision.



Economic Health & Diversification



Education, Health, & Human Services



Growth & City Form



Historic & Cultural Resources



Housing



Infrastructure



Natural Resources & Open Lands



Parks & Recreation



Public Safety



Transportation & Mobility



WAGON 16490 LBS OF POTATOES HAULED IN 4 MILES
BY NORTHERN CO. PRODUCE CO. GREELEY. BY H. F. P.

01

BACKGROUND

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PLAN PURPOSE

The Westside Area Plan is a long-range visioning and planning effort designed to guide Western Greeley's growth over the coming decades. Addressing land use, urban design, housing, transportation, public spaces, and environmental stewardship, among other factors, the Plan establishes a clear framework that reflects community values and informs future decision-making.

Given a projected annual population increase of 1.8 percent, Greeley must proactively plan for the infrastructure—places to live, work, learn, recreate, worship, shop, dine, and get around—needed to support its growing population. This growth requires a methodical and implementable approach. Currently, Western Greeley is dominated by auto-oriented sprawl: disparate single-use pods that require

residents, employees, and visitors to drive to live, work, learn, play, and more. This Plan presents an opportunity for the City to advance an economically vibrant, sustainable, inviting, connected, and human-scale urban form.

With direct access to US 34 and US 85 and expansive views of Longs Peak and the Rocky Mountains, Western Greeley is positioned to play a significant role in the Front Range's continued growth.

As part of a broader citywide planning effort, this inclusive process equips Western Greeley with the policies, strategies, and tools needed to guide development, coordinate investment, and support the creation of a complete and diverse community.



A walkable, mixed-use neighborhood with parks, diverse housing, and vibrant shops—illustrating how thoughtful planning can balance housing, mobility, and environmental stewardship while avoiding sprawl can be seen in Central Park, CO.

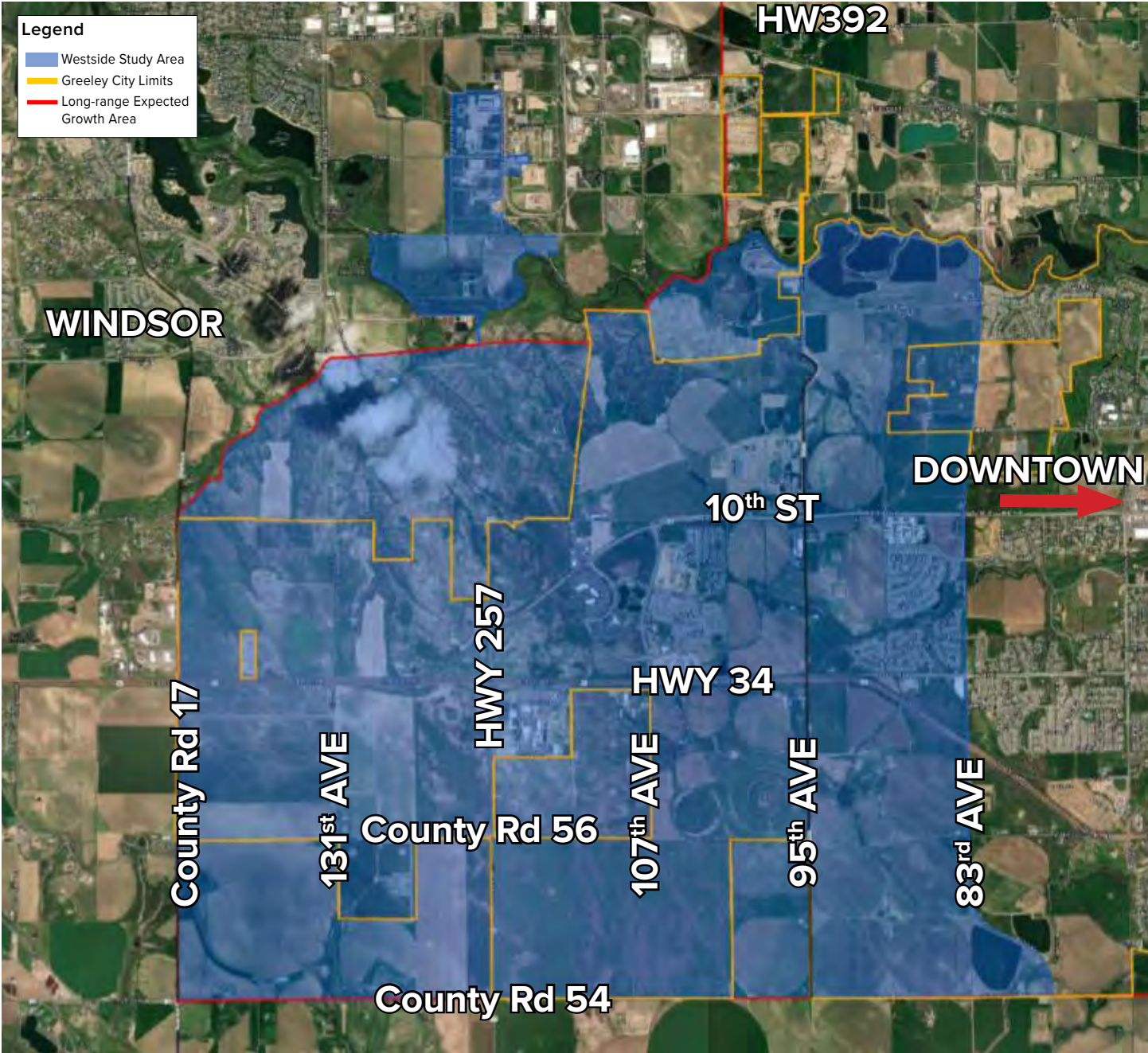
STUDY AREA

Bounded by Greeley’s northern boundary, 37th Street to the south, 83rd Avenue to the east, and County Road 17 to the west, Western Greeley encompasses 22 square miles.

The subarea features a portion of the Cache La Poudre River corridor, the Arroyos del Sol, a 978-acre swath of arroyos, bluffs, and prairies, AES, existing suburban and exurban neighborhoods, industrial and commercial

areas, and traditional rural agricultural land. Given this Plan’s grounding in regional connectivity, it considers infrastructure, land uses, and transportation corridors beyond the study area boundary.

Thus, per Colorado’s statutory requirements, the City expects the Plan to function as a three-mile annexation plan.



HOW TO USE THIS PLAN

As a long-range visioning and planning effort intended to guide Western Greeley’s growth for the next decades, the Westside Area Plan is aimed towards the City of Greeley’s public agencies, private stakeholders, and community members. These three overlapping entities will partner to shape a connected, livable, prosperous, and sustainable Western Greeley.

Greeley community members participated in this Plan’s formation by completing an online survey, attending charrette visioning exercises, partaking in stakeholder conversations, and more. Such engagement, along with a thorough review of Western Greeley’s existing conditions, informed this Plan’s direction and structure.

The Plan includes the following chapters, in sequence.

CHAPTER 2: CONDITIONS ASSESSMENT AND ANALYSIS

Effectively charting a path for Western Greeley’s growth required an understanding of its current conditions. The Project Team assessed the area’s landscape during site visits, gained insight into ideas, constraints, and priorities for Western Greeley during stakeholder conversations with City staff, landowners, and residents, and performed an analysis of Greeley’s prior planning efforts to conceptualize the City’s unique cultural, economic, geographic, political, and social context. Chapter 2 provides an analysis of these existing conditions.

CHAPTER 3: COMMUNITY OUTREACH

Given how this process will set the tone for Greeley’s future development, it necessarily began with deep engagement and welcomed a broad constituency to weigh in on their needs and aspirations for Western Greeley. This outreach effort, detailed in Chapter 3, sought to solicit a diverse range of community perspectives through creative in-person sessions and dynamic digital opportunities for input.

CHAPTER 4: PHYSICAL FRAMEWORK

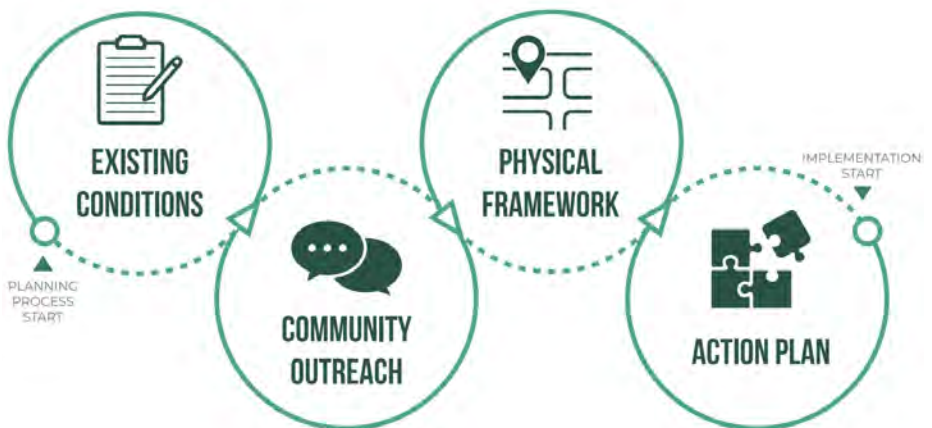
This Plan culminates in a regional plan that serves as an addition to the City’s comprehensive plan and helps evaluate development projects, direct capital improvements, and shape public policy. In addition, the Project Team created illustration plans with representative outcomes for development for five sites in Western Greeley: the Chismar, Delantero Center, Uptown, and Weld Trust properties, and JBS’ Headquarters. Chapter 4 depicts and describes the regional plan and five illustration plans.

CHAPTER 5: IMPLEMENTATION

Implementing the regional plan and illustrative plans depends on applying development regulations that emphasize the physical character of new growth—specifically the relationship between buildings, streets, and public spaces—rather than focusing solely on land use. Successful implementation will require coordination across many aspects of development and among numerous City departments and partner entities as Western Greeley grows. This Chapter outlines strategies and actions for each plan element to guide that collaborative effort.

APPENDIX

This Plan’s Appendix contains results from the online survey.

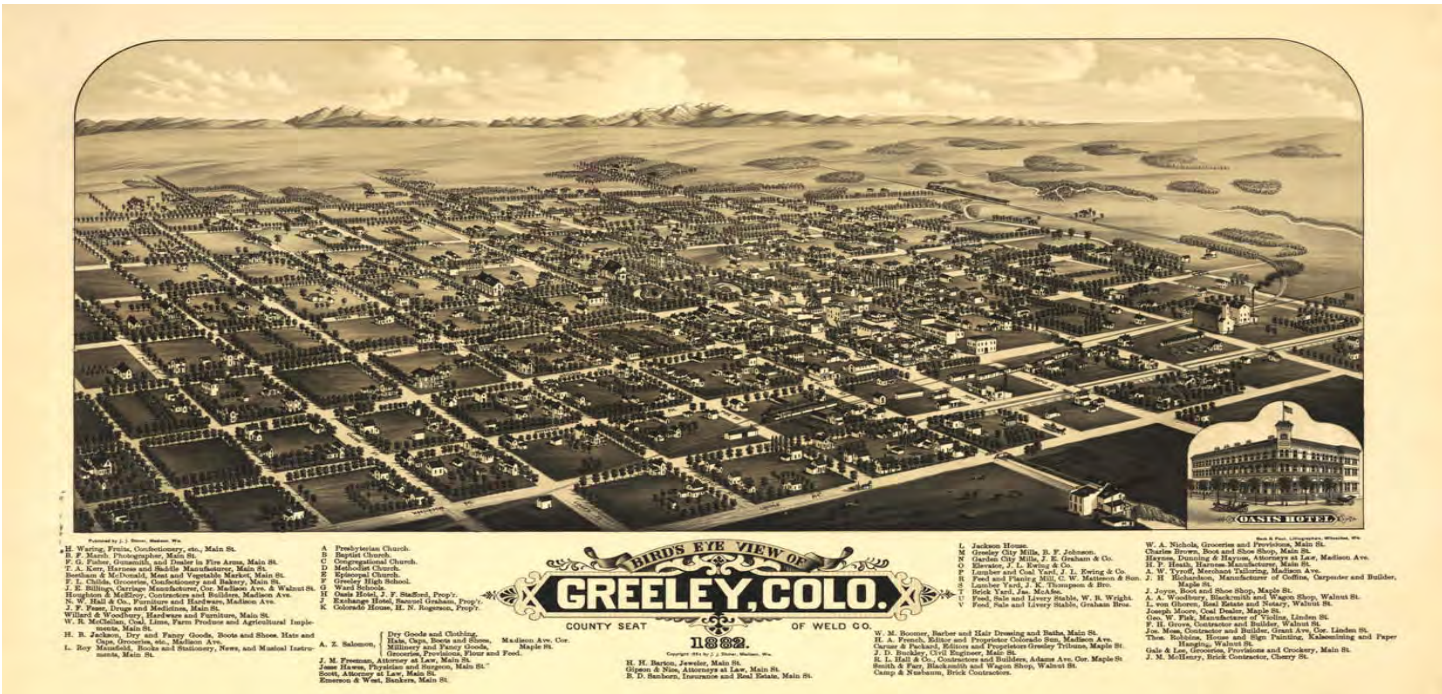


HISTORICAL CONTEXT

Journalist Nathan Meeker and over 700 colonists formed the Union Colony, a utopian agrarian colony, in 1869. After a comprehensive search for a site to host the settlement, the Colony’s “Locating Committee” landed upon a 60,000-acre site flanked by the Cache la Poudre and South Platte Rivers in 1870. With fertile soil and proximity to the Denver-Pacific Railroad, which linked Denver and Cheyenne, this land emerged as a prime location for the Union Colony to flourish.

Inspired by the beloved greenery of Northampton, Massachusetts, and Painesville, Ohio, the Union Colony began growing as a porous grid with tree-lined streets. At this time, the only way to traverse the Colony was afoot or on a horse, so the colonists planned it for the human scale. The original plat included 660 residential lots, 483 business lots, and additional lots designated for a courthouse, town hall, churches, and schools, among other civic uses, all of which thrived in propinquity to each other.

In the Colony’s first fifteen years, its population mushroomed over fourfold from 480 to 2,177. In 1886, it gained the legitimacy to incorporate as the City of Greeley, named after Horace Greeley, the influential newspaper editor and advocate of Western settlement whose “Go West, young man” ethos helped inspire the community’s founding. Throughout the next decades, during which Greeley’s population passed 10,000, the city continued to foster a rich sense of place. Architects erected signature archways at Greeley’s boundaries to welcome visitors and crafted mixed-use buildings, such as retail below housing and offices adjacent to an opera house, with meticulous detail. The Monroe and Cranford residential neighborhoods featured architectural styles ranging from Italianate to Queen Anne to European Revival, and full-scale factories and grain elevators constituted foundations for the City’s agrarian economy. Residents lived in a range of housing types that complemented community gems like Glenmere Park and enjoyed a largely walkable lifestyle.



Historic map of Greeley

However, Greeley’s architectural, design, and development patterns shifted in the early 20th century with the advent of the automobile. Like cities across the United States, Greeley rejected its traditional mixed-use form and instead embraced Euclidean zoning, the practice of isolating land uses. Euclidean zoning originated in the Industrial Revolution-era crusade to separate incompatible uses but soon evolved to encompass the separation of all uses: housing from schools, offices from retail, and parks from civic institutions. The city abandoned its affinity for vibrant streets that offered places to live, work, shop, and dine all within one slow-speed block and instead championed a sprawling pattern of growth that required people to drive through streets, or worse, sit through traffic, to fulfill their daily life needs: shopping for groceries, attending school, recreating in parks, working, worshiping, and more.

Greeley’s built environment began to reflect these choices. Cars were foisted upon front yards, sitting in street-facing driveways and garages rather than tucked away in alleys. Porches became obsolete, as televisions and air conditioners lured people inside. No longer fronting the sidewalk’s edge, retail became surrounded by seas of asphalt—surface parking lots. Greeley morphed into a city built not for its people, but for its cars.

In Greeley’s current period of population growth, sprawl has remained the principal form of development. Yet, Greeley’s citizens have expressed a burgeoning desire to further develop the City’s sense of place and recover a walkable lifestyle. Porches, natural amenities, and views that terminate on mountain vistas appear in new development, and infill is filling in missing teeth downtown. City leadership is committed to planning Greeley’s growth so it occurs intentionally, rather than just occurring. The Westside Area Plan sets the framework essential for materializing this vision.



Studebaker wagon in Greeley

WESTERN GREELEY'S HISTORY

Since Greeley's inception, it has primarily grown westward. In keeping with the City's deep agrarian tradition, Western Greeley is predominantly active and latent farmland. However, the subarea has also served as a hub for business, with JBS' corporate headquarters at Promontory Circle. At the same time, companies such as Carestream Health, Inc., a subsidiary of Kodak, and State Farm have shuttered their offices in Western Greeley in recent years. In contrast, residential developers have identified the subarea as a highly competitive location for new housing. Such interest has spurred the Westside Area Plan, which leverages best planning and design practices to institute a methodical framework for this development. Western Greeley's growth is inevitable; how that growth occurs is not.



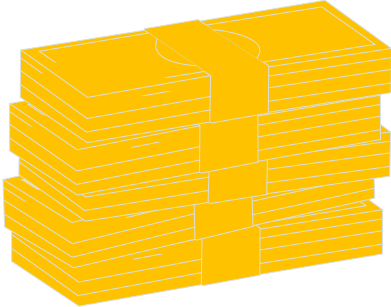
Nineteenth-century buildings in Greeley



The existing form of development in Western Greeley reflects historic, sector-based planning practices characterized by single-use zoning and auto-oriented infrastructure.

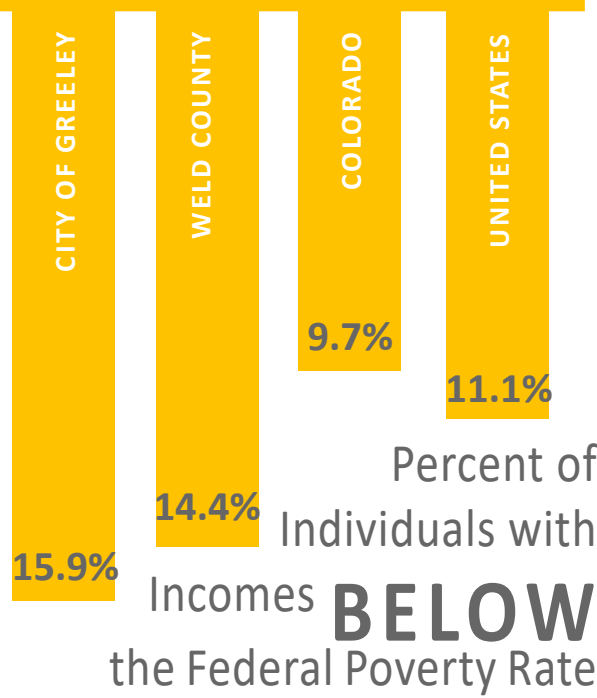
WESTERN GREELEY PROFILE

\$116,659

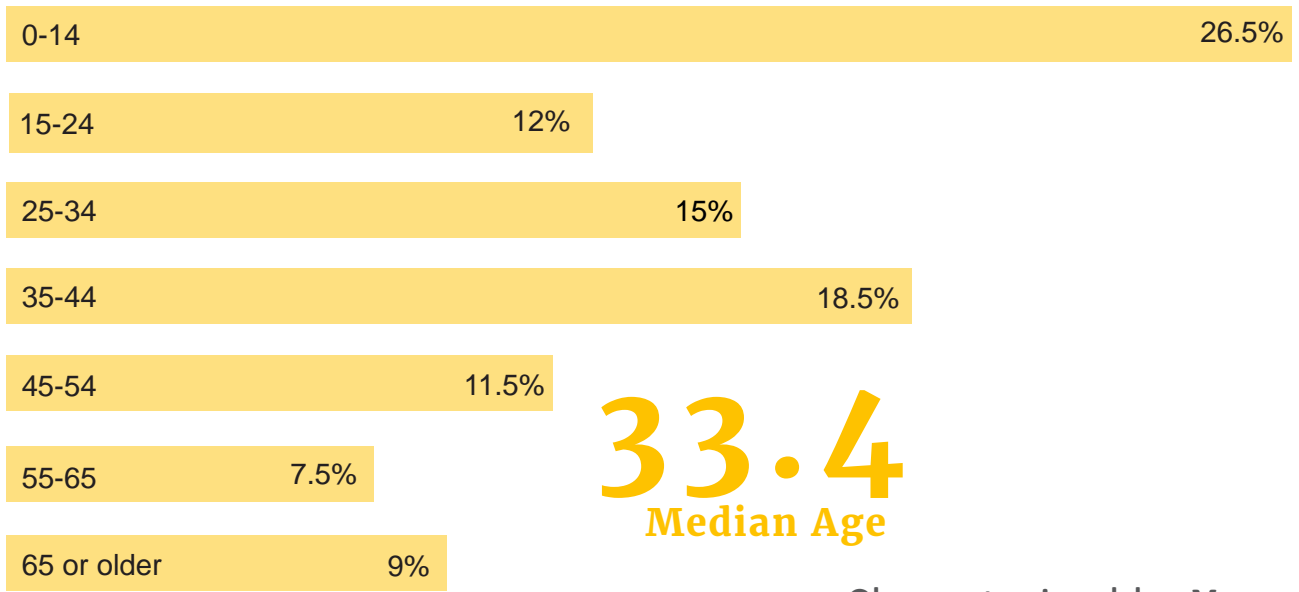


Median Household Income in Western Greeley

City of Greeley: \$63,526
 Weld County: \$93,287
 Colorado: \$97,100



Western Greeley Population by Age



33.4

Median Age

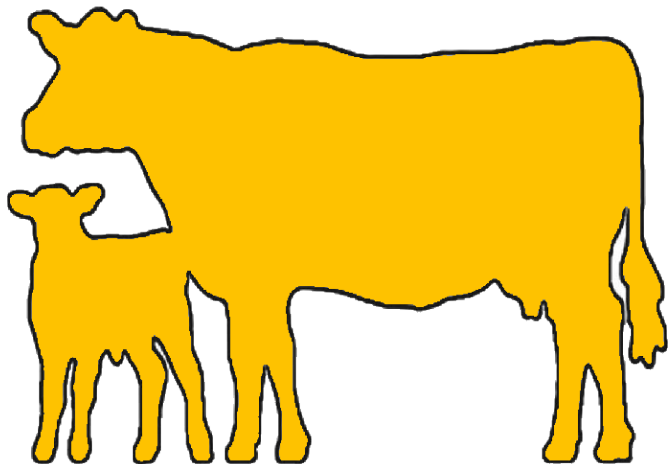
Characterized by Young Families & Professionals



≈ 4,608

PEOPLE LIVE IN WESTERN GREELEY

Source: United States Census Bureau



AGRICULTURAL LANDS

Remain Important for
Local Identity and Resilience

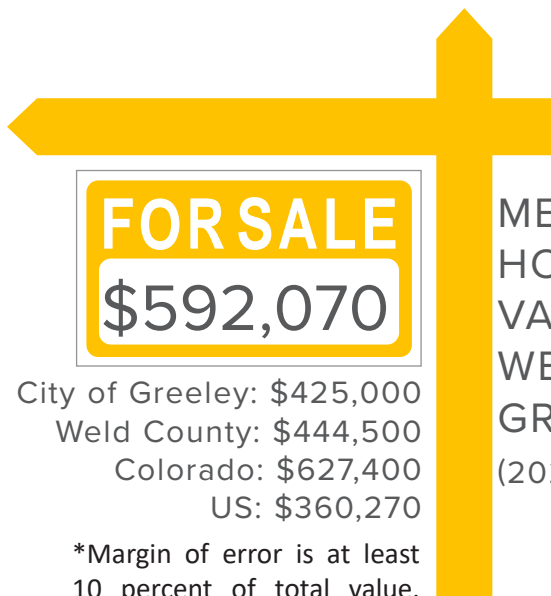
TRAVEL TIME TO WORK



Percent of people 25+ with Associates Degrees or Higher



Supported by access to the University of
Northern Colorado, Aims Community College



City of Greeley: \$425,000
 Weld County: \$444,500
 Colorado: \$627,400
 US: \$360,270

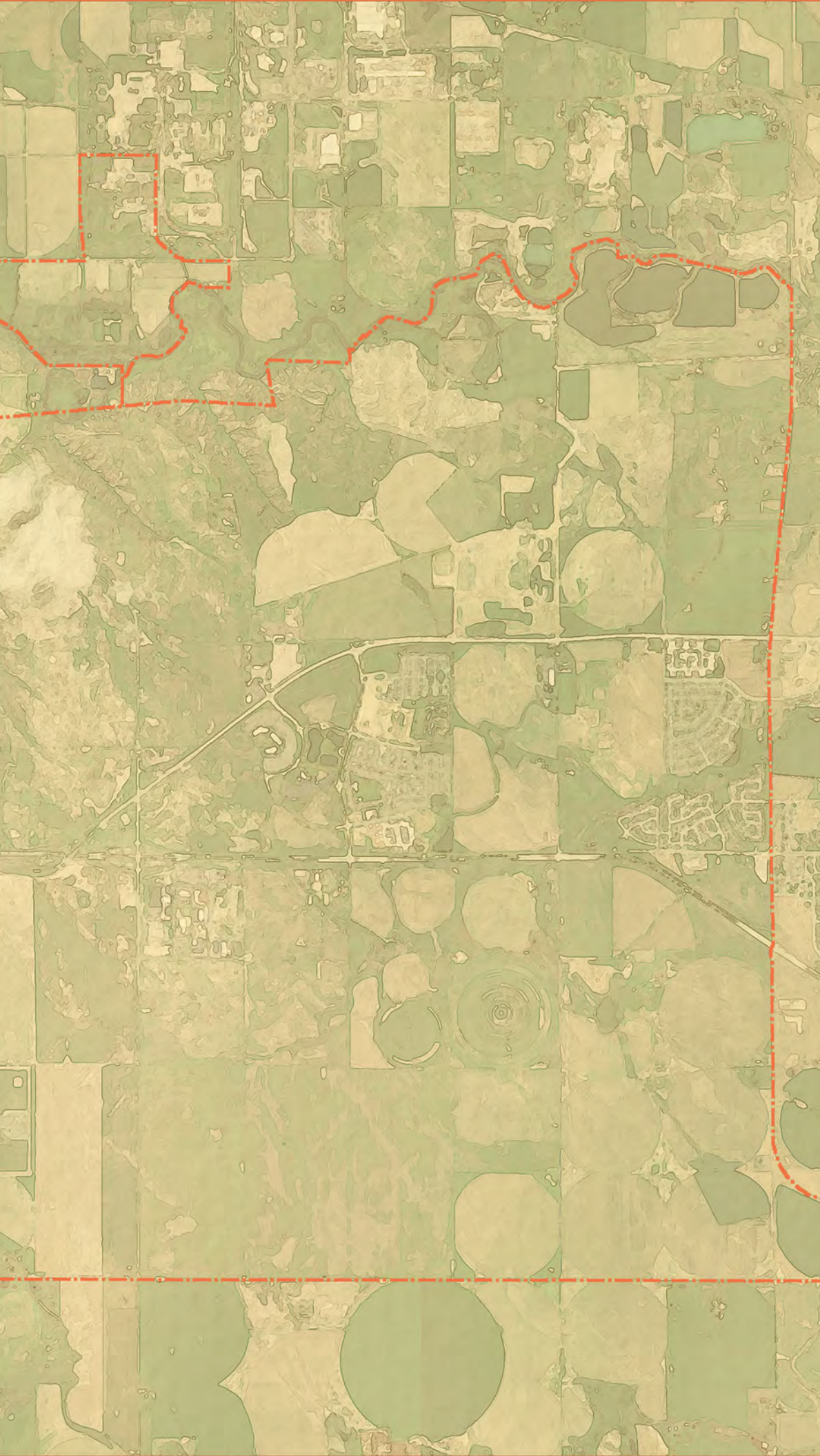
*Margin of error is at least
10 percent of total value.

MEDIAN
HOME
VALUE IN
WESTERN
GREELEY
(2023-2024)

≈ 1,396



Households in
Western Greeley



02

EXISTING CONDITIONS

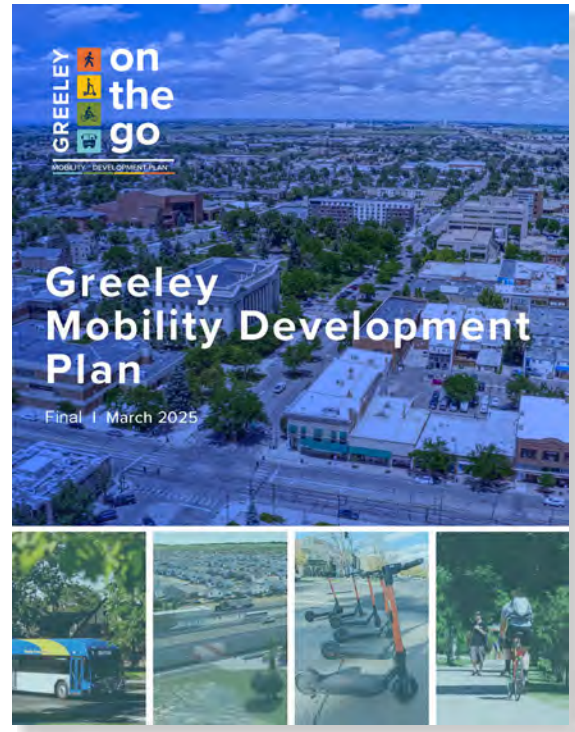
REVIEW OF PAST PLANS	12
ECONOMIC HEALTH & DIVERSIFICATION	16
EDUCATION, HEALTH & HUMAN SERVICES	20
GROWTH & CITY FORM	22
HISTORIC & CULTURAL RESOURCES	26
NATURAL RESOURCES & OPEN LANDS	27
HOUSING	28
INFRASTRUCTURE	30
PARKS AND RECREATION	32
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REVIEW OF PAST PLANS

The City of Greeley has endeavored to commission a series of planning efforts in recent years. Doing so has helped the city continue developing a distinct and attractive identity in the Front Range and tactfully anticipate future growth. Of such efforts, many hold relevance to the Westside Area Plan.

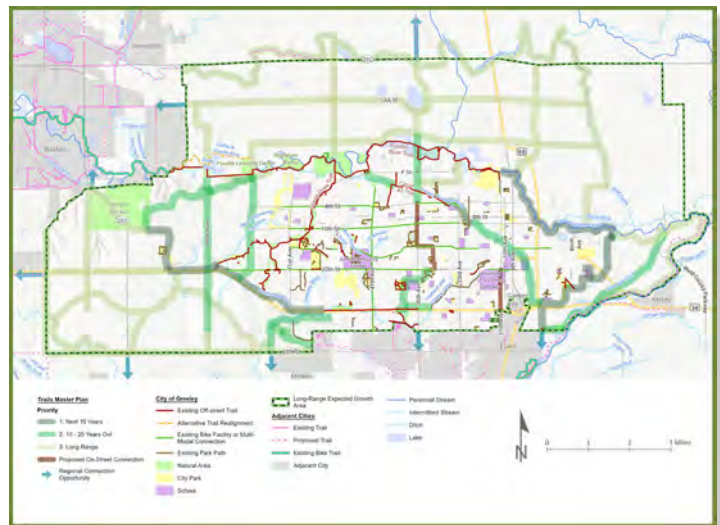
Thus, the Project Team conducted a robust review of the following plans to grasp existing conditions in Greeley:

- City of Greeley Bicycle Master Plan, May 2015
- Greeley Parks, Trails, and Open Lands Master Plan, 2016
- Imagine Greeley Comprehensive Plan, February 2018
- Adequate Public Facilities Analyses Maps, December 2018
- City of Greeley Strategic Housing Plan, February 2019
- Get Outdoors Greeley, February 2021
- Greeley Historic Resources Survey Plan, May 2021
- City of Greeley Wastewater Utility Plan, June 2021
- City of Greeley Water Transmission and Distribution Master Plan, June 2021
- Greeley Non-Potable Water Master Plan, June 2021
- Greeley Water Efficiency Plan, 2022
- Greeley-on-the-Go Transportation Master Plan, 2023
- City of Greeley Art Master Plan, 2023
- Annual Growth & Development Projections Report, March 2023
- City of Greeley Integrated Water Resources Plan, August 2023
- At Home in Greeley: Greeley Housing Needs Assessment and Plan, September 2023
- Greeley Trails Master Plan Update, March 2025
- Greeley Mobility Development Plan, May 2025
- Transitional Housing Feasibility Study, ongoing
- Homeless and Housing Scoping and Assessment, June 2022
- Culture, Parks and Recreation Master Plan, 2025
- Poudre River Restoration Initiative Master Plan, ongoing
- The City of Greeley Master Plan Procedures / Preparation Guide, ongoing



Greeley-on-the-Go Mobility Plan, March 2025

An analysis of these plans is included in the following pages.



Trails Master Plan Update, March 2025

SETTING THE SCENE: IMAGINE GREELEY

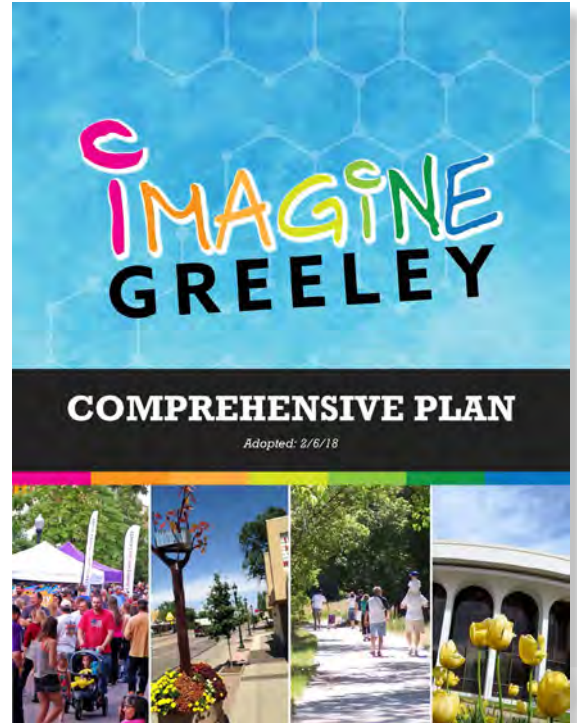
The existing conditions review for the Westside Area Plan began with an exploration of Imagine Greeley, the City’s comprehensive plan.

Imagine Greeley (2018) is the City of Greeley’s fifth master plan formally adopted by its City Council. An updated comprehensive plan, Imagine Greeley offers targeted recommendations aimed at helping the City address issues and opportunities identified since its last comprehensive plan was adopted in 2009. The plan charts Greeley’s planning history as an agricultural colony in the 19th century, its subsequent growth into a planned community with a lively downtown acclaimed for its central park and tree-lined streets, and its eventual rapid population growth in the 21st century, which has produced sprawling suburbanization patterns. Today, Greeley is Colorado’s 12th largest city, boasts an affordable and high-quality lifestyle, and hosts cultural and academic hubs like the University of Northern Colorado (UNC) and Aims Community College.

Imagine Greeley serves as a policy guide to provide a framework for public and private growth and the City’s development decisions over the next 10 to 20 years. This framework applies both within the City’s current geographic boundaries and its Long Range Expected Growth Area (LREGA). While Western Greeley is only occasionally explicitly referenced in the comprehensive plan, the goals and considerations shared throughout the document are especially pertinent to Western Greeley’s growing population.

With a proliferating population, the City of Greeley has asserted its goals to address such growth strategically, ranging from prioritizing compact urban development and infill over sprawl, to incentivizing mixed-use development, to increasing the affordability and diversity of the City’s housing stock.

Additionally, the City has emphasized the importance of ensuring that public facilities are adequate to support this growth and taking precautions to preserve the region’s ecologically sensitive lands.













Imagine Greeley Comprehensive Plan, 2018

PLANNING CONTEXT AND FRAMEWORK

Building on the policy direction of Imagine Greeley, the Westside Area Plan organizes analysis around ten planning categories introduced on p. IX. These categories provide a framework for evaluating prior planning efforts and guiding recommendations specific to Western Greeley.

Assessment of existing plans and stakeholder input highlights key findings, development trends, and area-specific opportunities. Together, these insights form the foundation for the illustrative concepts and strategies that follow.

	CATEGORY	KEY DOCUMENTS
	Economic Health and Diversification	Imagine Greeley (2018)
	Education, Health and Human Services	Imagine Greeley (2018)
	Growth & City Form	Imagine Greeley (2018) Biannual Growth and Development Plan Report (2023)
	Historic and Cultural Resources	Imagine Greeley (2018) Historic Preservation Survey Plan (2021) Art Master Plan (2023) Culture, Parks, and Recreation Master Plan (underway)
	Housing	Imagine Greeley (2018) Homelessness and Housing Services Assessment and Recommendations (2022) Biannual Growth and Development Plan (2023) Housing Needs Assessment (2023) Strategic Housing Plan (underway) Transitional Housing Feasibility Study (underway)
	Infrastructure	Imagine Greeley (2018) Energy Action Plan (2019) Greeley Non-Potable Water Master Plan (2021) Water Transmission and Distribution Master Plan (2021) City of Greeley Wastewater Utility Plan (2021) Water Efficiency Plan (2022) Integrated Water Resources Plan (2023) Adequate Public Facilities Plan Maps
	Natural Resources and Open Lands	Greeley Parks, Trails, and Open Lands Master Plan (2016) Imagine Greeley (2018) Get Outdoors Greeley (2017) Greeley Trails Master Plan Update (2025) Poudre River Restoration Initiative (Draft Presentation) (2025)
	Parks and Recreation	Greeley Parks, Trails, and Open Lands Master Plan (2016) Get Outdoors Greeley (2017) Imagine Greeley (2018) Greeley Trails Master Plan Update (2025) Culture, Parks and Recreation Master Plan (underway)
	Public Safety	Imagine Greeley (2018) Map of Proposed Fire Stations
	Transportation and Mobility	Bicycle Master Plan (2015) Imagine Greeley (2018) Greeley-on-the Go Transportation Master Plan (2023) Greeley Mobility Development Plan (2025)

KEY STAKEHOLDERS	KEY TAKEAWAYS
Economic Development and Urban Revitalization, Chamber of Commerce, Local employers, Local employees	<ul style="list-style-type: none"> • Diversify industries • Attract and retain employers and employees • Expand workforce development opportunities
Windsor School District RE-4, Johnstown-Milliken School District RE-5, Aims Community College, University of Northern Colorado, Human Services Providers, Medical Centers	<ul style="list-style-type: none"> • Align with current and anticipated capacity of school districts • Conduct school district review of new development • Capitalize on current health center expansion
Economic Development and Urban Revitalization, Planning and Zoning	<ul style="list-style-type: none"> • Strategically accommodate population growth • Minimize sprawling, car-centric infrastructure • Enhance the public realm
Culture, Parks, and Recreation, Art Commission Board, Historic Preservation, Historic Preservation Commission	<ul style="list-style-type: none"> • Align new developments with Greeley’s cultural character and Historic Districts • Collect and fund public art
Planning and Zoning, Housing and Homeless Solutions	<ul style="list-style-type: none"> • Strategically accommodate population growth • Attract new residents of all incomes • Increase affordable housing stock • Diversify housing stock within neighborhoods
Public Works, Water and Sewer, Poudre Valley Rural Electric Association, Xcel Energy	<ul style="list-style-type: none"> • Ensure adequate public facilities • Maintain and improve water and sewer infrastructure • Accommodate growing electricity demand
Natural Areas & Trails Division, Poudre River Restoration Initiative	<ul style="list-style-type: none"> • Conserve natural and ecological features • Invest in river restoration • Facilitate access to natural amenities • Anticipate future needs for natural resources
Culture, Parks, and Recreation, Natural Areas & Trails Division, Poudre River Restoration Initiative, Family FunPlex	<ul style="list-style-type: none"> • Increase recreation facilities • Improve green space and trail access
Fire Department, Police Department	<ul style="list-style-type: none"> • Ensure adequate public safety facilities • Locate stations to minimize response times • Identify key corridors for emergency response
Planning and Zoning, Public Works, Greeley-Evans Transit	<ul style="list-style-type: none"> • Increase walkability and multi-modal travel options • Improve east-west connectivity • Enhance bike and pedestrian infrastructure • Implement Bus Rapid Transit (BRT)



Economic Health and Diversification

Western Greeley hosts a diverse economy, with major employment sectors including services, retail trade, construction, and manufacturing. Agriculture—particularly cattle raising and processing—continues to shape the surrounding landscape but employs relatively few people compared to major employers such as JBS’ corporate headquarters. Education is a key sector in the broader City of Greeley, with the UNC and Aims Community College among the largest employers, though no colleges are located in Western Greeley. City leadership has consistently emphasized the need to diversify industries, strengthen the employment base, and foster workforce development to build Greeley’s reputation beyond its agricultural roots.

Strategic plans, including Imagine Greeley, identify new industrial, commercial, and employment hubs in Western Greeley. Stakeholders have highlighted opportunities to expand and retain businesses through investments in executive housing and neighborhood amenities that attract employers. While there is excitement for a more compact, mixed-use, walkable urban form, Greeley’s current development pattern remains largely sprawl: single-use pockets with isolated housing types and land uses. This Plan presents an opportunity to build better and yield economic benefits for both local businesses and the city. Proposed mixed-use neighborhoods are designed to integrate residential, retail, and commercial uses, creating connected, walkable communities that support residents and businesses alike. Western Greeley’s agricultural assets also present opportunities to attract agritech companies, supported by new industrial and office spaces.

Growth along key corridors, such as U.S. Highway 34 and 10th Street, is gradually bringing employment closer to residential areas, reducing commute times and supporting local services. Construction, logistics, and light industrial sectors remain strong contributors to the local economy, benefiting from improved regional transportation access and available developable land. At the same time, new offices and technology-driven industries are emerging, supported by infrastructure investment.

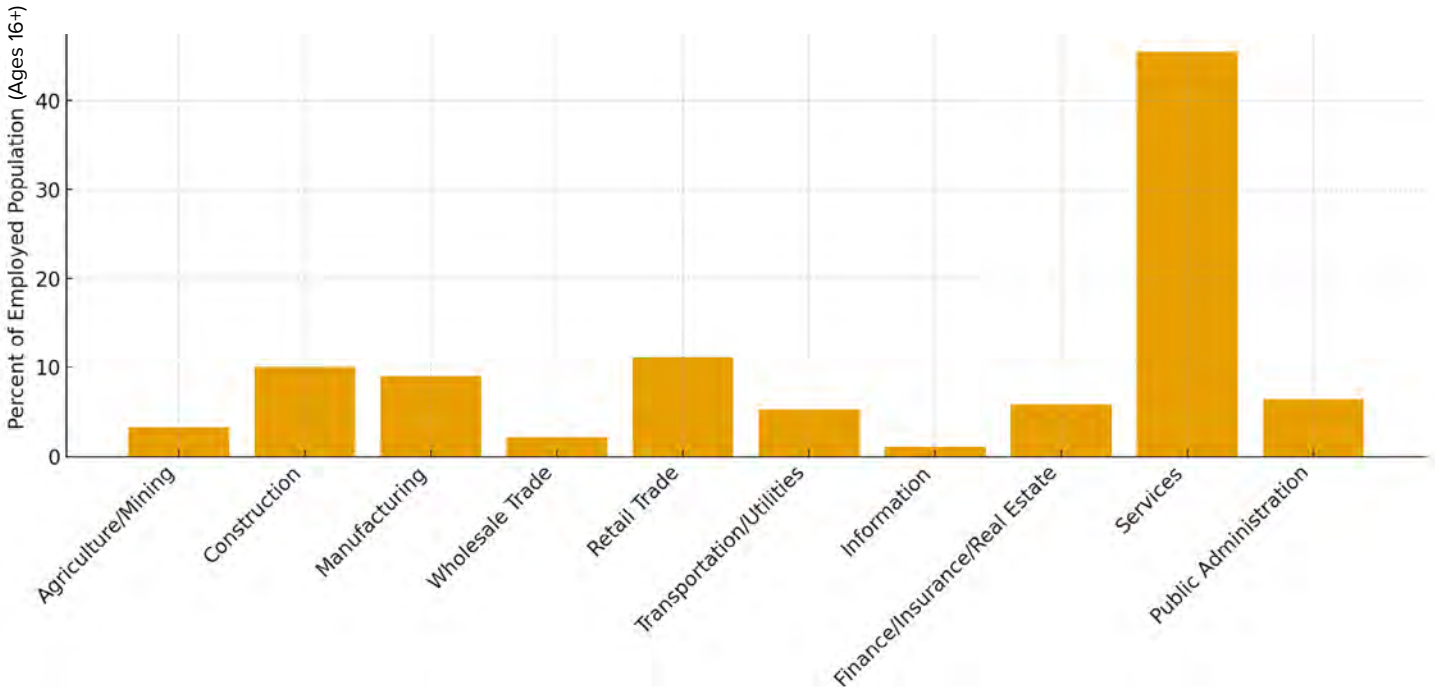
Workforce development partnerships with UNC, Aims Community College, and regional employers are helping to address skills and labor gaps, foster upward mobility, and retain talent within Weld County. Investments in broadband and transportation infrastructure further support remote work, entrepreneurship, and home-based businesses.

The City’s long-term economic strategy emphasizes diversification, balancing traditional industries with innovation, education, and sustainable development. By fostering mixed-use neighborhoods, attracting new employers, and leveraging its agricultural and educational assets, all proposed in this Plan, Western Greeley is positioned to become a more connected, resilient, and economically dynamic part of the region.

The City’s long-term economic strategy emphasizes diversification, balancing traditional industries with innovation, education, and sustainable development. By fostering mixed-use neighborhoods, attracting new employers, and leveraging its agricultural and educational assets, all proposed in this Plan, Western Greeley is positioned to remain a vibrant, resilient, and economically dynamic part of the region.

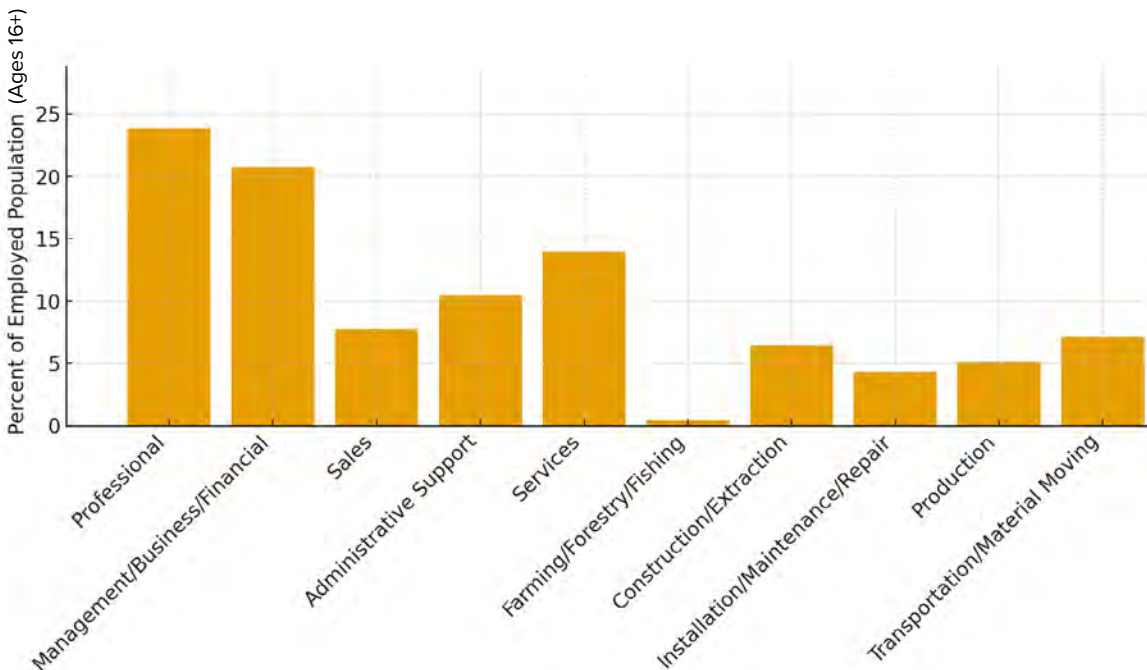


Walkable, amenity-rich districts can strengthen Greeley’s economic base by supporting local businesses and high quality employment areas. This image scored high on Community Image Surveys during the July Public Forum and Workshop.



Westside Study Area: 2025 Employed Population (ages 16+) by Industry

Source: ARC GIS Business Analyst 2025



Note:
 White-collar occupations include professional, management/ business/ financial, sales, and administrative support roles.
 Blue-collar occupations include service, farming/ forestry/ fishing, construction/ extraction, production, and transportation/ material moving fields.

Westside Study Area: 2025 Employed Population (ages 16+) by Occupation

Source: ARC GIS Business Analyst 2025

MARKET ASSESSMENT

Western Greeley has emerged as one of the City's most active growth areas, driven by ongoing residential development, proximity to U.S. Highway 34, and access to regional employment centers. As population growth is projected to exceed 200,000 citywide by 2055, market demand is expanding for a greater mix of housing types, neighborhood retail, community services, and employment opportunities.

Economic growth in the area reflects broader regional trends, with increasing household incomes, significant infrastructure investment, and improved transportation access. These dynamics have helped position Greeley as a desirable community for families and professionals seeking attainable housing and a high quality of life.

MARKET AREAS

The study area includes established neighborhoods west of 83rd Avenue, new subdivisions, and agricultural lands transitioning to residential and commercial development. These corridors are positioned as key growth frontiers linking Western Greeley to surrounding municipalities and major transportation routes.

Commercial and light industrial nodes are forming near U.S. 34 and 10th Street, while higher-density residential areas are clustering near community parks, schools, and natural amenities, such as the Arroyos del Sol Natural Area.

The City is committed to closely tying future development to infrastructure expansion and the formalization of a citywide trail network, connecting every new subdivision to greenways, open spaces, and regional destinations. This approach ensures that growth supports mobility, recreation, and the area's long-term environmental sustainability.

PEER CITIES

Studying comparable fast-growing communities in northern Colorado, such as Windsor, Westminster, Loveland, and Johnston, provides insight into the development pressures and opportunities facing Western Greeley. While each of these communities are primarily comprised of suburban sprawl, they have attempted to incorporate pockets of compact, mixed-use centers and improved mobility networks to support livability as their populations grow. Greeley has the opportunity to set the standard even higher.

Like its peer cities, Greeley's success will depend on managing infrastructure, housing, and open space investments to sustain growth, all while preserving the region's agricultural heritage and water resources.

KEY FINDINGS

- Western Greeley continues to experience some of the highest growth rates in northern Colorado, supported by steady in-migration, strong employment centers, and the availability of developable land.
- Cascadia, a recently approved mixed-use development featuring a regional and sports hub, represents a major investment in Western Greeley and can serve as a catalyst for additional commercial and residential growth.
- Regional population growth continues to drive demand for diverse housing options, retail space, and infrastructure investments, aligning with Weld County's strong economic expansion across the manufacturing, education, and healthcare sectors.
- Access to open space and recreation remains a cornerstone of livability, with the Arroyos del Sol, a recently-protected 978-acre swath of prairies, bluffs, and arroyos, serving as both a natural amenity and an ecological anchor for nearby development.
- The strategic management of water resources and farmland preservation will remain critical to balancing the area's rapid growth with the protection of agricultural heritage and beloved natural amenities.

SHOP & DINE

Western Greeley’s growing population is driving demand for retail and dining, with new centers emerging along U.S. Highway 34 and 10th Street. There is great potential for mixed-use developments to integrate shopping and dining with housing, parks, and pedestrian paths, supporting local businesses and encouraging residents to shop and dine nearby. Opportunities exist for small-scale commercial nodes, outdoor dining, farmers’ markets, and seasonal events that contribute to the area’s emerging community character, emphasizing human-scale design, public spaces, and trail connectivity.

Bradburn Village

Westminster, Colorado

In a regional north-Denver landscape characterized almost entirely by single-use, car-dependent pods, Bradburn breaks the mold and serves as an impressive model for Greeley to emulate. Surrounded entirely by housing subdivisions, apartment clusters, office parks, strip shopping centers, and isolated institutional uses, this new neighborhood demonstrates that it is possible to build towns the way we used to, as mutually supportive combinations of homes, offices, shops, churches, and schools. Even the homes are varied, ranging from small apartments to mansions, with bungalows, row-houses, and live/work units thrown in for good measure. These uses are all collected in a picturesque, walkable landscape of narrow, tree-lined streets, squares, and parks.

When every aspect of contemporary real estate development—from zoning to engineering to financing—is ruthlessly geared around the conventional patterns of suburban sprawl, creating complete communities from scratch presents a challenge, though not an insurmountable one. What was required in Bradburn was principally a shared desire among municipality (Westminster) and developer (Continuum) to plan the site collectively along a new urbanist framework. As a 2004 Urban Land Institute case study about Bradburn stresses, “high-density, mixed-income housing can work in a suburban location...projects like Bradburn tap an unmet demand.”

VISIT & STAY

Western Greeley’s location near regional attractions and outdoor amenities positions it as a hub for short-term visitors and business travelers. New lodging options, including hotels and extended-stay accommodations, can serve both leisure and work travel, while parks, trails, and nearby open spaces can enhance the visitor experience. As the area grows, investments in signage, wayfinding, and connections between hotels, retail, and recreation will support weekend tourism, local festivals, and cultural events that strengthen Western Greeley’s identity.



Bradburn Village Overhead View



Bradburn Village Center



Education, Health, and Human Services

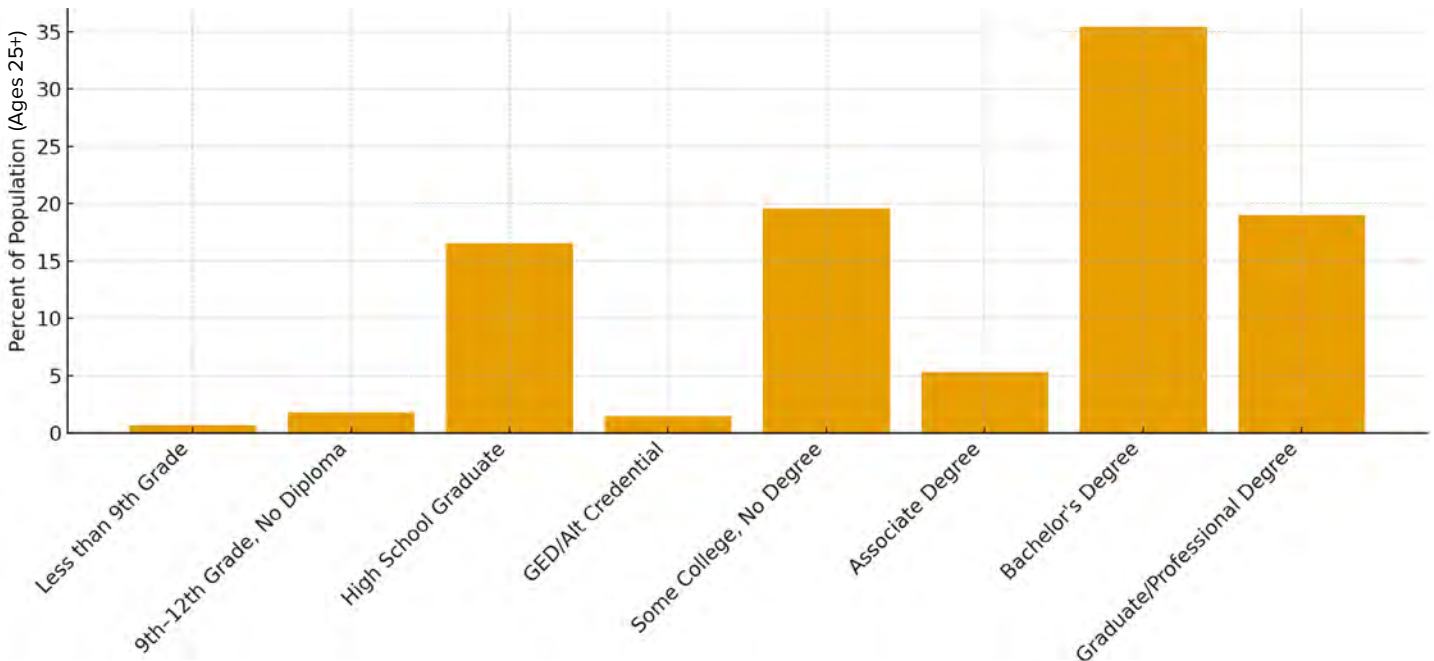
EDUCATION

Greeley boasts strong educational offerings, with regional hubs like the UNC and Aims Community College and a high-quality local school district. However, new developments must align with the City’s school districts to ensure all of Greeley’s students can attain a free, robust education.

The City of Greeley considers the effects of new development on its education system by referring proposed residential developments to its school districts for their review. The City also plans to collaborate with school districts in developing long-range school siting plans and encouraging new schools on sites that are proximate to their populations, co-located with facilities and amenities, separated from land use hazards, and served by transportation options providing safe routes to and from schools.

During stakeholder discussions, leaders from the Weld RE-4 and Johnstown Milliken RE-5J school districts, both of which are in Western Greeley, provided key insights into how the City’s rapid growth may impact their districts.

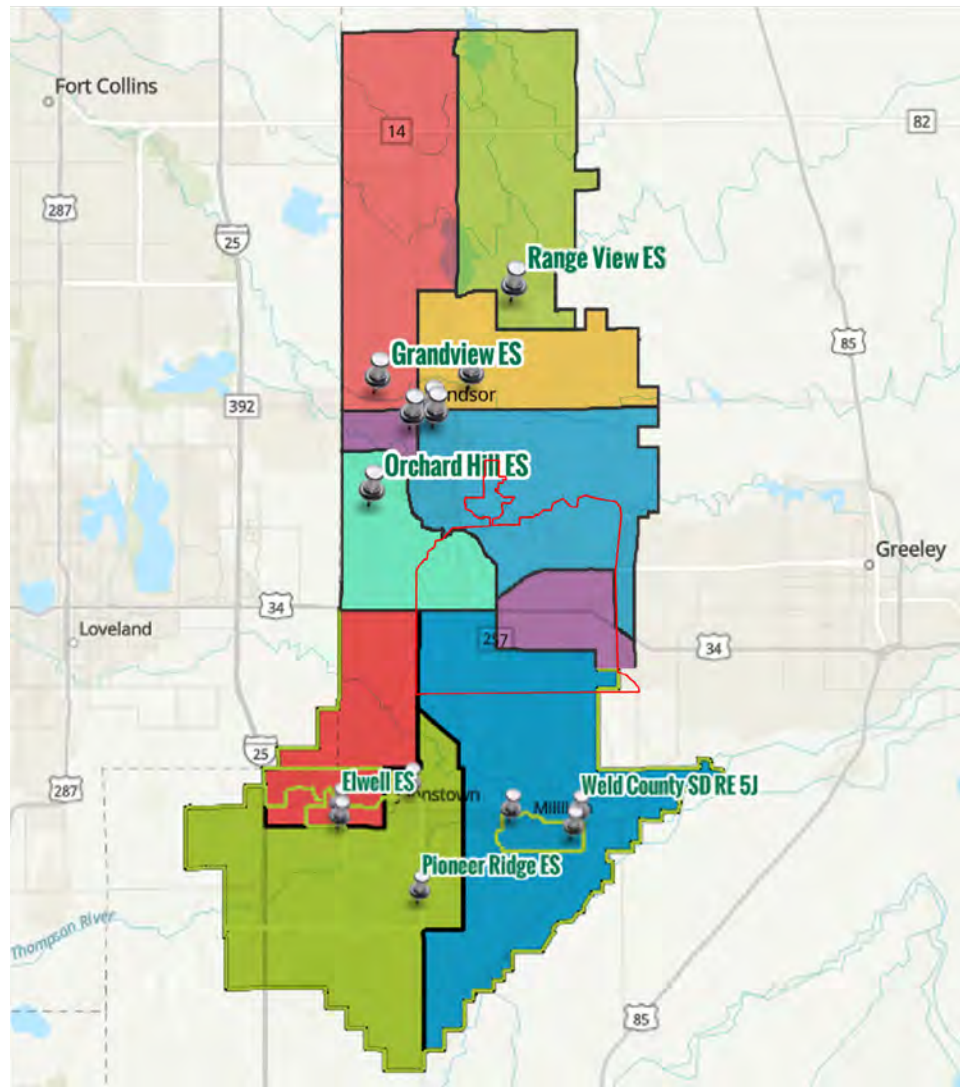
The K-12 Weld RE-4 School District serves over 8,000 students at seven elementary schools, three middle schools, two high schools, and two charter schools. Elementary, middle, and high schools have around 600, 900, and 1,200 students, respectively. An estimated one-third of students at Weld RE-4 use the bus transportation system. Johnstown Milliken RE-5J has just over 4,000 students, with one early childhood school, three elementary schools, two middle schools, one high school, one specialized K-12 school, and one specialized 6-12 school. For both school districts, elementary, middle, and high school acreage standards are 10, 25, and 40, with a density rate of 0.38 across all levels. District leaders estimated that eight in ten high school students drive to and from school daily.



Westside Study Area: 2025 Educational Attainment (Population ages 25+)

Source: ARC GIS Business Analyst 2025

Both the Weld RE-4 and Johnstown Milliken RE-5J districts have established cash-in-lieu agreements with the City of Greeley, which dictate school growth relative to the City's growth. Representatives from each district shared that proposed developments, including Cascadia, have catalyzed the need for school growth, particularly in Weld RE-4. In general, westward growth in Greeley has engendered conversations with developers about designing neighborhoods that accommodate centrally located schools. This Plan's illustrative plans for the Chismar, Weld Trust, and Delantero Center properties all propose schools located within walking distance of housing, in contrast to Greeley's current reality, in which the vast majority of students travel to school by bus or in a car.



School District Map

HEALTH

As Greeley grows, it needs to keep its people healthy. Greeley is home to North Colorado Medical Center, which serves as the Weld County Public Hospital. The City has established a commitment to capitalize on educational offerings in health sciences at its medical facilities to promote the city as a hub for health education training in Colorado. The mixed-use development pattern envisioned in this Plan will help realize this commitment.

Western Greeley is currently home to a well-educated population, but this statistic is skewed by the area's small population, coupled with the high number of individuals who work at the JBS Headquarters and live nearby. The socio-demographics of people who will move to Western Greeley in the future will quickly be reflected in these statistics. The quality of the emerging built environment may be the most decisive factor that shapes such trends.



Growth and City Form

Greeley’s rapid growth in recent decades reflects many attractive qualities of the community. At the same time, development has occurred at steadily lower densities than historic patterns, consuming significantly more land per household. As a result, land is being used inefficiently and rapidly depleted. This dispersed development pattern—commonly referred to as suburban sprawl—has increased the cost of capital improvements and municipal services, limited transportation choices, and contributed to negative outcomes for Greeley’s local economy, environment, sense of community, and public health.

Greeley is expected to surpass 150,000 residents by 2038 and reach approximately 200,000 by 2055. Without a shift in approach, continued growth under existing development practices risks reinforcing these challenges and locking in long-term inefficiencies. In response, the City intends to move away from prevailing patterns and actively encourage a more compact urban form, including mixed-use and transit-supportive development, neighborhood centers, and infill redevelopment.

The City has also championed new development that reflects and enhances community character through design quality, streetscapes, public art, and landscaping. These elements are essential, but they are most effective when paired with land use patterns that support walkability, connectivity, and daily access to services.

Periods of rapid growth provide a critical opportunity to reset expectations. When population and investment are rising, local leaders have leverage to guide where and how growth occurs—prioritizing compact, mixed-use land patterns, well-connected street networks, and a diverse range of housing types. By directing development toward walkable centers rather than the isolated, single-use subdivisions that have largely characterized recent growth in Greeley, the City can reduce congestion, support transit, and create places that foster social interaction and civic identity. Early decisions about zoning, block size, parks, and multimodal access will shape whether daily life becomes more convenient, equitable, and affordable as the city expands.

Requirements for high-quality public spaces, shade and tree canopy, energy-efficient buildings, and stormwater-smart design can ensure that new neighborhoods age well and remain resilient in a changing climate. By aligning private development with public goals, Greeley can transform growth from a passive outcome into an intentional tool for long-term community well-being.



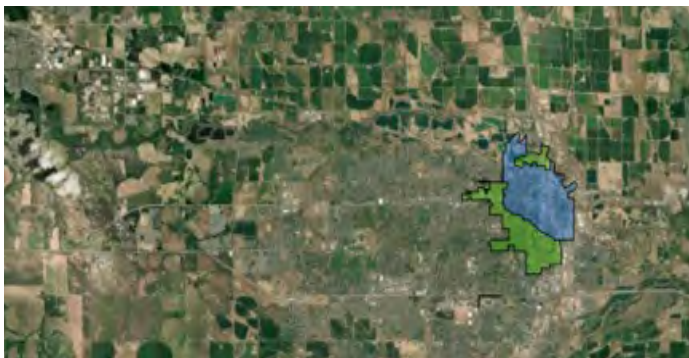
A walkable, mixed-use neighborhood center reflecting community preferences for human-scale streets and integrated uses. This image scored high on Community Image Surveys during the July Public Forum and Workshop.

GREELEY'S URBAN DESIGN HISTORY

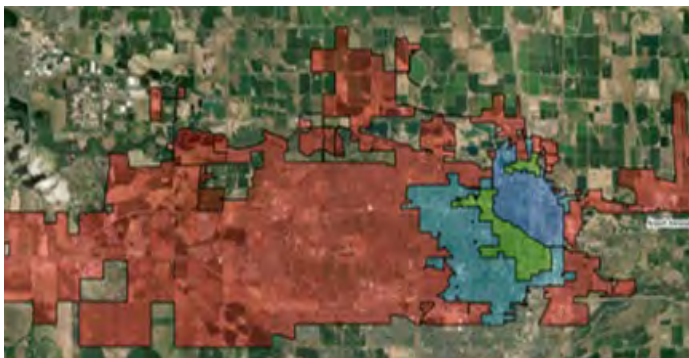
1880: After its founding the “Union Colony,” the City was bounded by the Poudre River and included only those lands east of 23rd Avenue and north of 17th Street. Around this time, the City’s most valued Downtown buildings were constructed, including multiple railroad structures, the former Camfield Building, the Opera House Block, and the First Presbyterian Church. The City was comprised of a grid of streets with ample squares and public open spaces in an effort to attract new residents during a time of intense competition for investment. Greeley was nationally renowned for its quality of life.



1920: Between 1880 and 1920, the City grew in population from roughly 600 people to 11,000 while spatially expanding only a few hundred acres. The City’s grid of streets extended slightly south and west and ended at UNC (founded in 1889). At the same time, some of the City’s most treasured neighborhoods, like Glenmere Park, were designed during the City Beautiful era. The City Beautiful movement aimed to beautify cities through grand and neoclassical architecture, public parks, and tree-lined boulevards. Homes constructed in the Arts and Crafts and English Revival architecture of the time are still some of the region’s most valued.



2020: As its population rose from 11,000 in 1920 to roughly 111,000 in 2020, the City developed quickly, consuming over 30 square miles. During the Westside Area Plan’s kick-off presentation in July of 2025, former Deputy City Manager Rachel Flynn described the growth as “rushed,” “unintentional,” and “under-designed,” compared to previous eras. The disciplined grid of urban streets gave way to expedient, but dizzying, contemporary subdivision patterns, and the overall quality of “design and construction went down.” In recent years, the Downtown has seen urban infill and the restoration of historic structures, and design standards have been elevated as the public looks to the past for inspiration, according to Flynn.



DEVELOPMENT CAPACITY ANALYSIS

A Development Capacity Analysis (also known as a “Build-out Analysis”) is a planning tool used to estimate the maximum potential development that could occur in a planning study area under current or proposed zoning regulations. A Development Capacity Analysis projects how much development (e.g., housing units, commercial space) could be built if all developable land were used to its full legal potential under the Plan’s Westside Land Use Guidance Map in Chapter 4. The map used 2055 as an endpoint. After 2055, the Westside Land Use Guidance Map is expected to be updated.

TOTAL WESTERN GREELEY SUBAREA

The Imagine Greeley Comprehensive Plan (2018) Land Use Guidance Map designates “Areas,” “Open Lands” and “Natural Areas,” “Neighborhoods,” and “Centers.”

The Westside Land Use Guidance Map retains the same land use designations as the previous map, but updates the other categories—such as Dwelling Units Per Acre and Maximum Square Feet Per Acre—based on the information provided below.

- **Employment, Industrial, Commercial** (Dwelling units per acre: 10; Maximum square feet per acre: 210,000)
- **Mixed-use High Intensity:** (Dwelling units per acre: 20; Maximum square feet per acre: 175,000)
- **Mixed-use:** (Dwelling units per acre: 20; Maximum square feet per acre: 175,000)
- **Suburban:** (Dwelling units per acre: 10; Maximum square feet per acre: 20,000)
- **Rural:** (Dwelling units per acre: 2)
- **Natural Areas including:**
 - » River
 - » Tributaries
 - » River / Tributary Buffer
 - » Protected Land / Arroyos

Note that the Imagine Greeley Comprehensive Plan does not provide a maximum square feet per acre for its land use commercial designations and its zoning requirements (i.e., parking requirements and dimensional requirements).

For the purposes of this analysis, the maximums allowed by the highest zoning district (Employment, Industrial, Commercial) were assumed to be six stories with 80 percent lot coverage (requiring 20 percent open space), rounded to the nearest thousand, and five stories for all mixed-use districts.

In addition, note that a likely development outcome would be just a fraction of the highest amount allowed by the maximum densities. The low end of the projection is computed based on a study of square footages and residential densities of recent developments in Western Greeley.

CONCLUSION

The Westside Area Plan’s Land Use Guidance Map provides a strong framework for directing growth in Western Greeley. The Map also provides enough developable area to accommodate even the most optimistic growth projection. The flexibility inherent in the Land Use Guidance Map allows the city to respond to changing market conditions and demographic trends. By providing a range of zoning categories and development options, the Plan accommodates both higher-density infill projects and more conventional suburban development. This versatility not only supports the City’s population growth but also promotes a diverse and resilient community fabric.

The Land Use Guidance Map serves as a reliable tool for guiding development while preserving community character and ensuring that infrastructure, services, and amenities can support a growing population. Continued monitoring and periodic updates will be key to maintaining this alignment and sustaining the City’s long-term vision for growth.

**Western Greeley Land Use Guidance Map Analysis:
Dwelling Units Per Acre and Maximum Square Feet Per Acre**

LAND USE CATEGORY	ACREAGE	MAXIMUM DENSITIES
Employment, Industrial, Commercial	1,320	Dwelling units per acre: 10; Maximum square feet per acre: 210,000
Mixed-use High Intensity	496	Dwelling units per acre: 20; Maximum square feet per acre: 175,000
Mixed-use	4,755	Dwelling units per acre: 20; Maximum square feet per acre: 175,000
Suburban	1,341	Dwelling units per acre: 10; Maximum square feet per acre: 20,000
Rural	3,596	Dwelling units per acre: 2
Natural Areas	3,044	N/A

**Western Greeley Land Use Guidance Map Analysis:
Development Capacity of the Land Use Guidance Map Up To 2055**

LAND USE CATEGORY	ACREAGE	COMMERCIAL SQUARE FEET (PROJECTED)	COMMERCIAL SQUARE FEET (MAXIMUM)	RESIDENTIAL UNITS (LOW PROJECTION)	RESIDENTIAL UNITS (HIGH PROJECTION)
Employment, Industrial, Commercial	1,320	13,860,000	277,200,000	2,640	13,200
Mixed-use High Intensity	496	5,208,000	104,160,000	992	4,960
Mixed-use	4,755	49,927,500	998,550,000	9,510	47,550
Suburban	1,341	N/A	N/A	2,682	13,410
Rural	3,596	N/A	N/A	N/A	N/A
Natural Areas	3,044	N/A	N/A	N/A	N/A
Total	14,552	68,995,500	1,379,910,000	15,824	79,120



Historic and Cultural Resources

HISTORIC RESOURCES

Greeley has two locally designated historic districts on its Greeley Historic Register: the Downtown Greeley Historic District and the Monroe Avenue Historic District. The Downtown Greeley Historic District encompasses the area where Greeley’s commercial center first developed—roughly between 7th and 10th Streets and between 8th and 9th Avenues. The Monroe Avenue Historic District is primarily residential and reflects some of Greeley’s early neighborhood development.

Greeley’s historic districts offer a tangible connection to the City’s rich past, reflecting its evolution from a late-19th Century agricultural hub to a thriving modern community. These districts are characterized by well-preserved architecture, including Victorian-era homes, early commercial buildings, and civic landmarks that embody the City’s cultural and economic history. Walking through these areas provides residents and visitors alike with a sense of Greeley’s heritage—the craftsmanship, urban design, and historical patterns that shaped the city.

Historically, Western Greeley has been sparsely populated and principally home to farmland. As development pressure increases in Western Greeley, new neighborhoods should honor the region’s storied historic setting, while also accommodating a diverse mix of residents, incomes, and generations.

In many ways, recent residential developments in Western Greeley have missed a key opportunity to curate a strong sense of place and pay tribute to Greeley’s historic form. Thus, within new neighborhoods, building forms could reflect the unique character of Greeley’s historic built environment. During stakeholder conversations with Historic Preservation, the Monroe and Cranford neighborhoods emerged as prime precedents for the architecture and design of new developments.

PUBLIC ART

Furthermore, public art helps craft a citywide identity and celebrates Greeley’s diversity. The 2023 Art Master Plan noted that the City’s public art collection contains about 200 sculptures and murals, as well as 415 pieces within City buildings.¹

The City’s One Percent for Art program, which began in 1998, requires any capital improvement project of \$250,000 or more to allocate one percent of its project cost to public art.² Greeley’s westward expansion could tap into this funding source to elevate the City’s historic cultural values and strengthen its cachet.



Historic bungalows in the Cranford neighborhood.

¹ Art Master Plan (2023), 9.

² Art Master Plan (2023), 5.



Natural Resources and Open Lands

During this Plan's creation, Western Greeley stakeholders, including farmers, landowners, business owners, and residents, consistently emphasized the importance of Greeley's natural areas. Building on this shared value, the Westside Area Plan seeks to preserve these landscapes and continue the momentum created by the Arroyos del Sol Natural Area, a 978-acre stretch of arroyos, bluffs, and prairie that serves as an iconic and protected community separator.¹

Other natural areas demand urgent attention, too. While Greeley's community members consider the Poudre River a beloved amenity, it direly needs significant investment and restoration. Channelization and gravel mining have resulted in an approximately two-mile (15 percent) reduction in the river's length since 1950, and encroaching development, particularly in Western Greeley, further threatens its character and existence.² Thus, the City must proactively take measures to protect and celebrate the river as a thriving resource that will exist for generations to come.

Regarding new development, the City has established goals to anticipate future needs for natural resources, protect and conserve its water sources, promote the use of non-potable water for irrigation, manage emissions and pollution, and demonstrate environmental stewardship.³ New development is expected to support these objectives by incorporating natural features into site design, utilizing low-impact stormwater strategies, preserving view corridors, and providing connected open space networks that enhance long-term environmental sustainability.

1 Arroyos del Sol Site Plan (2025), 2.

2 Poudre River Restoration Initiative (2025).

3 Imagine Greeley (2018), 68.



Greeley's newest natural area is the Arroyos del Sol Natural Area.



Housing

Greeley is at a pivotal moment of growth, and expanding access to mixed-income housing has emerged as a citywide priority. The 2023 Housing Needs Assessment found that more than one-third of Greeley households are cost-burdened, home prices have tripled since 2011, and approximately 19,400 new housing units will be needed by 2042 to meet demand.¹ While Greeley’s housing stock has historically been dominated by single-family homes, recent growth has introduced more multifamily development—though often in isolated locations rather than as part of integrated neighborhoods.

Stakeholder conversations with the Housing Solutions Department highlighted a range of tactics currently used to improve housing affordability and feasibility, including deferring fees until the end of sales, land banking, and offering fee offsets tied to property taxes. At the same time, City leadership has emphasized the inclusion of higher-end, or “executive,” housing as a strategy to attract and retain a desired workforce. While each of these approaches responds to real market pressures, they have often been applied within development patterns that separate housing types and price points rather than integrating them.

¹ Housing Needs Assessment (2024).

This Plan sets the groundwork for a broader and more intentional mix of housing types—one that acknowledges both market realities and the social and economic costs of continued segregation by income. The illustrative plans demonstrate how executive housing, workforce housing, apartments, and row houses can coexist within the same neighborhoods, rather than being developed as separate, economically uniform enclaves. Creating this level of integration represents a significant shift from current practice and requires both public-sector leadership and private-sector adaptation. The Plan is designed to support this transition by reducing uncertainty, aligning expectations, and creating conditions for projects that both pencil financially and advance community goals.

Western Greeley’s housing market is among the fastest-growing in Weld County, driven largely by demand for single-family homes and mixed-use residential development. However, much of this growth has occurred in a pattern of sprawl, characterized by single-use areas and clusters of housing that are uniform in type and price. Without intervention, this trajectory risks reinforcing economic separation, increasing infrastructure costs, and limiting access to opportunity.




Separated Apartment Building Types in Western Greeley.



Today, Western Greeley remains largely owner-occupied, with larger lots and family-oriented homes that continue to offer comparatively attainable homeownership when viewed alongside nearby communities such as Loveland and Windsor. Recent developments, including those around Cascadia, reflect growing demand for connected neighborhoods linked to parks, trails, and community amenities. This demand underscores both the opportunity—and the necessity—to move beyond conventional development patterns and toward neighborhoods that support a greater mix of incomes, housing types, and daily needs.


WEST GREELEY

2025




\$116,659

MEDIAN HOUSEHOLD INCOME




\$592,070

MEDIAN HOME VALUE



\$41,610

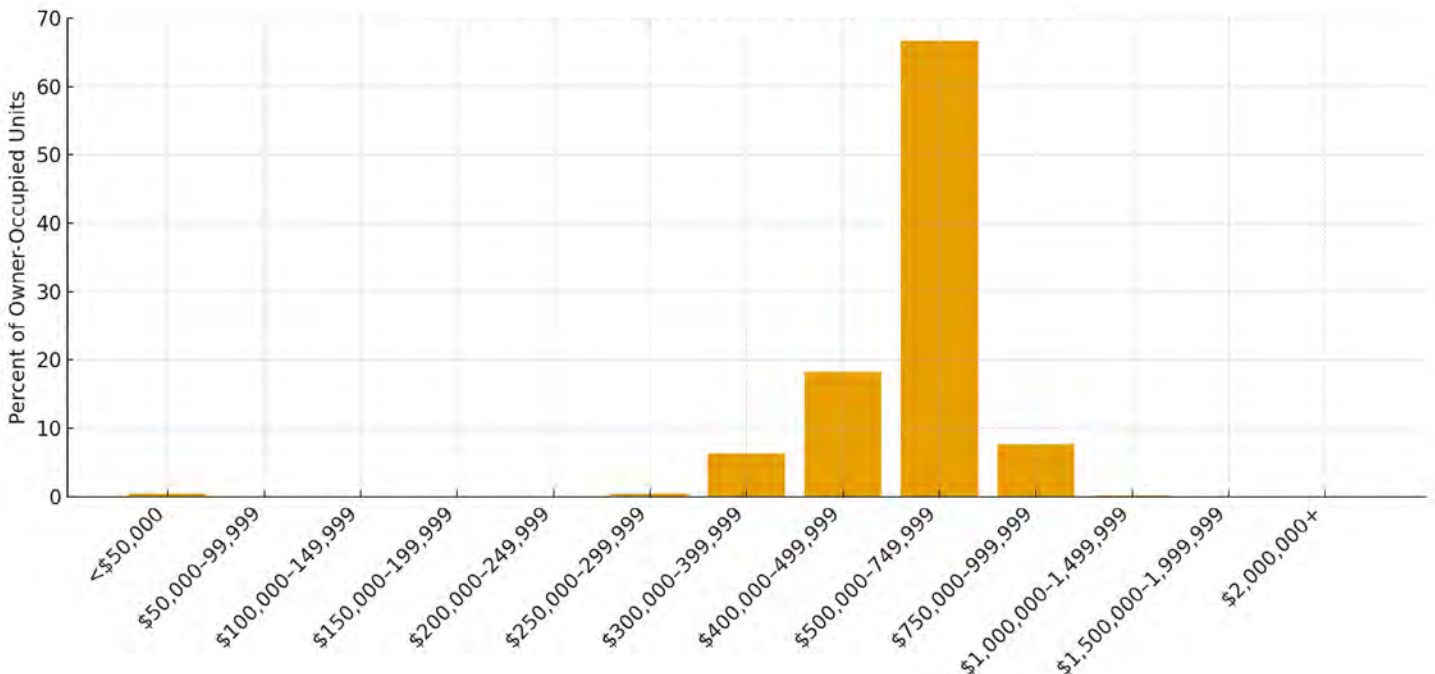
PER CAPITA INCOME



33.4

MEDIAN AGE

Source: ARC GIS Business Analyst 2025



Westside Study Area: Owner-Occupied Housing Units by Value

Source: ARC GIS Business Analyst 2025



Infrastructure

Western Greeley benefits from a strong foundation of utility infrastructure that positions much of the area for continued growth. High-capacity water lines—eight inches in diameter or larger—serve much of Western Greeley, providing reliable water supply for both residential and commercial development and sufficient capacity to accommodate projected population and employment growth.

Wastewater infrastructure is more uneven. The eastern portion of Western Greeley is served by high-capacity sewer lines, also eight inches or larger, capable of supporting both existing development and future growth. In contrast, the westernmost portion of Western Greeley currently lacks comparable sewer service, which limits near-term development in that area.

Planned sewer extensions will address this gap and bring service to the westernmost portion of Western Greeley, consistent with Colorado requirements for new development. Once completed, these improvements will allow growth to occur more efficiently and in coordination with land use planning, supporting public health, service reliability, and orderly development across the area.

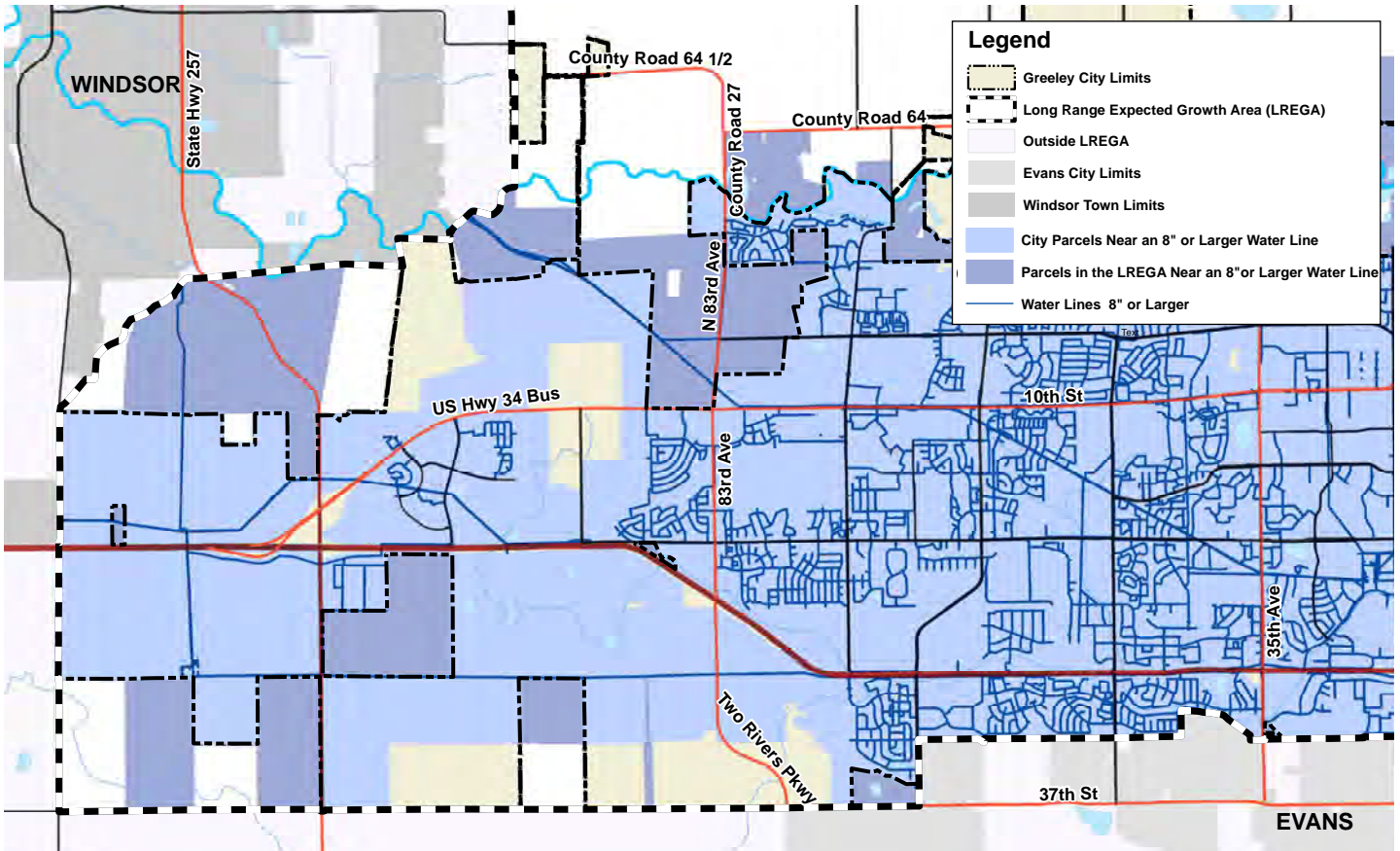
Electric service in Greeley is primarily provided by Xcel Energy, though the Poudre Valley Rural Electric Association (PVREA) serves Western Greeley. According to the 2019 Energy Action Plan, PVREA customers accounted for approximately six percent of the city’s residential electricity use and four percent of commercial electricity use in 2017.¹ While this represents a relatively small share, the number of PVREA residential customers increased by nearly 60 percent between 2015 and 2017, reflecting recent growth trends in Western Greeley.²

¹ Energy Action Plan (2019).

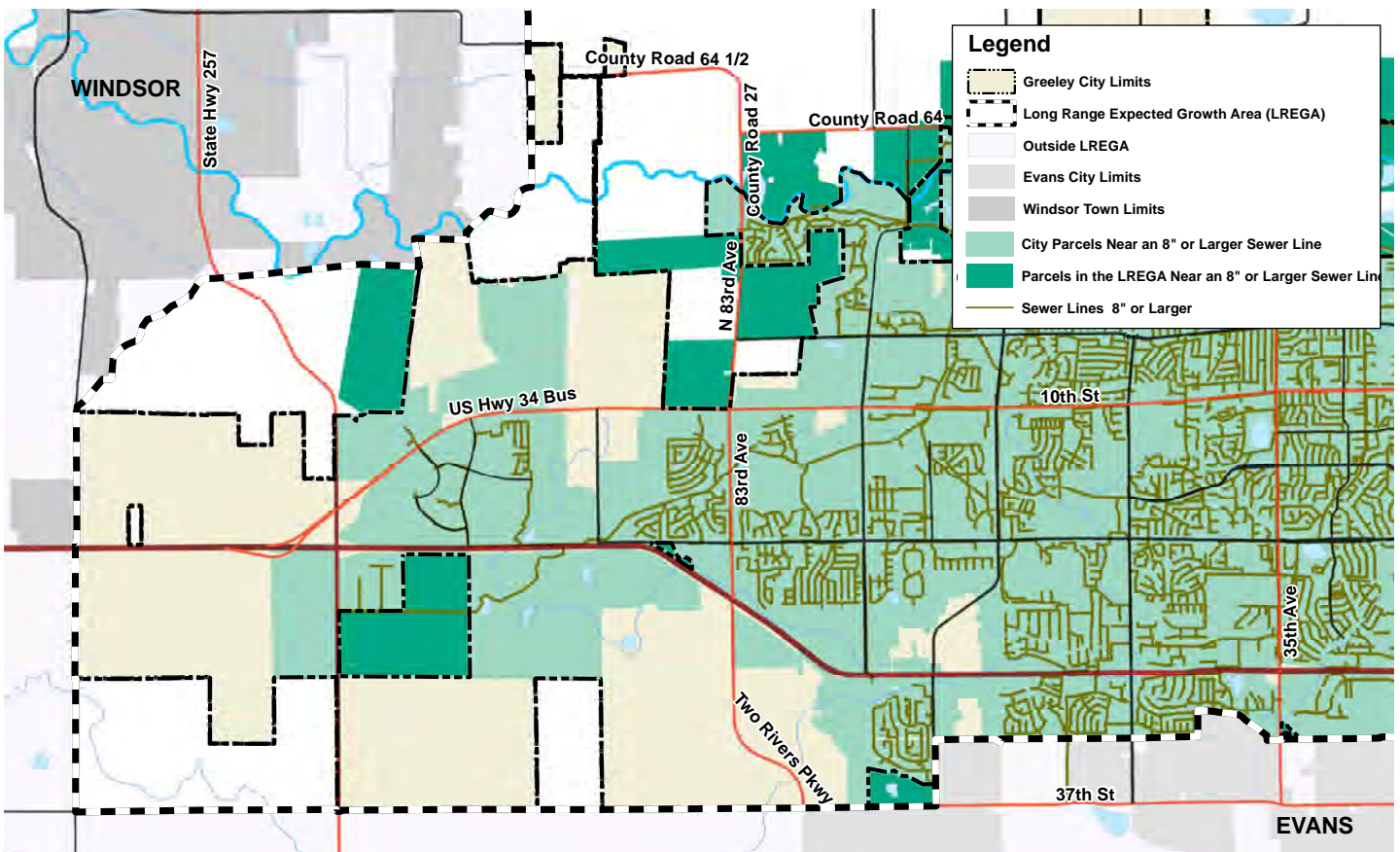
² Energy Action Plan (2019).

Projection Tier	Acres	Population	Residential Demand	Non-Residential Demand	Total Demand
			MGD (MILLION GALLONS PER DAY)	MGD (MILLION GALLONS PER DAY)	MGD (MILLION GALLONS PER DAY)
Existing	11,883	107,146	N/A	N/A	46.0
2025	15,116	128,458	2.4	2.1	50.5
2040	29,467	179,430	8.2	5.5	59.7
Build-out	44,914	425,271	35.9	20.4	102.4

The 2021 Water Transmission & Distribution Master Plan prepares for successfully anticipating Greeley’s population growth.



Adequate Public Facilities Analysis (Water), 2018



Adequate Public Facilities Analysis (Sewer), 2018



Parks and Recreation

A primary goal of the Westside Area Plan is the creation of regional parks and trails. In stakeholder meetings, members of the Culture, Parks, and Recreation department voiced a dearth of such amenities, especially west of 83rd Street. This presents an opportunity for the Plan to fill this gap. The Regional Plan identifies a potential regional park site just south of the Arroyos del Sol Natural Area, positioned to serve both existing neighborhoods and the City's planned westward growth.

Greeley touts two public recreation facilities: the Family FunPlex and the Greeley Recreation Center. The City's rapid population growth will likely necessitate a third recreation center. Such space, which is proposed to be located in Western Greeley, must be open for public access, as private recreational and entertainment districts cannot sufficiently serve Greeley's broad population.

The Trails Master Plan Update, adopted by City Council in March 2025, charts a strategy for developing a robust trail system in Greeley and highlights "nature deficit areas," or neighborhoods that are further than a ten-minute walk to a high-quality natural area.¹ While Western Greeley has a comparatively lower stock of deficit areas than the rest of the City, this Plan seeks to maintain access to parks and natural areas amidst new development. Citywide, 82,000 residents (75 percent of households) do not live within walking distance of a quality park or natural area.²

As Western Greeley continues to develop, new neighborhoods should expand the network of public spaces. Increased access to parks, open spaces, and natural drainage corridors provides daily recreational opportunities and enhances community well-being.

New open areas, such as the Arroyos del Sol Natural Area, can serve as anchors for Western Greeley's public realm, connecting residential and commercial districts through greenways and shared trails.

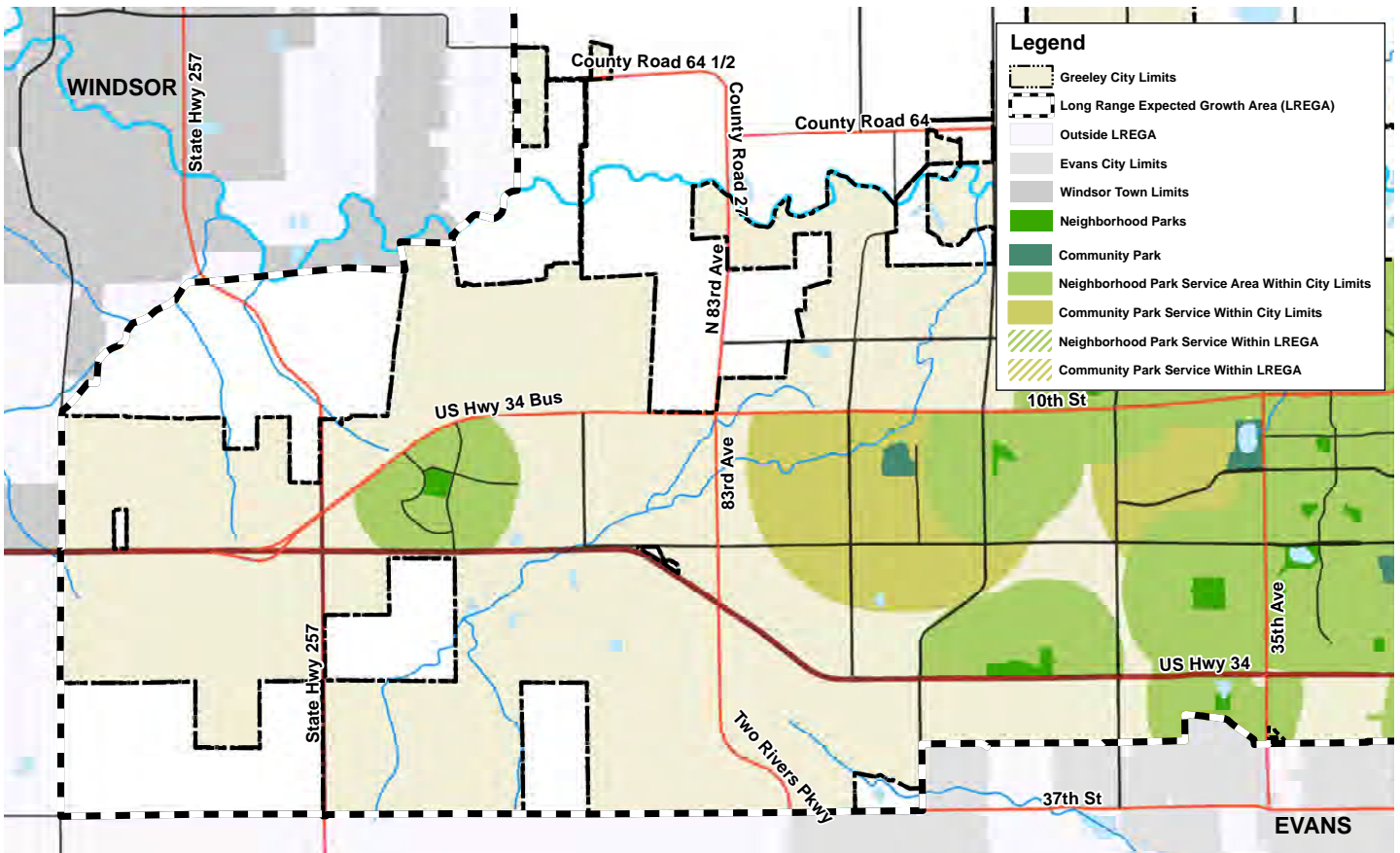
Each new subdivision presents an opportunity to incorporate small-scale plazas, community parks, and trailheads, strengthening both connectivity and social cohesion. Future development should prioritize an equitable distribution of public spaces across neighborhoods, supported by sustainable water management and thoughtful landscape design.

Chapter 4 of the Westside Area Plan proposes a comprehensive trail network that builds directly on the foundation established by the 2025 Trails Master Plan. By using the same alignments identified in the Master Plan, the Westside Area Plan ensures continuity with the City's existing and planned trail system while reinforcing regional connectivity. This approach maintains consistency in trail design and alignment, allowing residents to navigate the area efficiently and safely while promoting active transportation and recreational opportunities throughout Western Greeley.

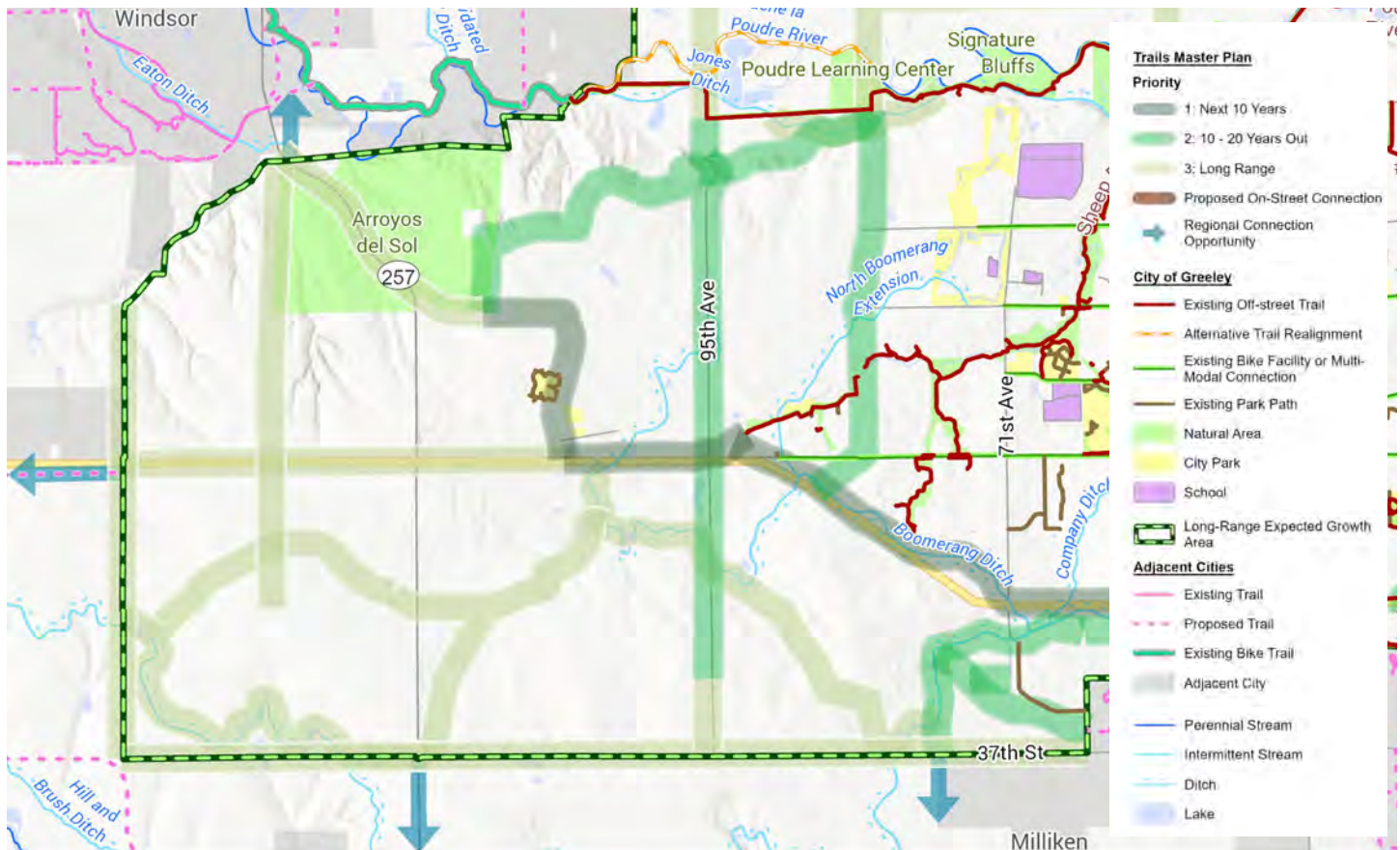
In addition to existing alignments, Chapter 4 introduces new loops and spurs that extend the trail network into emerging town centers and neighborhoods. These additions are designed to improve access to key destinations, offer convenient connections between residential areas and commercial hubs, and enhance recreational opportunities for all users. By strategically expanding the trail network, the Westside Area Plan fosters a connected, walkable, and bike-friendly community that supports sustainable growth and encourages healthy lifestyles.

¹ Trails Master Plan Update (2025).

² Get Outdoors Greeley (2017), 16.



Adequate Public Facilities Analysis (Parks), 2018



The 2025 Trails Master Plan update is ten-year vision for Greeley's trails system.



Public Safety

Ensuring timely and effective public safety services is essential to supporting continued growth and development in Western Greeley. Any new development in the area must be accessible to emergency response services, including fire and police, and the placement of facilities requires careful consideration of response times, operational needs, and long-term service demand. The national standard for fire response time is four minutes, which generally corresponds to a service area within approximately a 1.5-mile radius. As Western Greeley continues to develop, meeting this standard will require additional public safety infrastructure strategically located within the area.

The Westside Area Plan presents an opportunity for the Fire and Police Departments to coordinate closely with Planning and Zoning when evaluating development proposals and identifying appropriate locations for new public safety facilities. Currently, Fire Station 6 is the only station serving Western Greeley, and projected growth will increase service demands beyond the capacity of existing facilities. The Fire Department has expressed interest in locating a new station within or adjacent to the Cascadia development, potentially in the form of a mixed-use facility that combines fire operations with civic or community-oriented uses.

In addition to a new fire station, Western Greeley has a demonstrated need for a Public Safety Complex that would serve as a joint operations center for Fire and Police services. A five-acre site has been committed

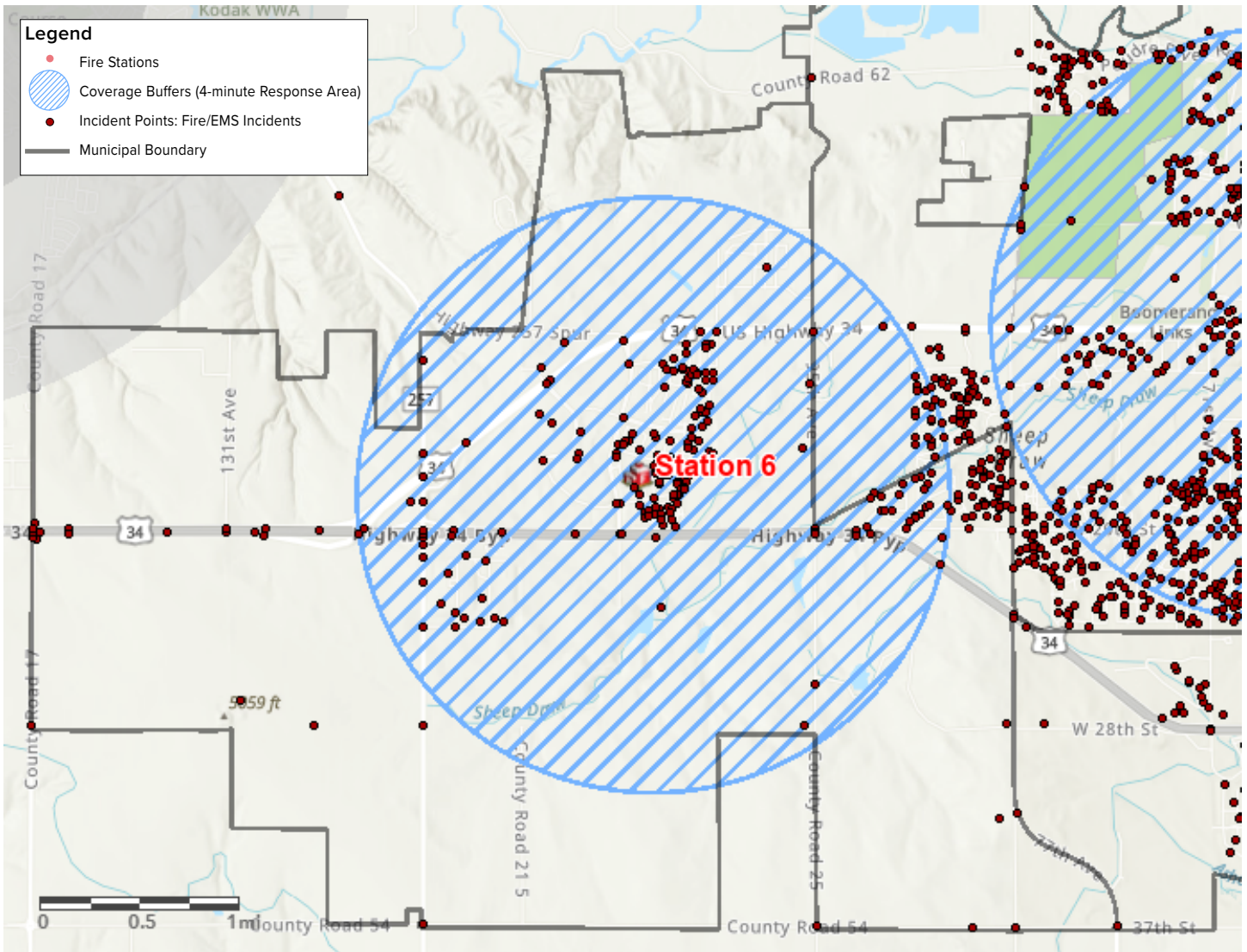
within the Cascadia project for emergency operations; however, this site alone may not be sufficient to accommodate the full range of facilities and functions required to serve the Western Greeley area over time. A larger or additional site may be necessary to support a comprehensive public safety presence, including space for emergency response vehicles, training, administration, equipment storage, and coordinated operations.

The proposed Public Safety Complex could function as a Joint Emergency Operations Center (JEOC), supporting daily coordination between Fire and Police services while also serving as a backup emergency operations and staging facility in the event that the City's downtown operations center is unavailable due to natural disasters, infrastructure failure, or other emergencies. Locating such a facility in Western Greeley would enhance system redundancy, improve citywide resilience, and strengthen emergency preparedness.

Future planning efforts should continue to evaluate site needs, access requirements, and compatibility with surrounding development to ensure that public safety facilities are well-integrated into the urban fabric while remaining highly functional. Early identification and preservation of adequate land for public safety uses will be critical to meeting response time standards, supporting interdepartmental coordination, and ensuring that Western Greeley residents and businesses receive a high level of emergency services as the area grows.



Greeley Police Department



Western Greeley currently has one fire station, Station 6.



Fire Station 6 in Western Greeley



Transportation and Mobility

Greeley maintains over 850 lane-miles of roadway and 700 miles of curb, gutter, and sidewalk, supported in part by the voter-approved 0.65 percent Keep Greeley Moving tax, which has funded improvements to more than 800 lane-miles of streets since 2016. The 2025 Mobility Development Plan guides future multimodal investments to expand walking, biking, transit, and regional connections.

Currently, the city largely lacks infrastructure that makes walking safe, comfortable, and practical. New development and planning efforts should prioritize walkability to foster a more connected, livable, human-scale city. The Plan also identifies opportunities for bus rapid transit (BRT) in premium transit corridors¹, with 10th Street as a key east-west route to enhance connectivity and deliver cultural, economic, and social benefits. Transportation demand management strategies are being pursued to encourage alternative modes of travel.

Cycling infrastructure remains limited, particularly along east-west corridors and outside downtown. Priorities include 4th Street and 20th Street, where bike lanes and improved signage, as recommended in the 2015 Bicycle Master Plan², would strengthen east-west connectivity.

Western Greeley's growth has been supported by roadway expansion along U.S. Highway 34 and 10th Street, improving mobility. Historically oriented toward vehicular access, future subdivisions are increasingly balancing auto-oriented design with trail and pedestrian connections. The emerging trail network, including future extensions of the Poudre River Trail and Arroyos del Sol corridors, will provide continuous options for pedestrians and cyclists. Applying complete streets principles—safe crossings, bike lanes, and landscape buffers—along major corridors offers additional opportunities as development continues.

STREET HIERARCHY

Western Greeley's roadway system is defined by wide arterials and collector roads that establish neighborhood boundaries and influence development patterns. These corridors, primarily the major arterials and collectors, should be transformed into mixed-use, pedestrian-friendly boulevards with consistent building frontage, street trees, and safe access points. Achieving this transformation will require coordinated planning, updated zoning standards, and strategic infrastructure improvements.

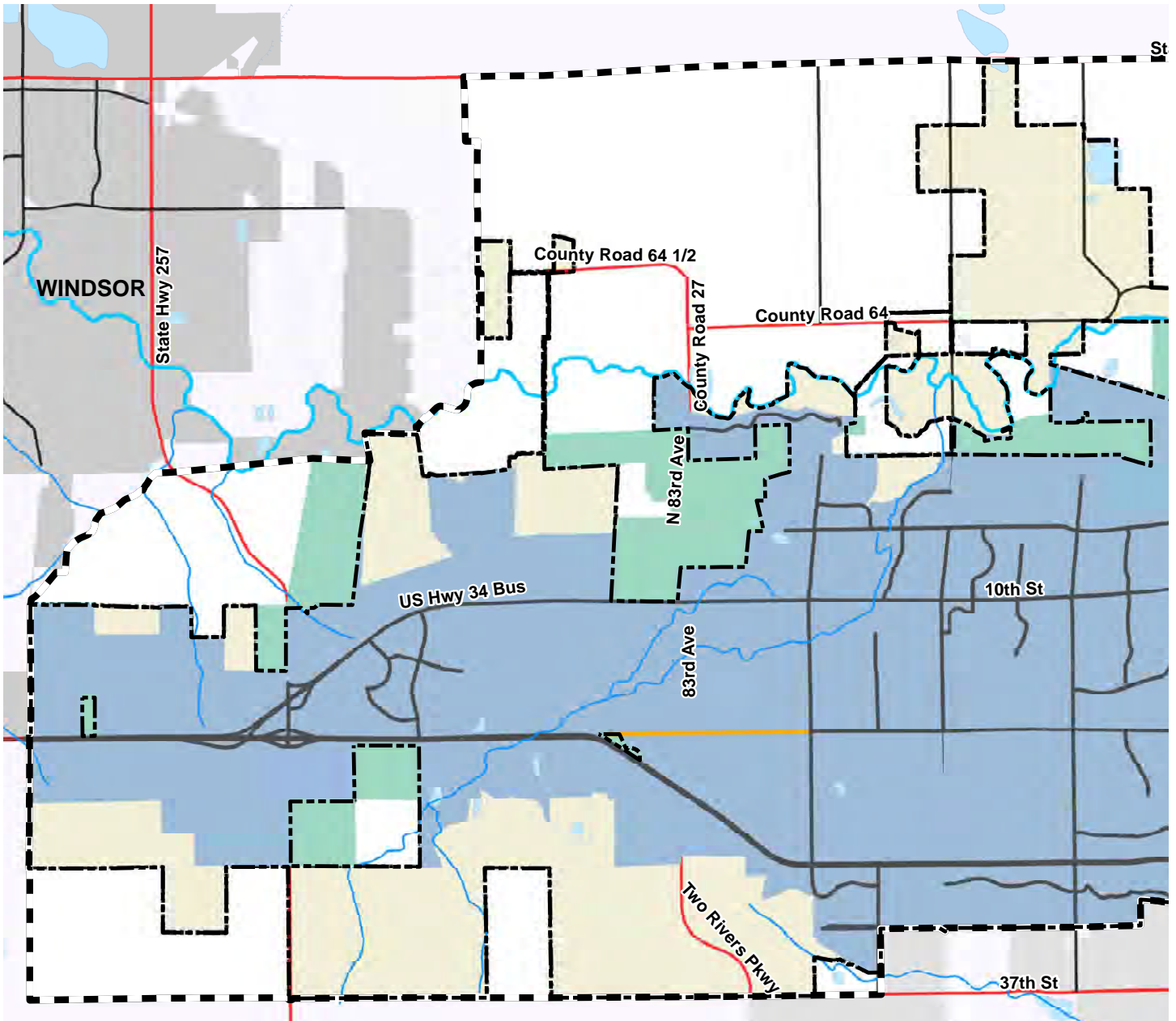
Residential subdivisions are increasingly designed with connected local streets rather than isolated cul-de-sacs; however, a robust network of through-streets and cross-access connections is still needed to limit congestion on the existing sparse street network and enhance neighborhood connectivity. As population density grows, multi-modal corridors will play an essential role in linking housing, parks, schools, and neighborhood centers. Current street standards in Western Greeley are often wider than necessary, and updates aligned with best practices for walkability, safety, and context-sensitive design should guide future development.

SAFETY

Recent infrastructure improvements have enhanced lighting and intersection visibility along major corridors, yet sidewalk and bike network continuity remain incomplete in several subdivisions. Design standards for new developments should continue to promote safe pedestrian crossings, traffic calming, and context-sensitive street design. Additionally, implementing stormwater drainage improvements along key roadways can reduce localized flooding and improve overall safety for all modes of travel.

¹ Greeley Mobility Development Plan (2025), 47.

² Bicycle Master Plan (2015), 5-11, 5-12.



Legend

- Greeley City Limits
- Long Range Expected Growth Area (LREGA)
- Outside LREGA
- Evans City Limits
- Windsor Town Limits
- City Parcels Accessible to Major Streets
- Parcels Within the LREGA Accessible to Major Streets
- Major Streets Constructed to City Standards
- Major Streets on 2-Year CIP to be Reconstructed to City Standards

AREA?

Please provide a written description of the scene or event for the area of the city.
Please check the box next to the description that is most appropriate for the area of the city.
If you are not sure, please check the box for "Not sure".



None of the



Not sure



03

COMMUNITY OUTREACH

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PLANNING PROCESS

To complete the Westside Area Plan, the project team conducted a year-long planning process, grounded in extensive public engagement and technical collaboration. It involved the following key components.

1. ANALYSIS

The project began with an in-person kickoff and a series of in-person and virtual tours across the study area, including comparative visits to peer communities. These efforts, combined with multiple stakeholder conversations and development review assistance, allowed the team to build a detailed understanding of Western Greeley's current development patterns, opportunities, and constraints.

The Speak Up Greeley website also played a critical role in public engagement. It advertised the Plan and upcoming public events, ensuring community members were aware of opportunities to participate. For those unable to attend in-person events, an online survey enabled residents to provide input and share their perspectives on the future of Western Greeley.

2. VISIONING

The visioning phase was the heart of the process and centered on a multi-day charrette held July 7–11, 2025. The charrette began with a tour of Western Greeley and selected peer communities, giving participants a shared frame of reference for discussing design, land use, mobility, and community character.

On July 8, the team facilitated a full day of stakeholder meetings followed by a public workshop that brought together residents, community leaders, and technical experts. The workshop featured two presentations to help ground the community's ideas in best practices. Jeff Speck of Speck Dempsey introduced core walkability principles and discussed the health, economic, and social benefits of designing neighborhoods around pedestrians rather than automobiles. Jason King of Able City introduced the concept of placetypes and demonstrated how these tools can help shape mixed-use, walkable, and livable neighborhoods tailored to the unique character of Western Greeley.



Site tours with the Project Team



Interactive exercises with members of the public

Following the presentations, participants worked collaboratively in small groups to draw directly on maps of the study area—identifying where new neighborhoods, centers, parks, pathways, and employment areas could be located. Community members emphasized the importance of offering a full range of housing options, from attainable and workforce housing to executive housing, while still respecting the rural heritage, long-standing agricultural landscapes, and distinct identity of Western Greeley.

On July 9 and 10, the Project Team conducted a series of technical meetings with City staff, developers, and service providers. During the same period, the team synthesized the ideas gathered during the workshop, identifying consensus themes, refining emerging development alternatives, and beginning to translate community aspirations into a regional plan framework. The team also prepared preliminary illustrations of sample neighborhoods and placetypes to demonstrate how walkable mixed-use development patterns can be integrated into Western Greeley in context-sensitive ways.

The charrette concluded on July 11 with a wrap-up meeting with the City, during which the consultant team summarized major findings, confirmed future directions, and established the foundation for the Plan and code.

3. PLAN AND CODE DRAFTING

Building on the community-guided vision, the team drafted the Westside Area Plan and the optional Westside Form-Based Code Overlay. These documents were refined through iterative testing, interdepartmental review, and additional stakeholder input. Throughout drafting, the ideas generated during the charrette—particularly those related to walkability, placetypes, housing diversity, and rural character—guided the Plan’s land use framework, mobility recommendations, and development standards.

4. ADOPTION

The final phase involved staff training, public review, and formal adoption procedures. City staff were prepared to administer the Plan and Form-Based Code, ensuring that the community’s vision for a walkable, mixed-use, and context-sensitive Western Greeley can be implemented effectively.



Participants at the open house



Designing on-site in Greeley

STAKEHOLDER OUTREACH METHODS

TECHNICAL WORKING GROUP

A Technical Working Group, composed of representatives from City of Greeley departments, met with the Project Team throughout the planning process. This group provided essential data, reviewed technical materials, and offered guidance on infrastructure, land use, and environmental considerations to help align Plan recommendations with citywide goals and service capacities.

PUBLIC OUTREACH & EVENTS

The public was engaged through multiple avenues, including community open houses and online surveys. These events provided opportunities for residents to share their priorities regarding housing diversity, trail access, and the preservation of open space. Feedback gathered through these events directly informed the Plan's guiding principles and implementation strategies for the subarea's future.

TOPIC-BASED FOCUS GROUPS

A series of topic-based focused group discussions were conducted early in the planning process to gather detailed feedback from subject-matter experts and residents. These discussions explored specific themes, such as:

- Infrastructure and Public Services (water, wastewater, drainage, and roads)
- Parks, Trails, and Open Space Connectivity
- Economic Development and Employment Opportunities
- Housing and Neighborhood Design

Each group contributed unique perspectives on how Western Greeley can balance growth, livability, and long-term sustainability.



Participants at the open house

CHARRETTE

To establish a shared vision for the future of Western Greeley, the Project Team facilitated a multi-day planning charrette that drew City staff, developers, and community members. The charrette included open work sessions and presentations focused on land use, infrastructure, parks, and form-based design concepts. Participants collaborated with the Project Team to identify opportunities for neighborhood connectivity, future trail alignments, and community amenities that support quality growth. Feedback gathered during the charrette directly shaped the Plan's development framework and key recommendations.

FOCUSED OUTREACH

Targeted outreach efforts were designed to solicit input from a broad range of stakeholders, including residents from emerging neighborhoods, local business owners, and developers active in Western Greeley. Through small-group meetings and interviews, the team gathered perspectives on issues, such as water availability, infrastructure needs, traffic management, and housing diversity. This outreach helped ensure that the Plan reflects the priorities of both long-term residents and new community members shaping Greeley's westward growth.

OPEN HOUSE

The Project Team shared draft goals, maps, and concepts with the public during a final community open house at the Greeley Family FunPlex. Attendees were invited to review visual displays, vote on actions, and discuss ideas directly with the Project Team and City staff. The event was promoted through the City's website, social media, and neighborhood communications, attracting residents, property owners, and local merchants. Public input from the open house helped refine the Plan's final recommendations related to land use, parks, and infrastructure.



Community members at the charrette

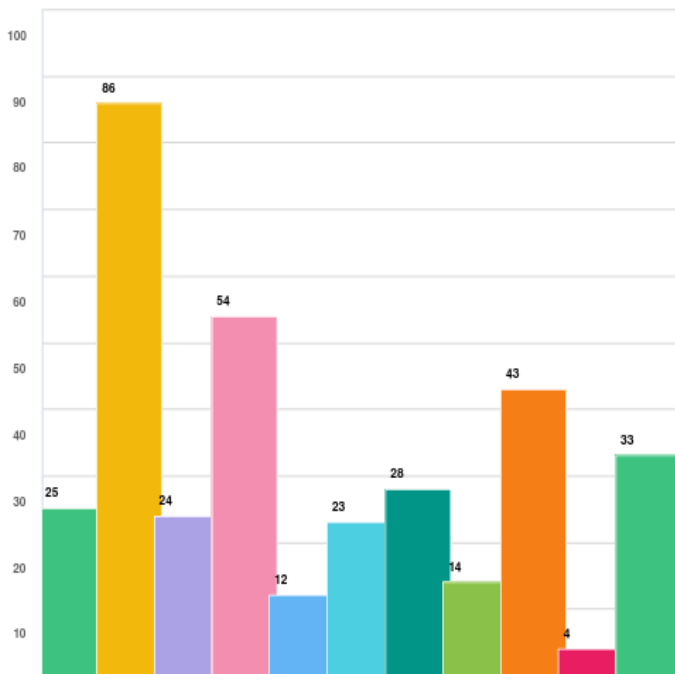
ONLINE SURVEY

An online community survey solicited public input on growth priorities, neighborhood character, and infrastructure needs across Western Greeley. The survey, available in both English and Spanish, received over 100 responses from residents, property owners, and business representatives.

The survey provided valuable insights into how the community envisions Western Greeley’s future, highlighting preferences for diverse housing options, improved trail connections, extended parks, and well-coordinated infrastructure to support continued growth.

Visitors	1046	Contributors	148	CONTRIBUTIONS	150
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Which of the following aspects of living in Greeley do you value most? Please select your top three.



Question options

- Community Engagement
- Natural beauty, parks, green spaces, and trails
- Cultural diversity
- Access to amenities – shopping, retail, grocery, dining
- Transportation access
- Entertainment
- Outdoor recreation
- Water parks, pools and splash pads
- Access to health care
- Available childcare
- Housing options and affordability

Example of survey results



Existing auto-oriented residential development in Greeley. Community members expressed a desire for higher-quality, more walkable neighborhood design standards moving forward.

SURVEY FINDINGS

GREELEY TODAY

Survey responses reveal a clear picture of what residents appreciate most about living in Greeley:

Community Values:

- Natural beauty, parks, and trails were highly valued, reflecting strong support for continued investment in open space and outdoor recreation.
- Access to health care, shopping and dining, and cultural diversity were also frequently selected as top community strengths.
- Residents appreciated community engagement and felt connected to Greeley’s welcoming atmosphere and strong local identity.

Sense of Place:

- A majority of respondents (over 70 percent) agreed that Greeley’s heritage and character should be preserved and strengthened as new development occurs.
- Many noted that portions of Western Greeley lack a distinctive identity and could benefit from placemaking efforts, improved streetscapes, and more cohesive neighborhood centers.

GREELEY TOMORROW

Survey participants shared strong preferences for how growth should occur in Greeley:

Housing & Neighborhood Form:

- Single-family detached homes (47.6 percent) and townhomes (8.3 percent) were the most desired new housing types, followed by duplexes and fourplexes.
- Many respondents (28.3 percent) selected “other” often, noting a desire for more variety, affordability, and higher-quality design.
- A majority of respondents ranked it “important” or “very important” that new housing be located within walking distance of parks, shops, and schools.

Mobility & Transportation

- » Residents primarily drive, but many also reported carpooling or using public transit.
- » Despite the high reliance on cars, respondents expressed strong support for safer walking and biking connections, better access to trails, and enhanced multimodal travel options.

Growth, Identity, and Agriculture

- » Over half of respondents rated it “important” to celebrate the area’s agricultural roots and suggested that future development should incorporate a rural-to-urban transition character, natural features and open spaces, and design elements that reflect Greeley’s history.

Opportunities for Improvement

Residents identified several key areas where Western Greeley could improve:

- » More coordinated infrastructure and roadway design as growth continues.
- » Additional parks, green spaces, and recreational amenities in new neighborhoods.
- » Stronger identity and cohesion across Western Greeley.
- » Better connection to existing neighborhoods, UNC, and employment areas.

THE COMMUNITY'S TOP PRIORITIES

Inclusive and ongoing community engagement was a cornerstone of the Westside Area Plan process. From early coordination with residents, developers, and business owners to input-gathering through public meetings, online surveys, and stakeholder discussions, the Project Team worked to center community voices in formulating the Plan's vision and opportunities.

Engagement activities focused on understanding residents' experiences in Greeley's expanding western neighborhoods, including what they value most, the challenges they face, and the opportunities they see for the future. Feedback helped identify key themes around livability, access, infrastructure, and community identity, forming the foundation for the Plan's goals and implementation strategies.

The following priorities summarize what participants expressed as most important for supporting high-quality growth, connectivity, and placemaking across Western Greeley.

ACCESSIBILITY AND INCLUSIVITY

- Create neighborhoods that are safe, connected, and welcoming for residents of all ages and backgrounds.
- Expand access to community amenities, parks, trails, and gathering spaces that foster belonging and social connection.
- Encourage mixed-use and diverse housing types, in contrast to Greeley's predominant pattern of isolated single-income pods, to support residents across income levels and life stages.
- Celebrate Greeley's diversity through public art, events, and cultural programming that reflect local identity and pride.

PROGRAMMING AND ACTIVATION

- Support the creation of community spaces, such as plazas, greenways, and park nodes, that host recreation events and markets.
- Foster partnerships between schools, churches, and local organizations to activate public spaces and strengthen neighborhood cohesion.
- Continue to promote family-oriented activities and outdoor events that highlight the area's natural features and trail network.
- Explore opportunities for temporary or flexible uses in new subdivisions (food trucks, pop-up parks, or mobile farmers' markets).

STOREFRONT ECONOMY AND CHARACTER

- Support community interest in seeing existing strip-mall–style commercial centers along U.S. 34 and 10th Street evolve into more walkable, neighborhood-oriented places that are connected to nearby homes rather than auto-oriented corridors.
- Improve pedestrian comfort with more engaging storefronts and better integration with surrounding neighborhoods as a top priorities for the future of commercial and mixed-use areas.
- Encourage adaptive reuse of existing buildings and flexible retail formats that meet community needs.
- Promote a mix of neighborhood retail, dining, and services that reduce the need for long commutes.
- Incentivize small business ownership and local entrepreneurship, especially women and minority-owned ventures.
- Strengthen community identity through local design standards, signage, and consistent architectural character.

LIVABILITY

- Expand access to parks, trails, and open spaces within walking distance of every neighborhood.
- Promote diverse housing choices, like single-family, townhomes, and multi-family, to serve a range of incomes and lifestyles.
- Strengthen access to community services, schools, and recreation through coordinated planning.
- Encourage neighborhood amenities, such as corner cafes, plazas, and daycare centers, to foster social interaction.
- Support development patterns that balance growth alongside the preservation of Greeley’s agricultural roots and rural character.

INFRASTRUCTURE AND INFILL DEVELOPMENT

- Coordinate future growth with available water, wastewater, and transportation infrastructure capacity.
- Support infill development on underutilized parcels, ensuring new projects integrate with existing neighborhoods.
- Incorporate stormwater and natural drainage features into new neighborhoods to enhance sustainability.
- Prioritize infrastructure investments that improve service delivery and connectivity across Western Greeley.

CONNECTIVITY

- Improve regional connections along U.S. Highway 34, 10th Street, and local corridors linking Western Greeley to Downtown, Windsor, and Loveland.
- Expand the trail and bike network to connect new neighborhoods with parks, schools, and the Poudre River Trail.
- Integrate shaded sidewalks and pedestrian crossings into new development to promote walkability and comfort.
- Enhance wayfinding, signage, and multimodal options to foster safe, seamless mobility across neighborhoods.



04

PHYSICAL FRAMEWORK

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COMMUNITY DESIGN FRAMEWORK

THE NEIGHBORHOOD UNIT

Greeley is experienced—and remembered—at the neighborhood scale. From historic areas near Downtown to newer development in Western Greeley, neighborhoods shape daily life, travel patterns, and community identity. Planning at this scale matters because neighborhoods determine whether residents can meet everyday needs close to home or must rely on long car trips. Well-designed neighborhoods support social connection, walkability, and a human-scale environment.

In contrast, much of Western Greeley has developed as disconnected, single-use districts that prioritize vehicle movement over neighborhood life. Complete neighborhoods move beyond these isolated subdivisions and apartment complexes by offering a mix of housing, shops, workplaces, civic spaces, and services within a connected, walkable framework. While neighborhoods may vary in size, density, and housing mix, successful examples share common design principles that support accessibility, sustainability, and a strong sense of place.

Five design conventions provide a common framework for great neighborhoods.

1. IDENTIFIABLE CENTER AND EDGE

A well-formed neighborhood has a recognizable center and a clear sense of arrival. The center includes places where people naturally gather—such as a square, plaza, or a well-defined “four corners” intersection.

Effective centers are walkable from surrounding homes, include a mix of uses, and are framed by higher-density buildings scaled to pedestrians. Discernible centers matter because they support daily needs, create shared identity, and foster social interaction.

2. WALKABLE SIZE

Neighborhoods should be sized to support walking, typically between 40 and 200 acres. Most people are willing to walk about one-quarter mile before choosing to bike or drive—a pattern historically reflected in many Downtown Greeley neighborhoods, including Monroe, where centers are generally within this distance. Neighborhood-scale civic uses, such as elementary schools, parks, and playfields, reinforce walkability. Larger facilities serving multiple neighborhoods should still be located at neighborhood edges. Neighborhood centers should generally be no more than one-half mile apart.



Diagram of a complete neighborhood



Identifiable center and edge



Walkable neighborhood size

3. MIXED USES AND HOUSING TYPES

Great neighborhoods offer a fine-grained mix of uses and housing types that allow residents to live, work, shop, and socialize within walking distance. This approach contrasts with the single-use “pod” development common in Western Greeley.

Mixing uses helps reduce vehicle trips, while housing diversity supports inclusion by accommodating a range of incomes, household types, and life stages. Residents can remain in the same neighborhood as their needs change, and a variety of activities throughout the day contributes to safety and vitality.



Transect based planning allows a mix of land uses and housing types

4. INTEGRATED NETWORK OF WALKABLE STREETS

A connected street network enables pedestrians, cyclists, and motorists to move safely and comfortably. To achieve this, the average block perimeter should not exceed 1,500 feet; block faces should ideally be no longer than 450 feet, and streets should occur at intervals no greater than 600 feet.

A well-structured street network creates logical development sites, supports multiple modes of travel, and provides mobility for youth, older adults, and others who do not drive. Streets should be designed for walkability first—using slower vehicle speeds, narrow and two-way cross sections, street trees, on-street parking, buildings close to the street, four-way stop signs instead of signals, and tight corner radii. A connected web of streets disperses traffic and supports multiple travel routes.



Network of walkable streets

5. SPECIAL SITES FOR CIVIC USES

Complete neighborhoods reserve prominent locations for civic life. These sites gain importance through their placement within the neighborhood—at focal points, along terminated views, or at centers of activity—making them well-suited for parks, schools, civic buildings, and gathering spaces.

Each neighborhood should include at least one meaningful public place, such as a green, plaza, or square, with other open spaces serving distinct roles. Together, these civic sites anchor neighborhood identity and provide shared places for community life.



Special sites are reserved for civic purposes

UPDATING THE LAND DEVELOPMENT REGULATIONS: FORM-BASED CODES

DENSITY, DESIGN, AND LIVABILITY

In order to build more densely and efficiently while still maintaining a high quality of life, new zoning and land development tools are required. Density without design does not deliver the open spaces, variety, and walkability of historic neighborhoods and it is often rejected by communities concerned with maintaining community character and choice-worthy neighborhoods.

Thus, the Westside Area Plan recommends that the City adopt an optional Form-Based Code Overlay that would award developers greater density in exchange for better designed communities with walkable streets, central open spaces, a variety of housing types, and a mix of uses.

A foundational element of Form-Based Codes, transect diagrams, like the one below, depict a diversity of residential types, from most urban to least, transitioning from neighborhood center to edge. Along this continuum, courtyard apartment buildings and townhouses transition to small cottages, duplexes with edge yards, and single-family detached homes of increasing size and acreage, many with accessory dwelling units or granny flats.

The Transect Map below illustrates how the street network can be largely retained, while allowing the intensity of development to vary according to the allocation of transect zones throughout the town center.

The City should also continue to update its Land Development Regulations and Zoning to include Form-Based Code elements that are appropriate in any context, not just in emerging urban areas.



This specific rural-to-urban transect diagram is often used in Form-Based Codes. It was developed by Andrés Duany and his firm, Duany Plater-Zyberk & Company (DPZ), as the foundational theory for the SmartCode, the basis of modern Form-Based Codes.

TRANSECT ZONES ILLUSTRATED

T1: Natural

The natural transect is generally located outside the boundary of the neighborhood and consists of undeveloped lands, including lands unsuitable for settlement due to topography, hydrology, or vegetation. Trails and utilities may be situated in this transect.

T2: Open Space

Open space consists of sparsely settled lands in an open or cultivated state, like parks, squares, woodland, grasslands, trails, stormwater management features, and open space areas. Typical buildings are civic and recreational in nature.

T3: Edge

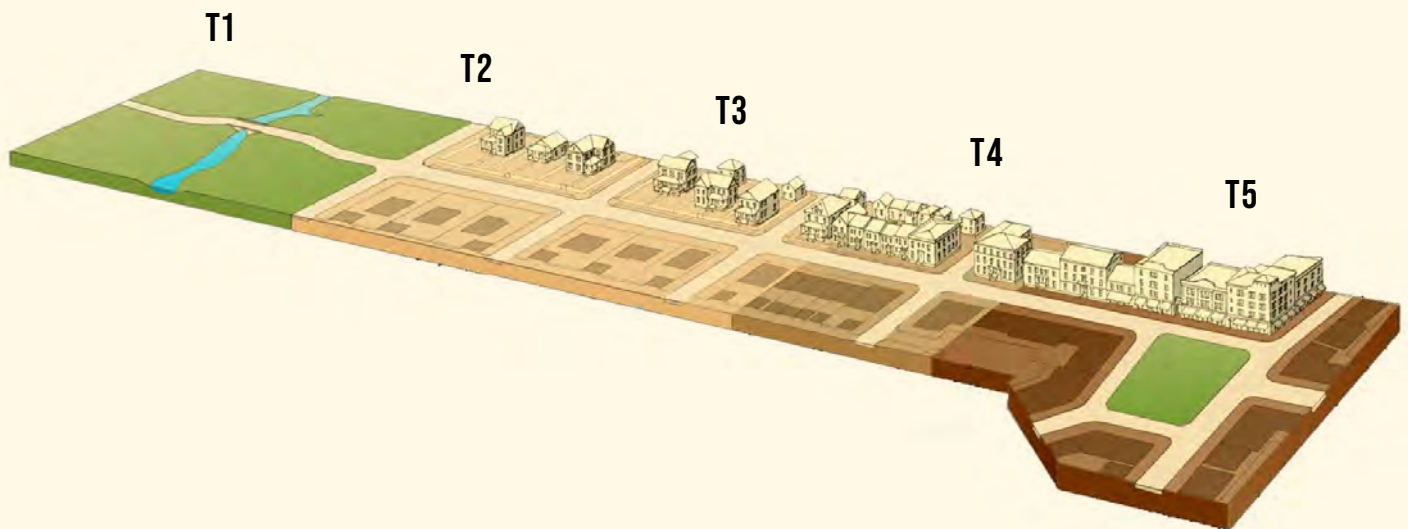
Edge consists of the primarily residential areas, but includes some mixed-uses, such as corner stores. Home occupations and accessory dwelling units are allowed. Plantings are naturalistic, and setbacks are set away from the public streets. Blocks may be large and the roads irregular to accommodate natural conditions.

T4: General

General maintains a residential village fabric coupled with mixed-use buildings. This transect may have a wide range of building types, such as single-family, cottages, townhomes, duplexes, and multi-family buildings. Setbacks are minimal, and landscaping varies. Streets typically define medium-sized blocks.

T5: Center

Center comprises the most walkable environment and has the greatest mix of uses, including retail, offices, and live/work units, and multi-family residential. T5 areas have fairly small blocks with wide sidewalks and regularly-spaced street planting. Buildings are taller, between two and four stories, with narrow or no setbacks from the right-of-way.



This graphic, created by illustrator James Dougherty of Dover, Kohl & Partners, shows the transect system using three-dimensional building types.

TEN STEPS FOR MAKING WALKABLE STREETS

Western Greeley has major and minor arterial roadways to accommodate through-moving traffic. In time, access to these highways will require a porous network of local streets, in contrast to a dendritic system of collector and local roads. Walkable streets offer a wide range of benefits for wealth, health, community, equity, and the environment.

1. DESIGN FOR PEDESTRIANS FIRST

Great streets are designed to provide a high-caliber experience for pedestrians foremost. Once this is accomplished, great streets generally accommodate a wide range of other modes of travel.

2. PROPORTIONS MATTER

A street or square is only as good as its edges or walls, and the public realm must be designed as a series of outdoor living rooms that offer a sense of enclosure. A 1:3 ratio for building height to street width is often cited as a minimum section for a sense of enclosure. Creating this sense of enclosure involves more than just narrow street width, however. There are well-defined eight-lane roads just as there are two-lane roads that seem to be impassable. Streets must be sized properly for their use and should be defined with appropriate building sites. Street trees and features such as lighting also play a critical role in defining the space of the street.

3. DESIGN THE STREET AS A UNIFIED WHOLE

An essential distinction of great streets is that the entire space is designed as an ensemble, from the travel lanes, trees, and sidewalks, to the very buildings that line the roadway. Building form and character are particularly important in shaping a sense of place. When streets consistently have buildings fronting them with appropriate height and massing, they create an adequate sense of enclosure.

The random edges generated by conventional zoning rarely produce this effect; form-based regulations must be put in place to control building form and placement. Furthermore, urban buildings must front the street with features such as doors, windows, balconies, and porches. These features promote a lively streetscape, and ultimately offer passive security for pedestrians by focusing “eyes on the street.”

4. INCLUDE SIDEWALKS

Appropriately designed sidewalks are essential for active pedestrian life. Pedestrians will be more willing to utilize sidewalks if they are protected from automobile traffic. One of the simplest ways to buffer the pedestrian is to place street trees between the street and the sidewalk; successful streets need trees, and lots of them. Beyond a tree zone adjacent to the curb—also ideal for outdoor dining and other street furniture, such as streetlights, bus shelters, and benches—urban sidewalks must maintain adequate clear zones for walking and rolling, and frontage zones for entering buildings, where book tables and clothing racks are welcome. New curb cuts across sidewalks must be made illegal, and, ideally, existing curb cuts are removed. The width of the sidewalk will vary according to the location. On most single-family residential streets, five or six feet is an appropriate width, but streets with townhouses and multi-family buildings require a more generous sidewalk. On Main Streets, sidewalk width must never fall below an absolute minimum of eight feet, and is ideally a minimum of 14 feet.

5. PROVIDE SHADE

Motorists, pedestrians, and cyclists typically prefer shady streets. Shade protects people from heat and sun and contributes to the spatial definition of a street. Shade can be provided with canopy trees or architectural encroachments over the sidewalk. Canopy trees should be planted in a planting zone between the sidewalk and the street to continuously define and shade the street and the sidewalk. Architectural encroachments over the sidewalk, such as awnings, arcades, and cantilevered balconies, are another way to protect pedestrians from the elements and shield storefronts from glare.

“It is not surprising that, given their multiple roles in urban life, streets require and use vast amounts of land. In the United States, from 25 to 35 percent of a city’s developed land is likely to be in public rights-of-way, mostly streets. If we can develop and design streets so that they are wonderful, fulfilling places to be, community building places, attractive public places for all people of cities and neighborhoods, then we will have accomplished a great deal for the city directly and will have an immense impact on the rest.”

— Allan Jacobs, *Great Streets*

6. MAKE MEDIANS SUFFICIENTLY WIDE

Where divided thoroughfares are unavoidable, medians must be generous enough to serve as a pedestrian amenity. A minimum median width of 8' will accommodate a row of street trees, which are necessary to distinguish streets from highways.

7. PLANT STREET TREES IN AN ORDERLY MANNER

Great streets are typically planted with rows of regularly-spaced trees, using consistent species. The proper spacing for most urban trees is 30 feet on-center. Narrower species chosen due to a tight fit can be planted as closely as 20 feet on-center, budget permitting. One tree per parking space is a nice solution along a main street. Spacing should be as consistent as possible to create a legible rhythm.

8. USE SMART LIGHTING

Streets should be appropriately lit for automobile and pedestrian safety. Widely-spaced, highway-scaled “cobra head” light fixtures do not provide enough light intensity and consistency for pedestrian well-being. More frequently-spaced, shorter fixtures are more appropriate and offer light beneath the tree canopy as street trees mature.

9. ALLOW ON-STREET PARKING IN SUITABLE LOCATIONS

On-street parking is an essential barrier of steel that protects the pedestrian from moving vehicles. Parallel parking is the ideal arrangement, because it keeps streets as narrow as possible. Diagonal parking is acceptable on some shopping streets, as long as the extra curb-to-curb width is not achieved at the expense of sidewalk width. Parking located in front of a street of businesses encourages people to get out of their cars and walk and is necessary to leasing street-oriented retail space.

10. AVOID PARKING LOTS IN FRONT OF BUILDINGS

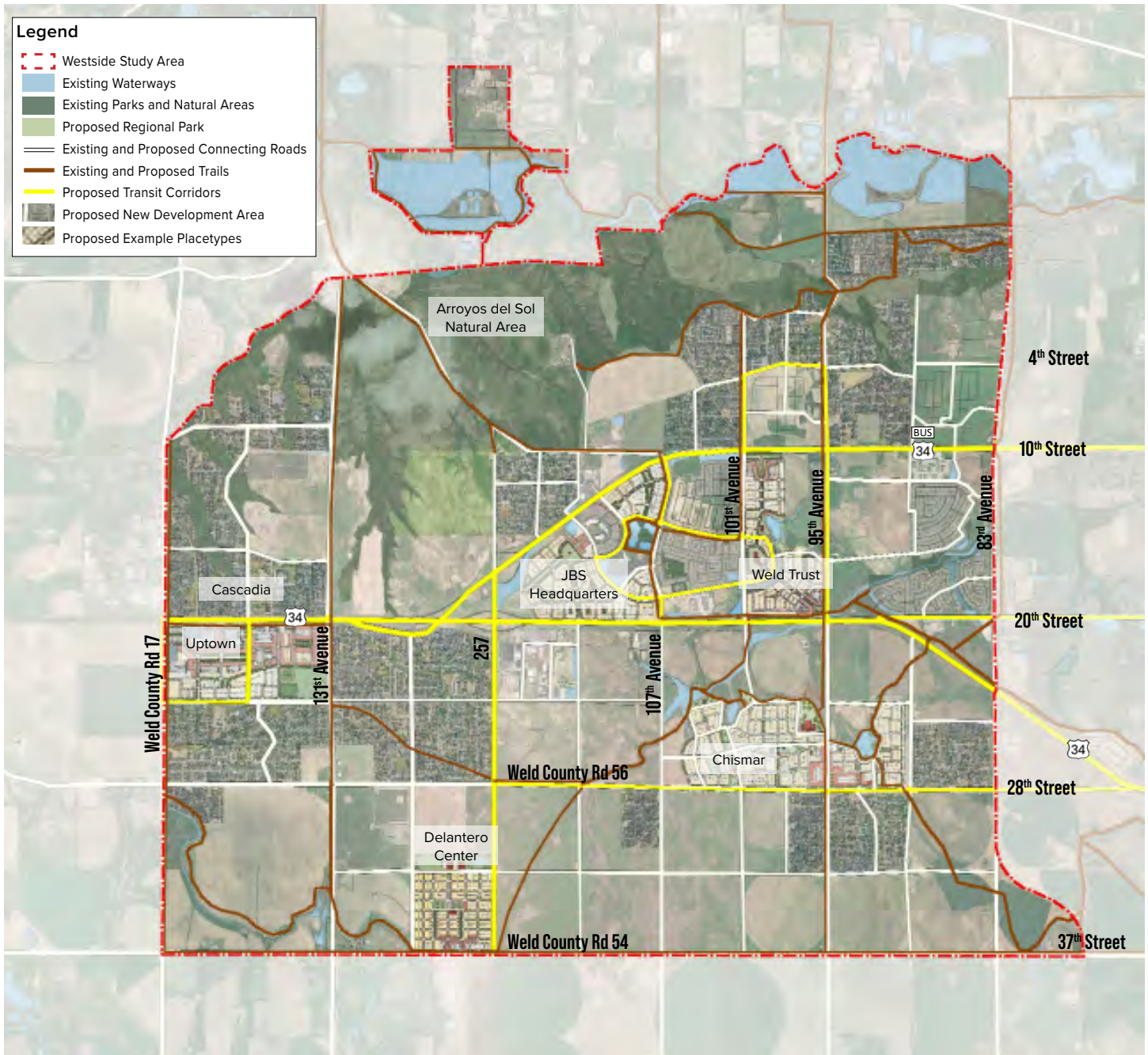
The bulk of a building’s parking supply should occur behind the building. The conventional practice of placing surface lots in front of buildings results in a disconnected pedestrian environment. If current zoning regulations are reformed to provide “build-to” lines rather than mandatory setbacks for commercial buildings, parking can be accommodated in the interior of the block. As a result, the pedestrian realm of the sidewalk will be defined by shop fronts and building entrances rather than parking lots.



Walkable streets around the United States are built with specific design features that are duplicable in Western Greeley.

WESTSIDE ILLUSTRATIVE PLAN

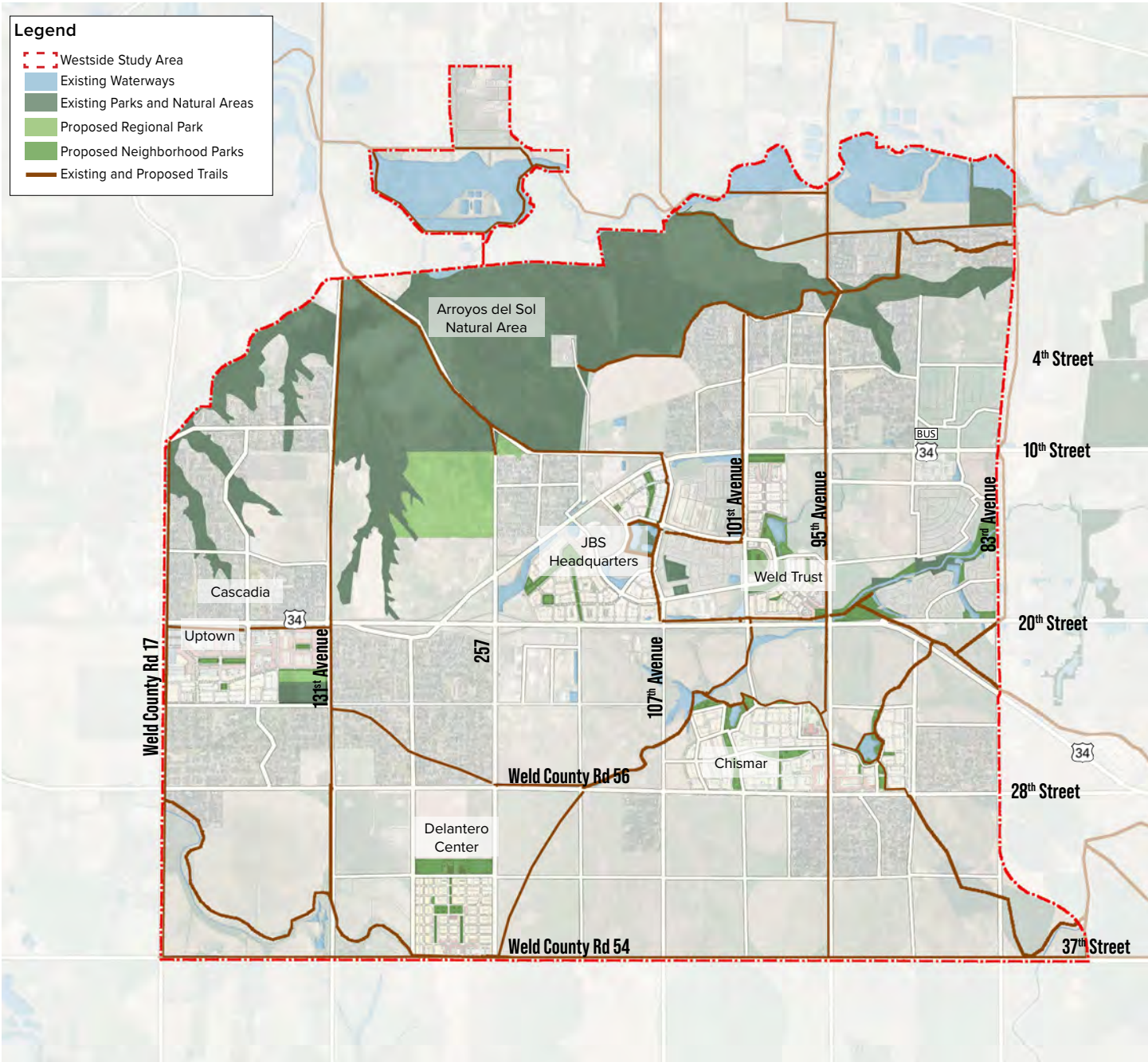
The Westside Area Plan illustrative plan depicts one possible “build-out” scenario as seen from an aerial view. This is a long-term vision of the year 2055 (approximately), based on the Development Capacity Analysis in Chapter 2. Real estate development occurs concurrent with land conservation and the development of adequate public infrastructure, recreational trail systems and parks, and public transit. Land is developed as complete, compact, and connected places, parks and trails are a short walk away for all residents, and Bus Rapid Transit (BRT) serves the area and connects it with the broader region.



PLAN DIAGRAMS

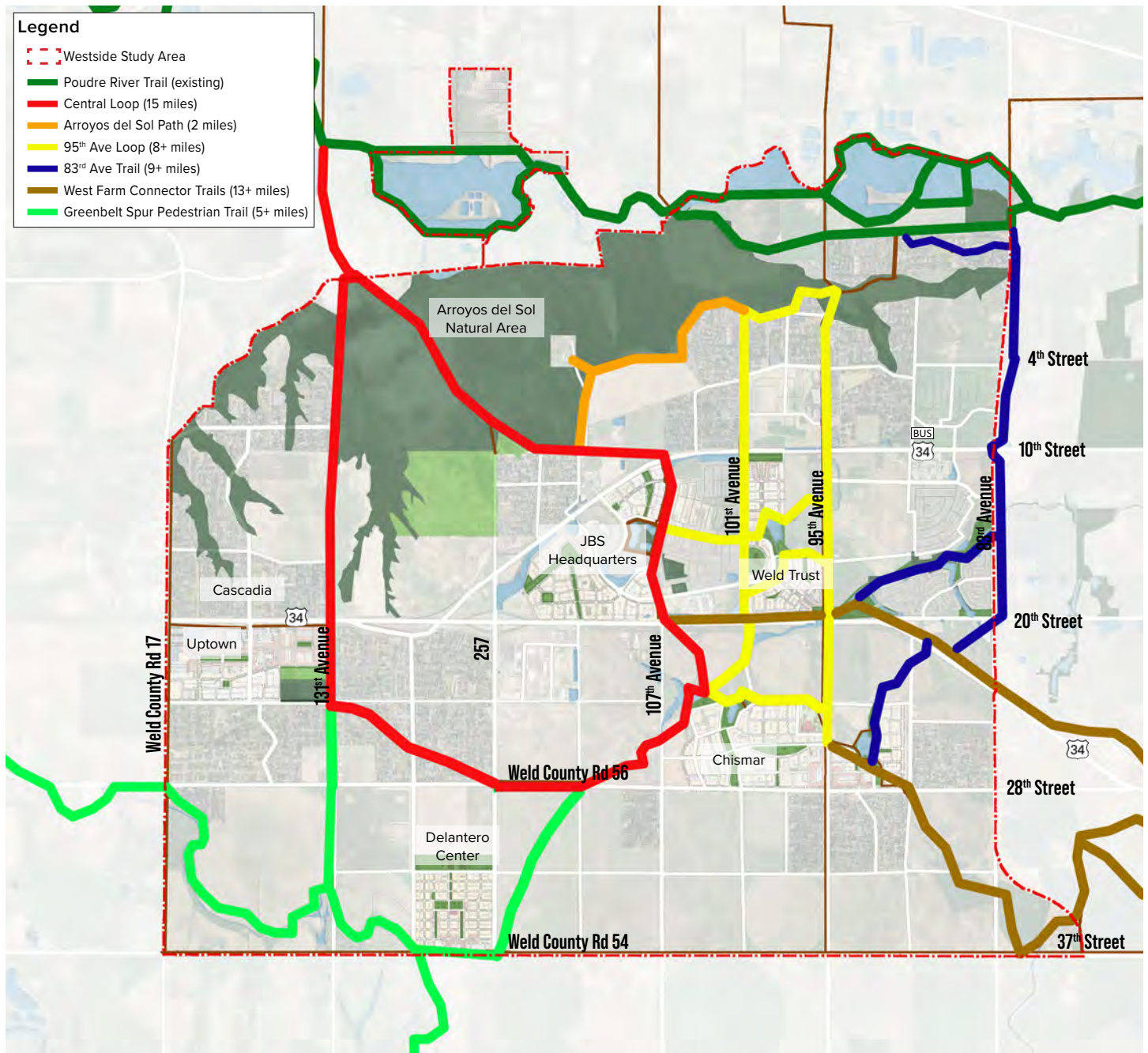
PARKS AND TRAILS

Open space in the form of protected lands, regional parks, and neighborhood parks are connected by a multi-modal trail system. Arroyos del Sol is a new natural area that is roughly 1,000 acres in size and provides habitat for a diversity of wildlife. A new regional park fulfills a critical community need and offers a central hub for active recreation. Playgrounds and small greens provide neighborhood parks.



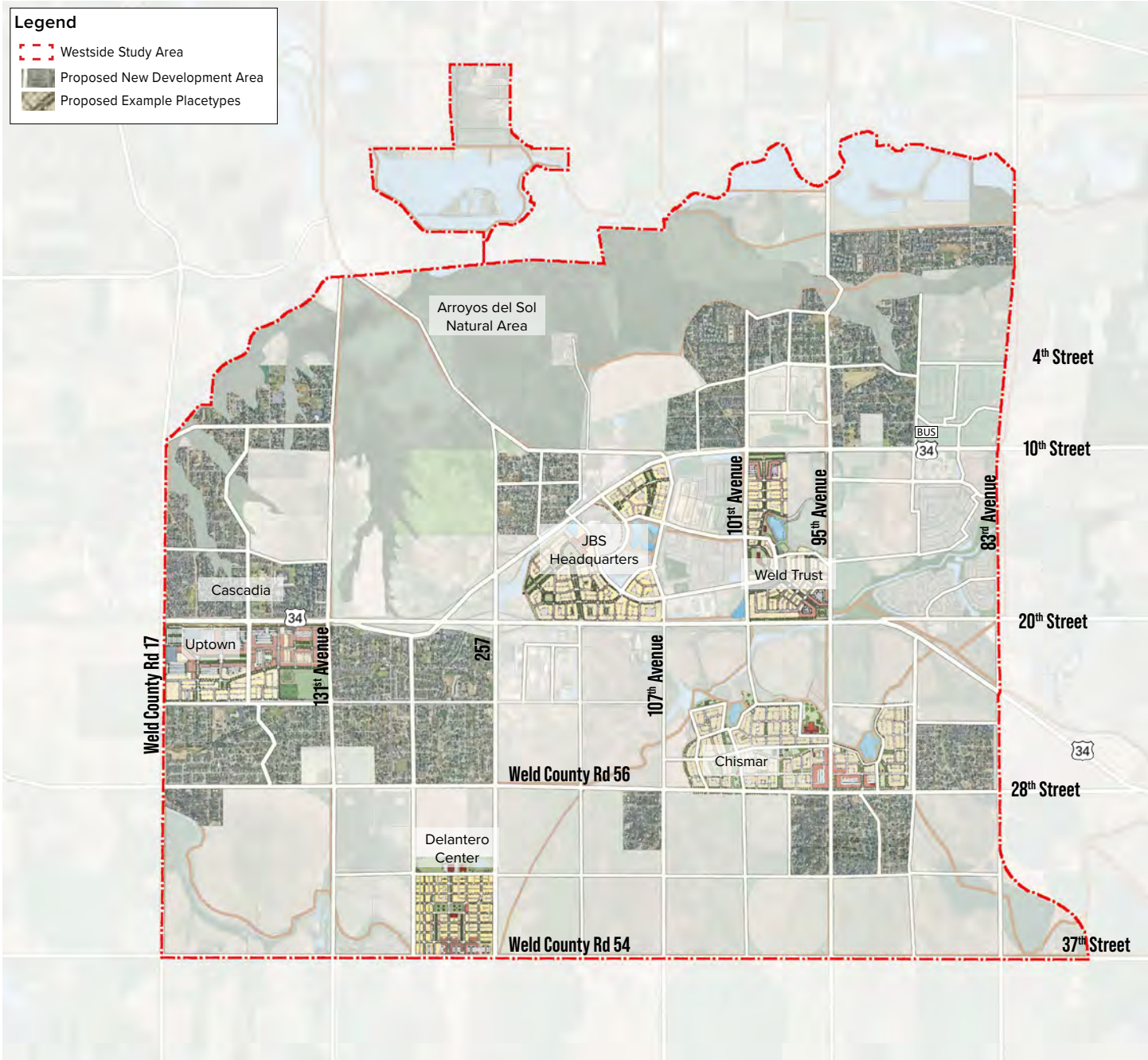
TRAIL LOOPS

Building on the City’s 2025 Trails Master Plan, a coherent system of mapped trails with loops and spurs is envisioned. Trail segments can be required to be dedicated as part of the subdivision process. Each loop connects the neighborhood centers with prairies, farms, bluffs, and arroyos. Long-distance connections span from Downtown Greeley across Western Greeley and into neighboring communities.



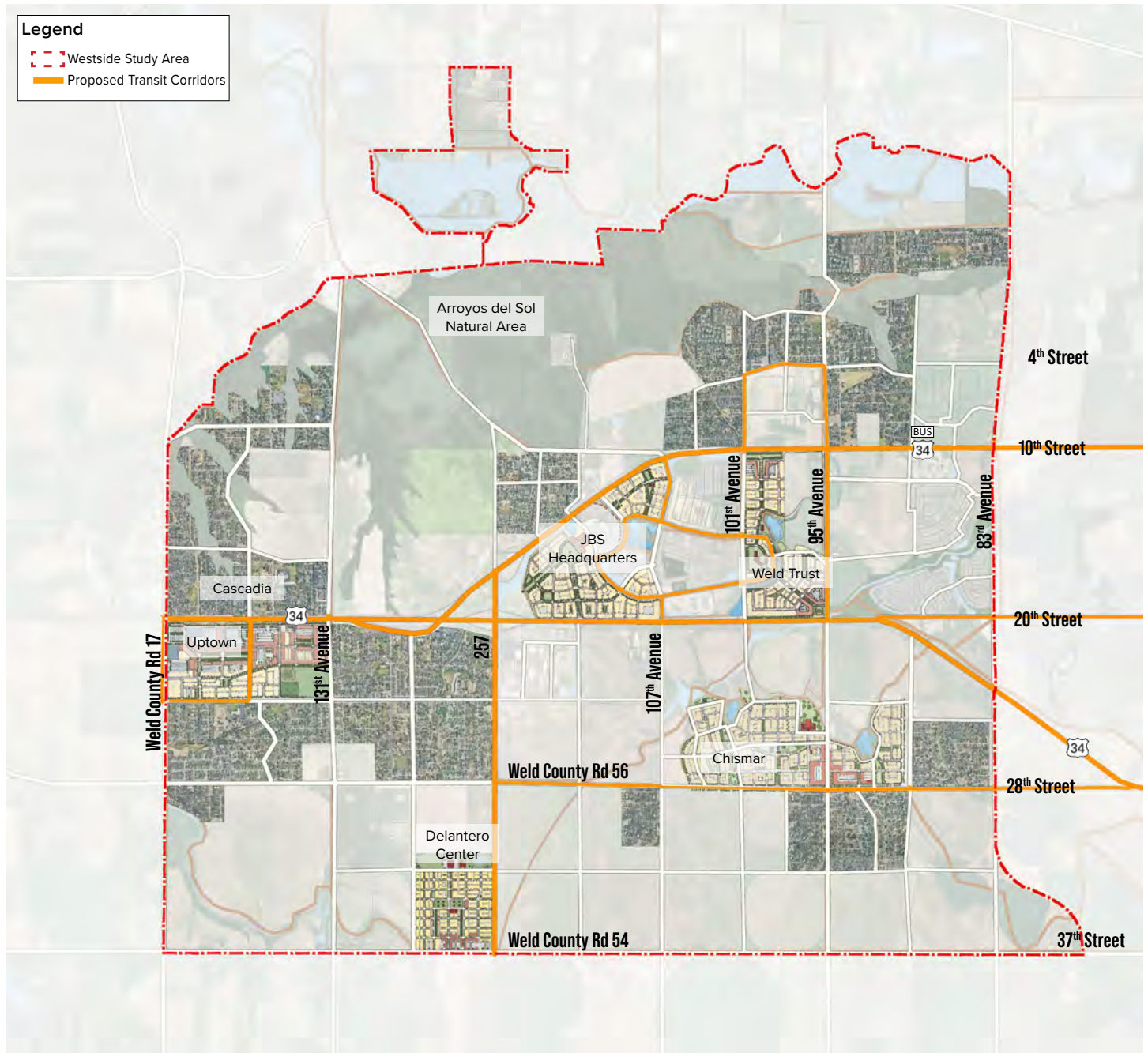
NEW DEVELOPMENT

Western Greeley is home to several parcels with development that is completed, underway, being considered, or likely given their location and zoning. The specific illustrative master plans described ahead in this Chapter are visible in this diagram. Other sites are depicted more generally with a network of walkable blocks.



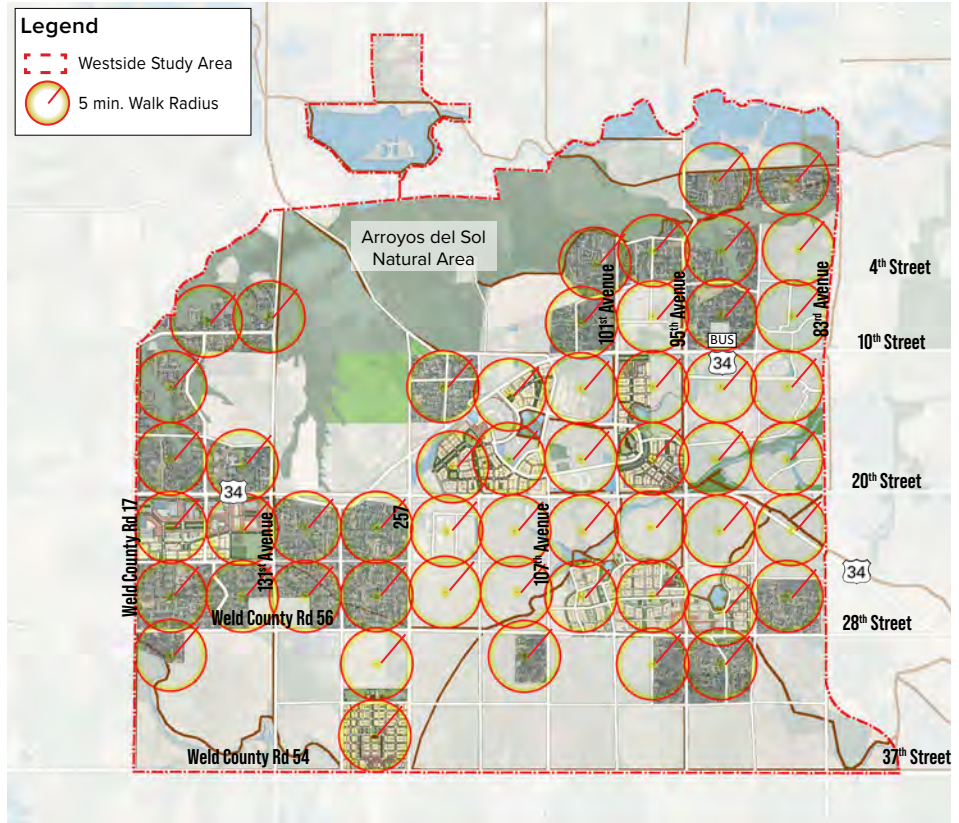
TRANSIT PLAN

After identifying the likely centers for new development, a transit network is imagined to service each center. By providing residents and visitors with an alternative to single-occupancy car travel, the Plan seeks to prevent roadway congestion at full build-out. Transit can take several forms, including fixed-route bus service, on-demand shuttles, and potential future bus rapid transit corridors. Each mode should be evaluated for its ability to connect key activity hubs, support anticipated population growth, and operate efficiently within existing rights-of-way.



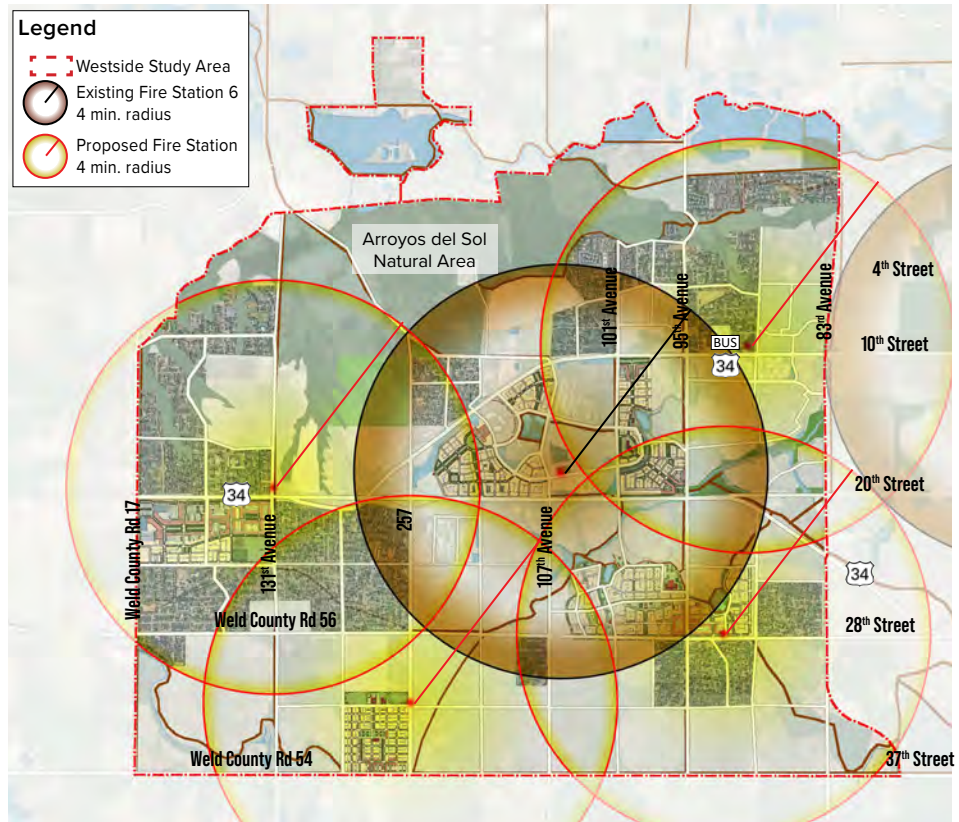
PEDESTRIAN SHEDS

Neighborhoods with clear centers and edges—and where the distance from center to edge is about a five-minute walk—form the basic building blocks of successful, desirable regions. Great cities are a collection of great neighborhoods. Great neighborhoods are complete with a mix of uses, compact with sustainable densities, and connected to each other by trails and multi-modal streets.





FIRE STATIONS

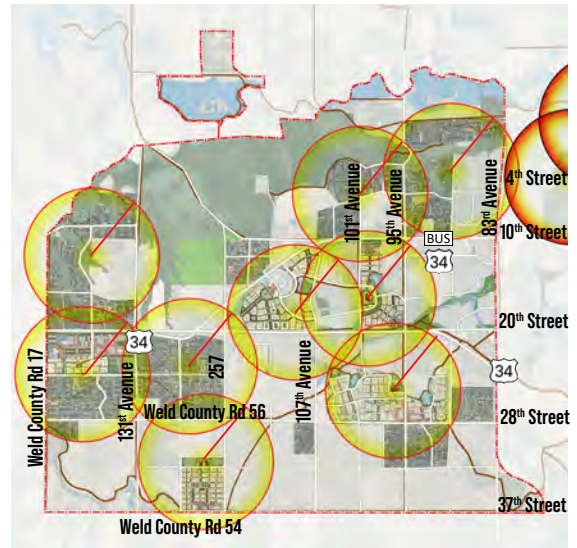
Fire stations typically service an area with a 1.5-mile radius, translating to an approximate four-minute response time. Fire Station 6, located in Western Greeley, currently provides service to the area. A first new fire station could be located within the Cascadia development, while additional stations should be considered as development progresses throughout Western Greeley. As build-out continues, there may be a need for up to four additional fire stations, with some overlapping coverage to ensure response times remain within recommended standards.



ELEMENTARY SCHOOLS



The typical service area for an elementary school has a radius of half a mile to a mile in walkable neighborhoods. Western Greeley would need approximately nine new elementary schools to accommodate full build-out.

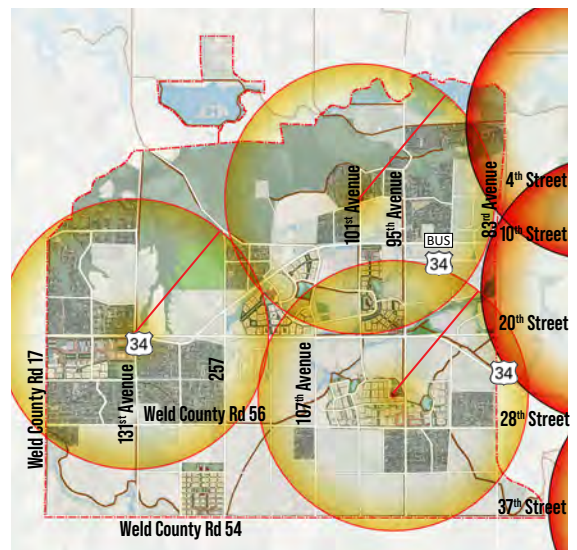
-  Existing Elementary School with typical 1.5 mile radius service area
-  Proposed Elementary School with typical 1.5 mile radius service area



MIDDLE SCHOOLS



The typical service area for a middle school has a radius of one-and-a-half miles to two miles, with students getting to school with a mix of walking and busing. Western Greeley would require two to three new middle schools.

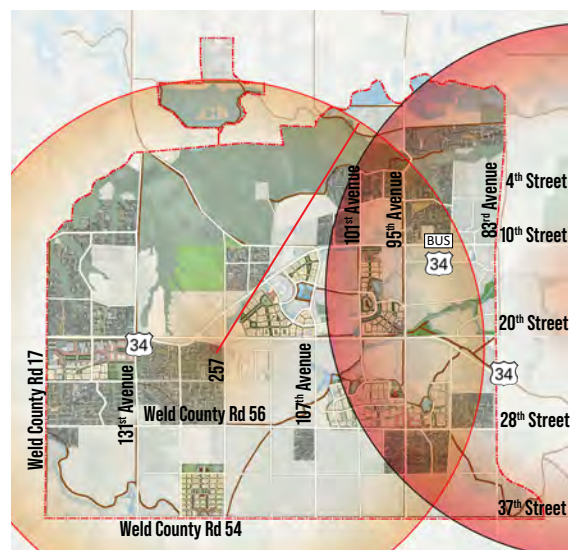
-  Existing Middle School with typical 2 mile radius service area
-  Proposed Middle School with typical 2 mile radius service area



HIGH SCHOOLS

The typical service area for a high school has a radius of three to five miles with nearly all students driving or being bused. These facilities tend to be large to accommodate larger populations of students but also to be able to offer a wider variety of subjects. There is an existing high school that could accommodate a portion of the students in Western Greeley, but a new high school would be needed with the onset of new development.

-  Existing High School with typical 5 mile radius service area
-  Proposed High School with typical 5 mile radius service area



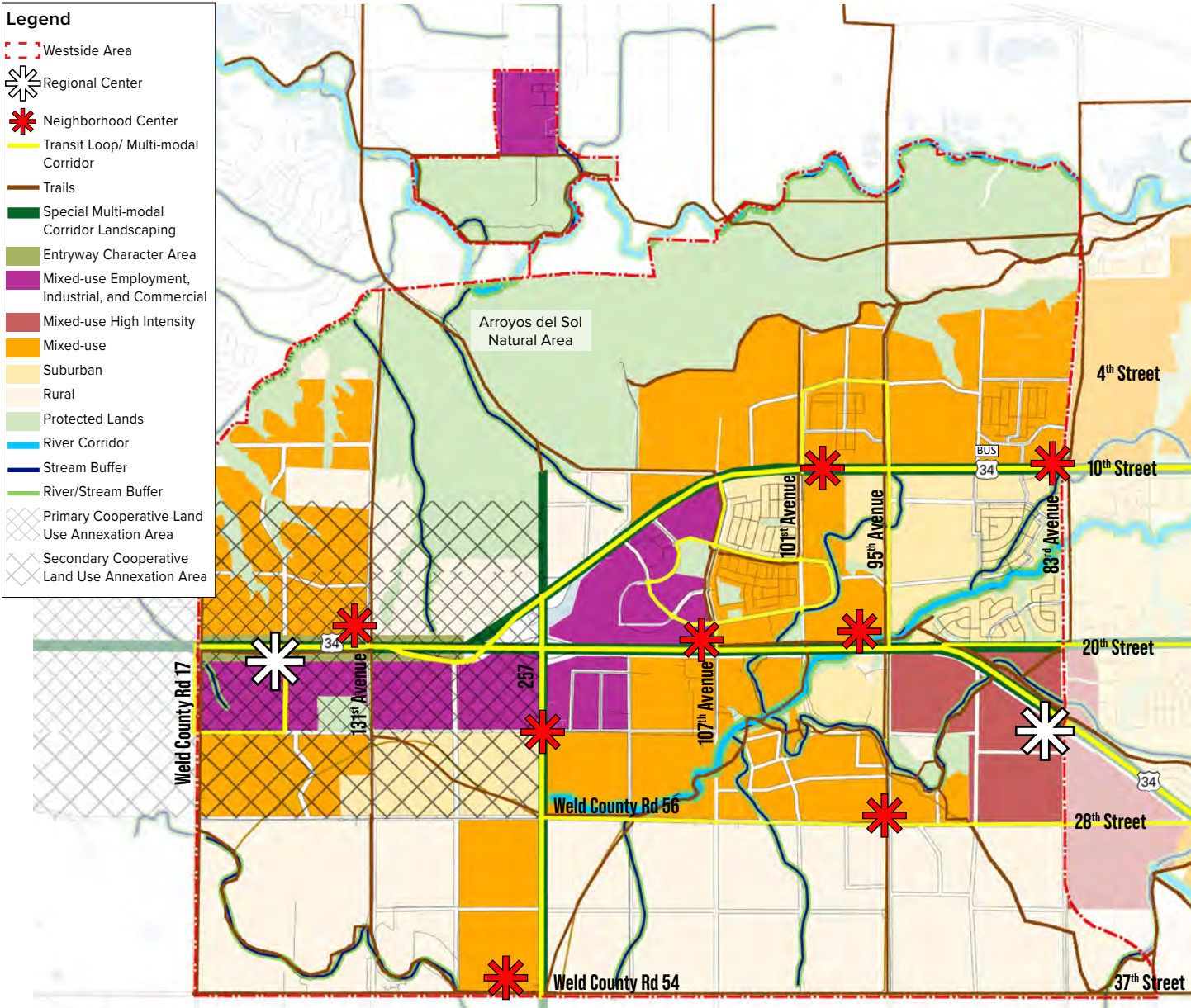
WESTSIDE LAND USE GUIDANCE MAP

The Land Use Guidance Map for Western Greeley provides a forward-looking framework to guide growth across the City’s rapidly expanding west side. As the area transitions from agricultural land to new neighborhoods, employment centers, and mixed-use destinations, updated land use direction is essential to ensure development supports Greeley’s goals for livability, sustainability, and coordinated urban form.

This update to Imagine Greeley’s Land Use Guidance Map refines the long-range land use vision by identifying the appropriate mix, intensity, and placement of residential, commercial, civic, and open space areas.

It also responds to evolving development patterns and community priorities unique to Western Greeley.

To support clear, predictable outcomes, the Land Use Guidance Map directly informs the Placetypes Map used in the optional Westside Form-Based Code (FBC) Overlay. Each placetype translates the land use vision into a physical development pattern—defining typical building forms, street relationships, public-realm expectations, and transitions between districts—to create a coordinated framework for high-quality, walkable growth.

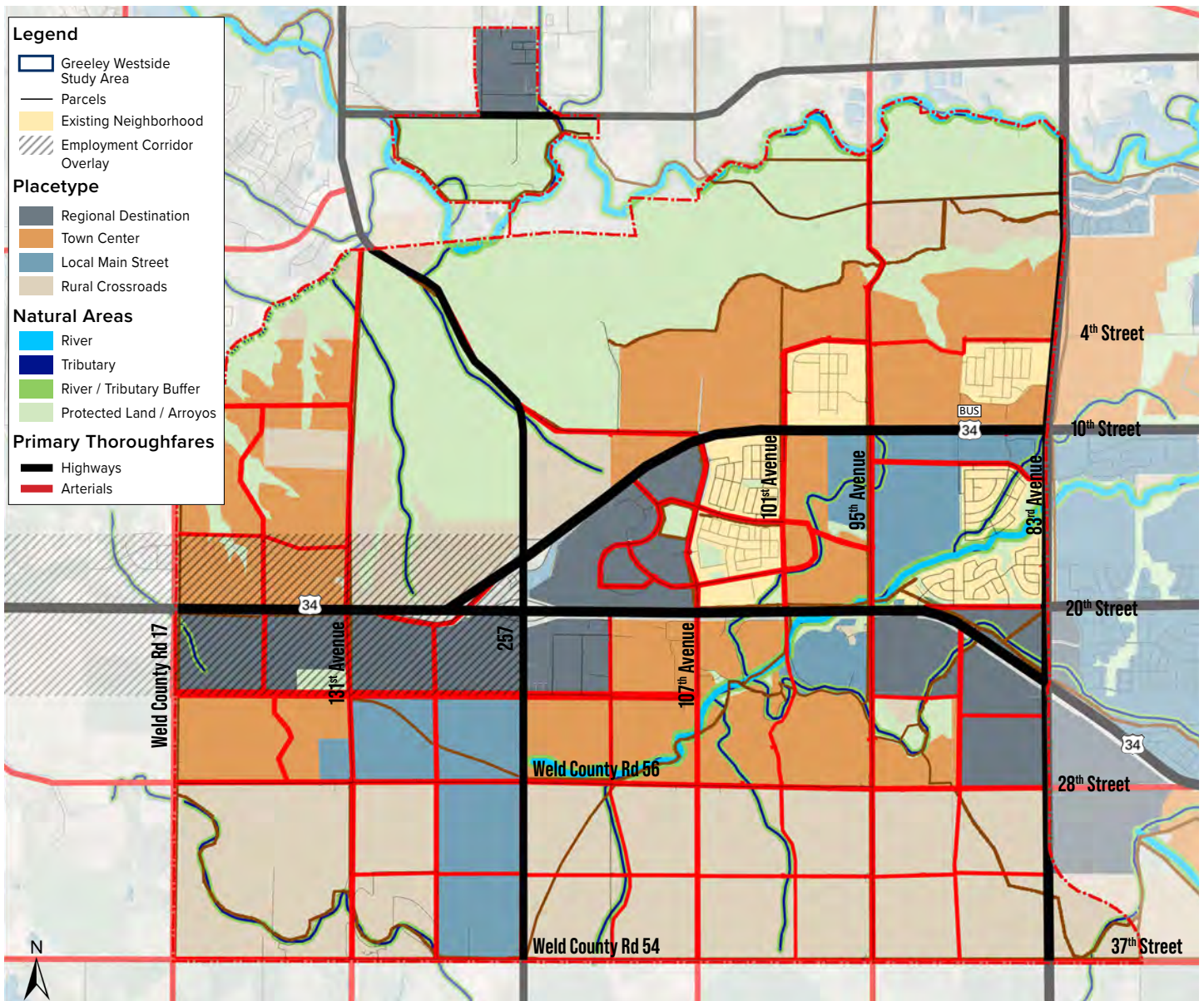


PLACETYPES

The Imagine Greeley Comprehensive Plan describes the use of Regional Centers and Neighborhood Centers to guide the development of compact, mixed-use development. Centers are concentrated nodes of activity within the City. Generally, all centers should be designed to be cohesive and walkable places, emphasizing pedestrian and bicycle connectivity both within the center and between the center and adjacent neighborhood or areas.

This Plan elaborates further on the concept of centers by defining the multiple types of Regional Centers and Neighborhood Centers.

These types are called Placetypes and they include two kinds of Regional Centers (RC1: Regional Destination and RC2: Town Center) and two types of Neighborhood Centers (NC1: Local Main Street and NC2: Rural Crossroads). The following pages describe how these Centers compare to strip-style commercial, the default Placetype for development. The Placetype Plan (below) identifies appropriate Placetypes, considering existing infrastructure, like roadways, utilities, and services, and how the City plans to expand infrastructure in the future. The Placetypes Plan is part of the optional Greeley Westside Form-Based Code Overlay.





**RC1:
REGIONAL DESTINATION**



**RC2:
TOWN CENTER**



**NC1:
LOCAL MAIN STREET**



**NC2:
RURAL CROSSROADS**



INTENSITY

STRIP-STYLE COMMERCIAL

(CURRENT DEFAULT PLACETYPE FOR DEVELOPMENT)



Strip-Style Commercial is not a deliberate Placetype but rather an urban form that results from insufficient urban planning. It refers to a type of retail development characterized by a linear layout of businesses, typically arranged along a single road. Cars are the defining feature of Strip-Style Commercial areas, which typically locate surface parking lots in front of businesses. These spaces cater to convenience-oriented shopping, service-oriented businesses, chain stores, and restaurants. Residential units may be close by, but few people walk due to a lack of sidewalks and overall poor connectivity.

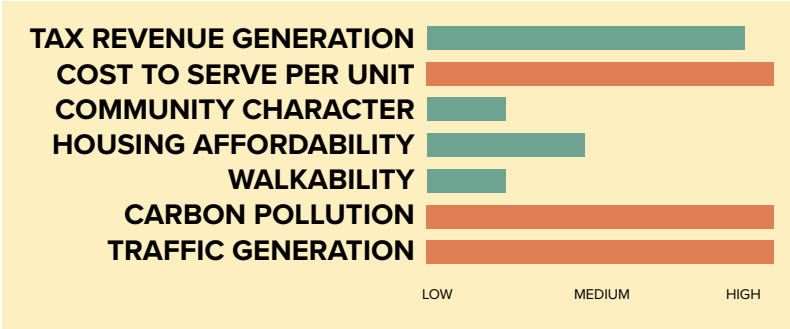
METRICS

DESIGN

- 3 to 7 units per acre
- 10 to 30 intersections per square mile
- No walkable blocks
- Negligible open space
- Few buildings over one story at main intersection

USES

- 350K to 650K SF leasable commercial
- 350 to 850 residential units
- Few residential units in walking distance



Note: In this Plan, “units per acre” is used as a generalized measure of development intensity. Depending on the placetype, units may represent residential dwellings, commercial buildings, or tenant spaces rather than housing alone.

TAX REVENUE GENERATION

Tax revenue refers to the money collected by local governments from property taxes, sales taxes, and business taxes, helping fund public services.

- Commercial businesses, including big-box retailers and chain stores, generate high revenue for the community.
- Strip-style businesses contribute to the local tax base through property taxes, sales taxes, and business-related fees.

COST TO SERVE PER UNIT

Cost to serve per unit represents the cost a municipality incurs to provide services (i.e., roads, utilities, waste management) to a given development. Lower-density, spread out developments often cost more per unit, as they require more infrastructure.

- Strip-style development generally leads to higher municipal costs for roads, utilities, and emergency services.
- The inefficient use of land in these areas further increases infrastructure expenses.

COMMUNITY CHARACTER

Community character reflects the unique identity, aesthetic, and cultural feel of a place, including architectural styles, walkability, public spaces, and how people interact with their surroundings.

- Strip-style commercial spaces often lack a strong identity and offer limited pedestrian-friendly environments.
- Their car-oriented design creates a disconnect from surrounding neighborhoods.
- These areas frequently lack public gathering spaces or the local charm that helps integrate a community.

CARBON POLLUTION

Carbon pollution indicates emissions of carbon dioxide and other greenhouse gas emissions from sources like cars, industries, and buildings.

- High levels of car dependency in strip-style commercial areas contribute to congestion and elevated emissions.
- Lacking public transit options in these environments further increase pollution.

HOUSING AFFORDABILITY

Housing affordability measures whether people can find reasonably priced homes relative to their income. True affordability also depends on access to daily needs without requiring car ownership. Mixed-use, higher-density development supports more affordable housing options by reducing transportation expenses and increasing access to services.

- Housing affordability is scarce along strip-style commercial corridors because these areas prioritize retail and commercial uses over residential integration.
- Auto-oriented development patterns isolate residents and often require car ownership, increasing household costs and undermining true housing affordability.
- The lack of mixed-use zoning and transit-supportive development further limits opportunities for affordable, accessible housing.

WALKABILITY

Walkability describes how easy and safe it is to get around an area on foot. It depends on factors such as sidewalks, street design, proximity of services, and pedestrian-friendly infrastructure.

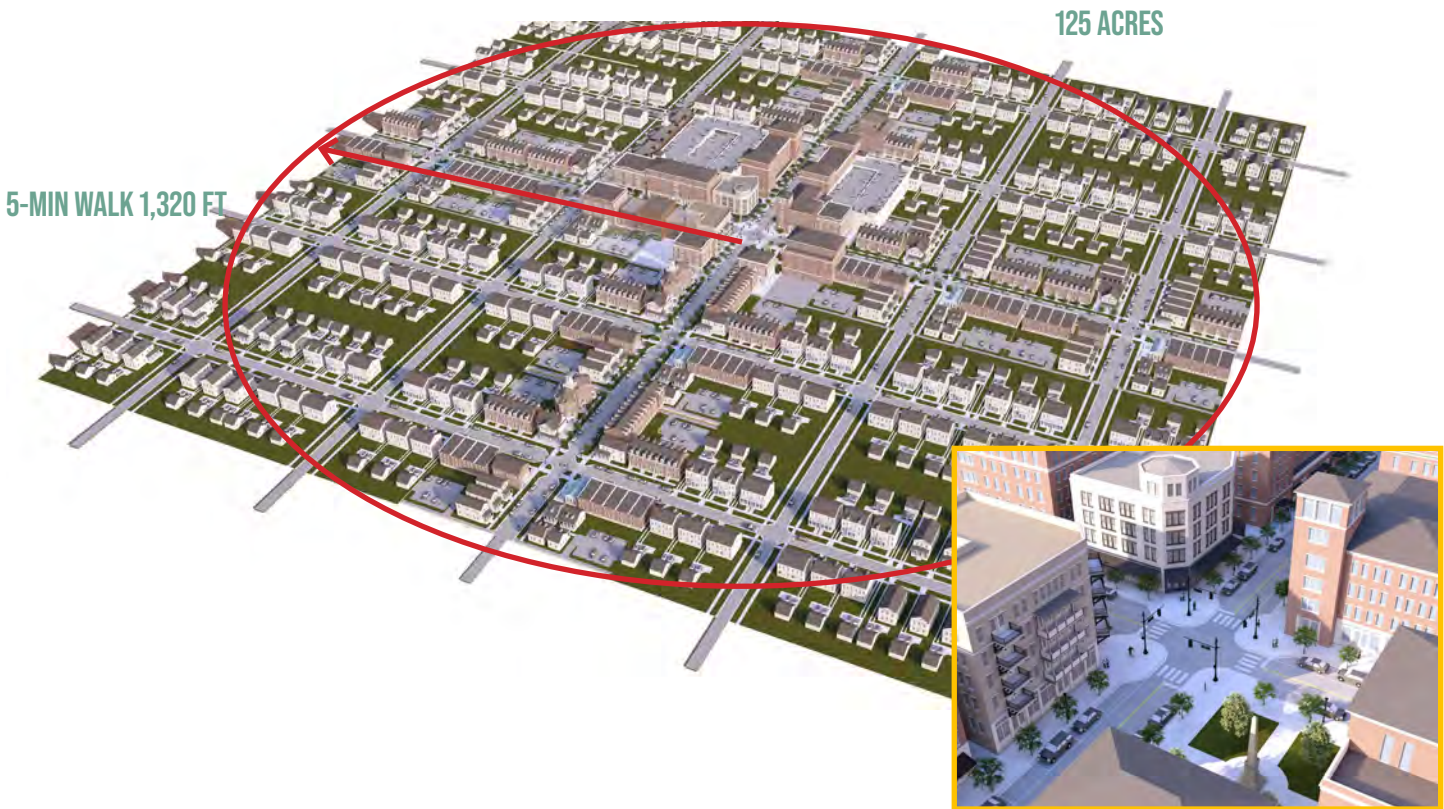
- Strip-style commercial development typically features poor pedestrian infrastructure and weak connectivity.
- Large parking lots and high-traffic roadways limit safe and comfortable foot travel.

TRAFFIC GENERATION

Traffic generation is the amount of vehicle movement caused by a development. Areas with high car dependency, poor public transit, and spread-out infrastructure lead to more congestion, longer travel times, and increased air pollution.

- High car dependency in these corridors contributes substantially to traffic congestion.
- The concentration of businesses along a single roadway, combined with multiple driveways, increases vehicle trips and intensifies congestion.

RC1: REGIONAL DESTINATION



Regional Destinations are a type of Regional Center. They are centers for state and local government, commerce, and culture, and they serve the city and region with a diversity of land uses, including homes, workplaces, universities, retail establishments, public facilities, entertainment venues, and medical centers. Blocks are generally organized as squares or rectangles to maximize the number of addresses and create a walkable network of streets with a variety of housing types to accommodate a range of people. The costs to provide services and infrastructure are reduced by virtue of building more densely. Regional Centers are typically serviced by public transit.

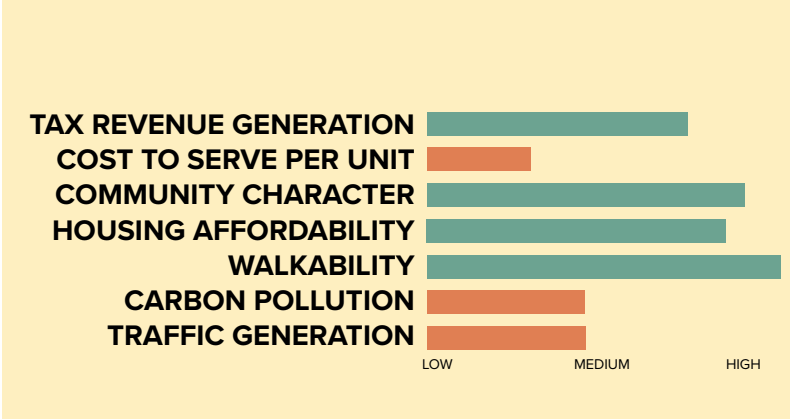
METRICS

DESIGN

- 30 to 45 units per acre
- 100 to 150 intersections per square mile
- 2,000 ft max block perimeters
- 10-20% protected open space
- 4 to 5 story mixed-use buildings at main intersection

USES

- 250K to 550K SF leasable commercial
- 1,300 to 1,500 residential units
- All residential units in walking distance



TAX REVENUE GENERATION

- More businesses and housing units yield more property and sales tax income.

COST TO SERVE PER UNIT

- While the cost to provide services (roads, utilities, emergency services) is higher than in low-density areas, the cost per unit is lower than in sprawling developments.

COMMUNITY CHARACTER

- Vibrant mixed-use environments tout walkable streets, diverse architecture, and engaging public spaces. The variety of businesses and residential options enhance local identity.

HOUSING AFFORDABILITY

- The presence of multi-story, mixed-use buildings allows for more housing units per acre. More attached housing leads to better affordability compared to single-use, low-density developments.

CARBON POLLUTION

- Higher walkability, mixed-used zoning, and access to public transit reduce reliance on cars, leading to lower carbon emissions than in car-dependent environments.

WALKABILITY

- Small block sizes, high intersection density, and mixed land uses maximize walkability. Residents can access jobs, shops, and entertainment within a short walking distance.

TRAFFIC GENERATION

- Since people can walk, bike, or use public transit for many daily trips, car dependency is lessened, mitigating the congestion that occurs in strip-mall or suburban-style development. This leads to less congestion.

LIKELY USES & ELEMENTS

- Apartments
- Artist collectives
- Bakery
- Banks
- Bars
- Breweries
- Cafes
- Cinemas
- Corporate offices
- Dry cleaning
- Government centers
- Gym
- Hair salons
- Luxury residential
- Medical offices
- Museums
- Multiple parking garages
- Multimodal streets
- Non-profit workplaces
- Pharmacies
- Performing arts centers
- Regional recreation facility
- Regional shopping
- Restaurants
- Streetcars
- Trails & street trees
- Townhomes
- University or college

RC2: TOWN CENTER



Town Centers are a type of Regional Center. Smaller than RC1: Regional Destinations, these mixed-use developments are designed to create vibrant, nearly-self-contained communities with a combination of residential, retail, and commercial spaces. Town Centers are located at the intersection of two major streets that transition from four travel lanes to two lanes with on-street parking to calm traffic and foster a safe pedestrian and cycling environment. Parking is located behind buildings that face complete streets. Town Centers provide a focal point for the community, offering convenient access to housing, workplaces, services, and public amenities.

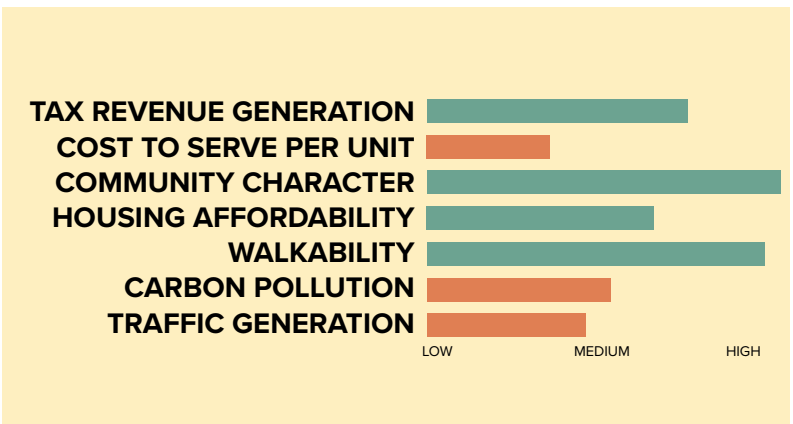
METRICS

DESIGN

- 15 to 30 units per acre
- 75 to 150 intersections per square mile
- 2,500 ft max block perimeters
- 20% protected open space
- 2 to 4 story mixed-use buildings at main intersection

USES

- 200K to 450K SF leasable commercial
- 1,000 to 1,200 residential units
- All residential units in walking distance



TAX REVENUE GENERATION

- A mix of residential, retail, and office uses broadens the tax base, generating revenue from property taxes, sales taxes, and business activity within a compact area.
- Higher land value and development intensity result in greater tax revenue per acre compared to single-use or strip commercial development.
- Local-serving businesses and year-round activity stabilize revenue streams and reduce reliance on large, single-source tax generators.

COST TO SERVE PER UNIT

- Infrastructure costs are lower than in suburban developments because roads, utilities, and emergency services cover a compact, walkable area.
- Public services, such as schools, transit, and maintenance, operate more efficiently due to the medium density and mix of commercial spaces.

COMMUNITY CHARACTER

- Walkable streets, public spaces, and locally owned businesses create a strong sense of place and identity.
- Human-scaled architecture, tree-lined streets, and civic spaces—such as plazas and markets—enhance aesthetic appeal and strengthen social cohesion.
- Unlike strip malls, town centers prioritize pedestrians over cars, reinforcing human-scale design and social interaction.

WALKABILITY

- Short, interconnected streets support walking and biking.
- Retail and residential areas are located close to each other, minimizing the need for car travel.
- Sidewalks, pedestrian crossings, and public gathering spaces create a safe, inviting environment for foot traffic.

HOUSING AFFORDABILITY

- A range of housing types, such as townhomes, small apartment buildings, and single-family homes, can accommodate an income-diverse population.
- Compared to downtowns, land and construction costs are generally lower, making housing more affordable on a per-unit basis.
- Proximity to local businesses and transit reduces transportation costs for residents, improving overall affordability.

CARBON POLLUTION

- Fewer car-dependent trips result in lower emissions than those produced by strip mall development.
- Mixed-use design helps eliminate the need for long commutes, thereby lowering the overall carbon footprint.
- Although some vehicle emissions persist, they are significantly lower than emissions in auto-oriented suburban areas.

TRAFFIC GENERATION

- More local trips are completed by walking or biking, which lessens overall car traffic.
- Traffic congestion is lower than in suburban strip mall areas, though town centers still accommodate some level of car use.
- Public transit access and mixed-use zoning help reduce vehicle miles traveled.

LIKELY USES & ELEMENTS

- Apartments
- Artist studios
- Bakery
- Banks
- Breweries
- Cafes
- Cinema
- Dry cleaning
- Frequent bus service
- Gym
- Hair salons
- Local government offices
- Local recreation facility
- Local theater venues
- Medical offices
- Multimodal center
- Municipal parking garage
- Non-profit workplaces
- Offices
- Pharmacies
- Regional shopping
- Restaurants
- Trails & street trees
- Townhomes

NC1: LOCAL MAIN STREET



Local Main Streets are a type of Neighborhood Center that offer opportunities for small-scale commercial places that serve surrounding neighborhoods. Local Main Streets support a range of commercial, retail, and service uses, as well as higher density housing types (e.g., townhomes and apartment buildings) than those found in surrounding neighborhoods. Often set in emerging suburban areas, Local Main Streets are planned with protected trail systems that connect to parks and preserved natural features. Local Main Streets may be served by public transit but should be accessible to the residents of the surrounding areas via walking or biking.

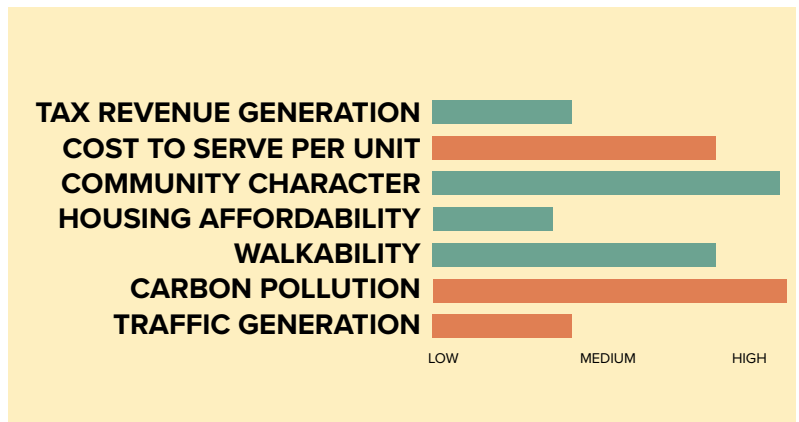
METRICS

DESIGN

- 5 to 12 units per acre
- 40 to 80 intersections per square mile
- 4,000 ft perimeter max
- 25 to 50% protected open space
- 1 to 2 story mixed-use buildings at main intersection

USES

- 50K to 150K SF leasable commercial
- 600 to 800 residential units
- Most residential units in walking distance



TAX REVENUE GENERATION

- More local trips are completed by walking or biking, which helps reduce overall traffic.
- Traffic congestion is generally lower than in suburban strip mall areas, although town centers still accommodate some level of car use.
- Access to public transit and the presence of mixed-use zoning help lessen vehicle miles traveled compared to car-centric development patterns.

COST TO SERVE PER UNIT

- Infrastructure, such as roads, water systems, and emergency services, must cover a large geographic area, increasing the cost per resident.
- Lower population density makes services like schools and public transit less efficient.
- Costs remain lower than in remote rural areas but higher than in urban centers due to the need for decentralized utility systems.

COMMUNITY CHARACTER

- Close-knit communities with a small-town feel help foster a strong local identity.
- Historic buildings, small businesses, and natural surroundings enhance the community's character.
- Although cultural and entertainment options are more limited than in cities, strong local traditions add meaningful value.

WALKABILITY

- The small core area—often including amenities, such as a general store, school, and post office—is walkable.
- A lack of pedestrian infrastructure beyond the main village hub limits overall walkability.
- Residents rely on cars for regional travel, though the immediate village center may be easily accessed on foot.

HOUSING AFFORDABILITY

- Larger lot sizes and detached homes increase land and construction costs.
- Limited housing options, such as the scarcity of apartments or townhomes, make it more difficult to find affordable choices.
- Higher transportation costs often offset the lower housing prices found in these communities compared to cities.

CARBON POLLUTION

- Longer commutes to jobs and services contribute to increased vehicle emissions.
- Limited public transit options lead to more costly per-capita fuel consumption.
- Lower density results in higher energy use per home, although some reliance on renewable energy, such as solar power, may help offset emissions.

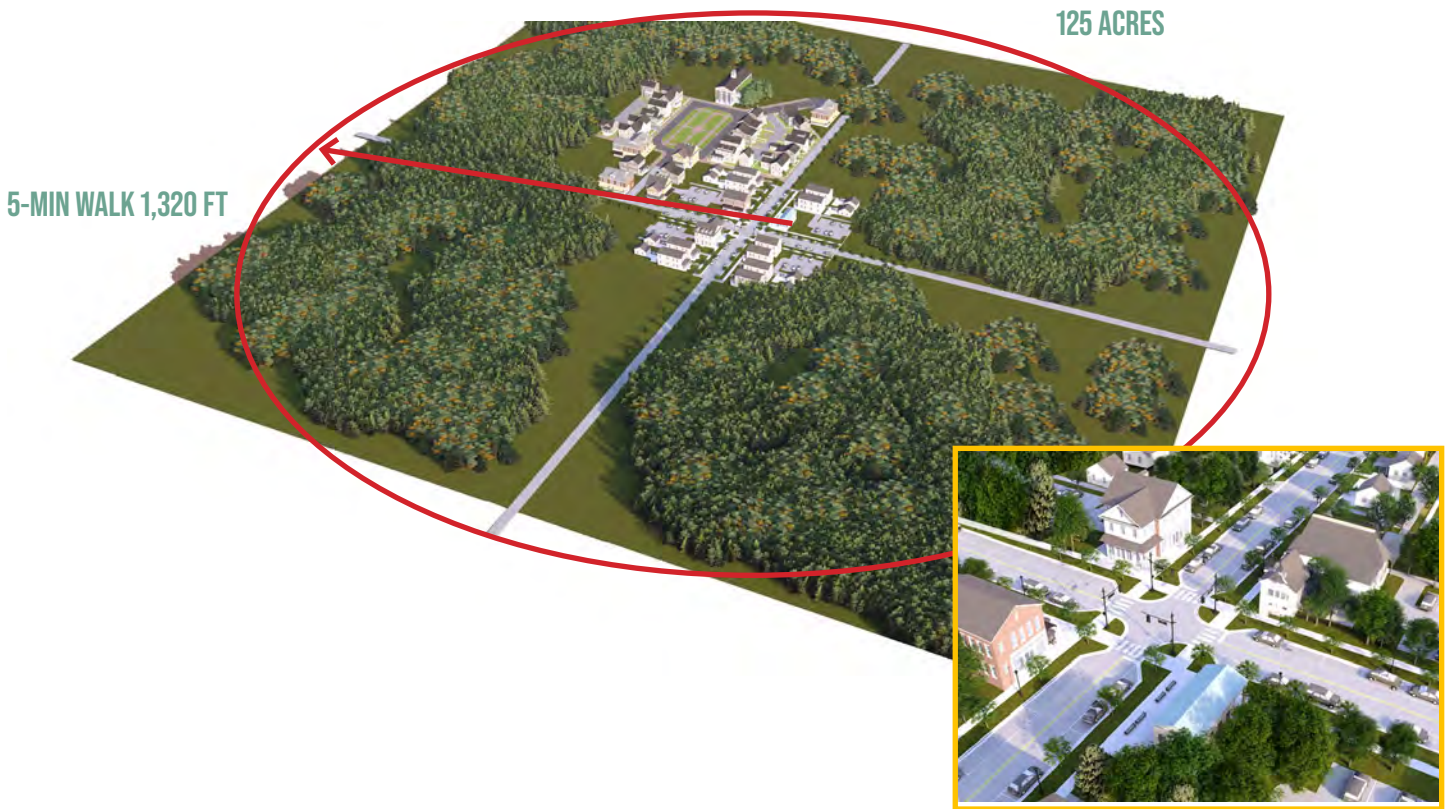
TRAFFIC GENERATION

- Fewer residents and businesses lead to minimal congestion.
- Low population density reduces the number of daily vehicle trips.
- Most trips are long-distance, but the total number of vehicles on the road remains low.

LIKELY USES & ELEMENTS

- Art galleries
- Banks
- Cafes
- Government centers
- Local workplaces
- Multimodal streets
- Restaurants
- Salons
- Single-family houses with accessory dwelling units
- Specialty retail
- Street trees
- Townhomes

NC2: RURAL CROSSROADS



A Rural Crossroads is a type of Neighborhood Center. Rural Crossroads primarily service rural areas with high levels of protected open space. They are planned as four-corner intersections with commercial and civic uses that face each other in an otherwise residential environment. Building densely at the main intersections allows open space with paths and parks. Rural Crossroads support public spaces, such as small plazas and formal greens, that host outdoor dining or neighborhood events. A coffee shop, locally-owned restaurant, post office, and bakery can be enough to create a center for community life in an otherwise sparsely settled, rural area.

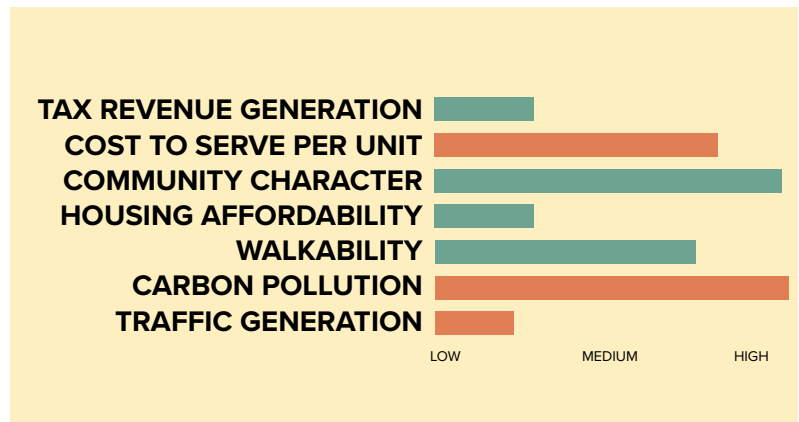
METRICS

DESIGN

- 3 to 7 units per acre
- 5 to 10 intersections per square mile
- 4,000 ft perimeter max in the center
- 40 to 60% protected open space
- 1 to 2 story mixed-use buildings at main intersection

USES

- 10K to 15K SF leasable commercial
- 30 to 80 residential units
- Some residential units in walking distance



TAX REVENUE GENERATION

- Sparse development and minimal commercial activity yield low property and sales tax revenues.
- Agricultural or industrial businesses may contribute to the tax base, but not at levels comparable to denser areas.
- With few taxable properties per acre, overall revenue generation is limited.

COST TO SERVE PER UNIT

- Infrastructure, such as roads, water systems, and emergency services, must cover greater distances, increasing the cost per resident.
- Low-density development makes public services less efficient, though slightly easier to maintain than in fully remote rural areas.
- School bus routes, utility extensions, and longer emergency response times add to municipal expenses.

COMMUNITY CHARACTER

- Small, tightly connected communities foster a strong local identity.
- Historic buildings, family-owned businesses, and gathering spots contribute to rural charm.
- These areas often have a stronger sense of character than sprawling rural regions but lack the density found in village centers.

WALKABILITY

- Spread-out development patterns require residents to rely heavily on personal vehicles.
- Limited sidewalks, pedestrian infrastructure, and clustered amenities reduce walkability.
- Most errands require driving, lowering accessibility for non-drivers.

HOUSING AFFORDABILITY

- Large lot sizes, single-family homes, and one- to two-story mixed-use buildings result in higher per-unit costs.
- Limited housing stock and a lack of rental options constrain affordability.
- Higher transportation costs increase the overall cost of living.

CARBON POLLUTION

- Longer vehicle commutes for work, school, and shopping lead to increased emissions.
- Limited public transit options contribute to high per-capita fossil fuel use.
- Larger homes and less energy-efficient infrastructure result in higher carbon footprints.

TRAFFIC GENERATION

- Fewer residents and businesses lead to minimal congestion.
- Low population density reduces the number of daily vehicle trips.
- Some truck or farm equipment traffic may occur, but overall vehicle flow remains light.

LIKELY USES & ELEMENTS

- General store
- Ice cream shop
- Post office
- Sidewalks
- Town offices

PHYSICAL FRAMEWORK OVERVIEW

The physical framework for Western Greeley outlines a compact and connected pattern of growth that balances development with environmental stewardship. It focuses new neighborhoods near existing infrastructure while preserving key natural systems, like wetlands and wildlife corridors. Streets, parks, and open spaces are organized to create walkable, livable communities with clear centers and strong local identity. Each of these sample sites were designed in accordance with the principles of walkable, mixed-use, mixed-income neighborhoods, representing a stark departure from Western Greeley's current development pattern of sprawl.



CHISMAR PROPERTY

- Example of a Town Center.
- Creates a compact, walkable neighborhood that blends housing, civic spaces, and nature through community gardens and green corridors.
- Emphasizes connected streets, accessible parks, and shared public spaces to foster social interaction and ecological balance.



WELD TRUST PROPERTY

- Example of a Town Center.
- Establishes a connected residential community defined by open space linkages, neighborhood centers, and environmentally sensitive design.
- Promotes diverse housing types, walkable streets, and preserved natural corridors that strengthen community identity and resilience.

UPTOWN

- Example of a Regional Center.
- Develops a vibrant mixed-use district featuring regional trails, a central park, senior living, multi-family housing, and maker spaces.
- Integrates employment, recreation, and housing through walkable streets and green corridors that link all parts of the neighborhood.



DELANTERO CENTER

- Example of a Local Main Street.
- Forms a balanced, compact community with townhomes, civic buildings, and preserved wetlands organized around a clear block network.
- Encourages daily walking and community life through short blocks, shaded streets, and a local corner store at its core.



JBS HEADQUARTERS

- Example of a Regional Center.
- Creates a regional employment hub combining corporate offices, mixed-use retail, and a regional park connected by a continuous green spine.
- Enhances accessibility and identity through pedestrian-friendly streets, trails, and signature gateway architecture.



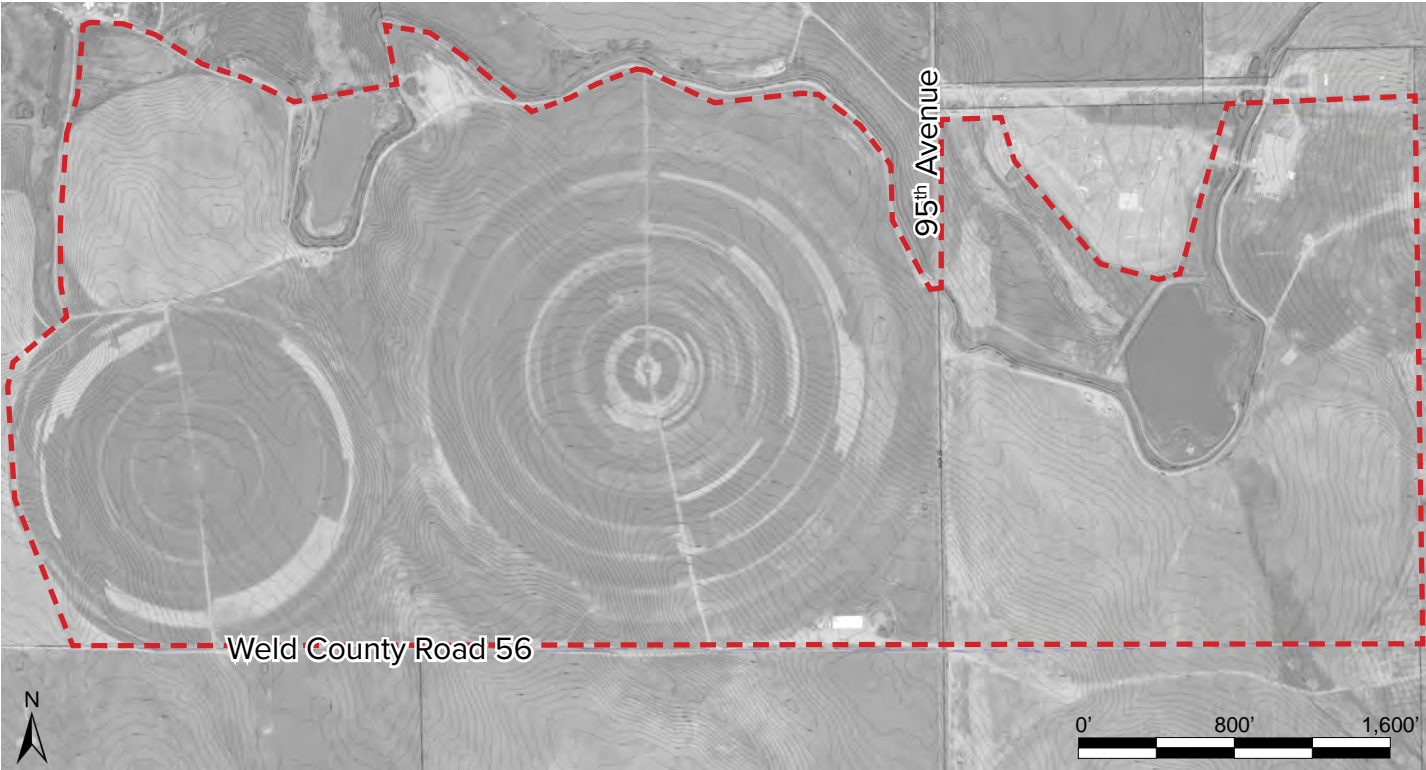


CHISMAR PROPERTY



CHISMAR PROPERTY

INTRODUCTION



Existing Conditions

The Chismar Property encompasses roughly 310 acres of gently sloping terrain framed by open fields and distant mountain views. The proposed mixed-use neighborhood at this site aims to connect Greeley’s established residential areas to the east with emerging employment corridors to the west. Unlike nearby single-use subdivisions with broad lots and disconnected streets, this plan envisions a compact, walkable community where daily needs and casual encounters are within easy reach.

The property’s unique landscape provides an opportunity to integrate greenways, neighborhood parks, and natural drainage corridors into the street network. These features enhance visual character, support ecological health, and lay the foundation for a sustainable, connected neighborhood.

ILLUSTRATIVE PLAN

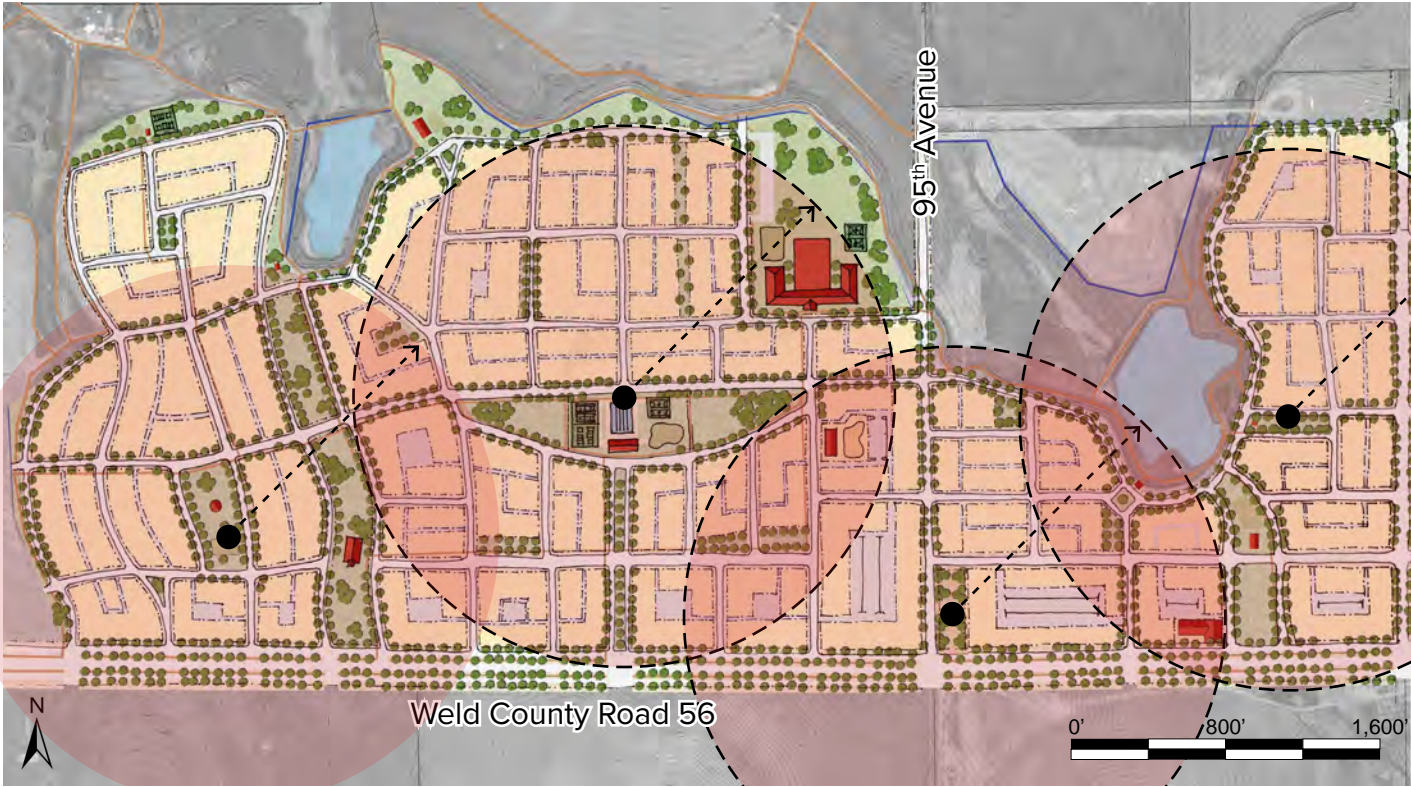


Illustrative Plan

DESIGN FEATURES

- 1 Neighborhood Greens:** Communal greens are distributed near homes to reinforce community identity and maintain open spaces in scale with surrounding buildings.
- 2 Aligned Frontages:** Building fronts follow consistent lines to create cohesive, well-defined streetscapes.
- 3 Civic Anchors:** Community buildings along greens or central spaces serve as visual and social gathering points.
- 4 Rear/ Mid-Block Parking:** In central and mixed-use areas, parking is mid-block or via rear lanes to keep frontages active and pedestrian-friendly.
- 5 Traffic-Calming Streets:** Curves, shifts, and terminations at intervals under 1,000 feet slow traffic and enhance walkability.
- 6 Preserved Natural Systems:** Existing wetlands are protected and new retention areas create continuous ecological corridors.
- 7 Perimeter Recreation Fields:** Larger fields are placed at the development's edge to accommodate scale while maintaining internal connectivity.

FIVE-MINUTE WALK



Five-Minute Walk

WALKABILITY FEATURES

- **Connected network:** Short blocks and multiple routes encourage pedestrian movement.
- **Comfortable streets:** Wide sidewalks, trees, and shade improve the walking experience.
- **Close amenities:** Daily needs are within a five-minute walk of most homes.
- **Safe crossings:** Calmed streets and clear crossings prioritize pedestrian safety.
- **Active edges:** Building fronts and shopfronts create lively, engaging streets.
- **Recreation trails:** Linked walking and biking trails connect parks and open spaces.

TRANSECT PLAN



Transect Plan

The form-based coding for the Chismar Property seeks to ensure walkability, aesthetic cohesion, and a strong sense of place. It also provides flexibility in how buildings may be used over time, supporting a vibrant, adaptable community while giving developers and residents a shared vision for long-term growth.

- Civic Park
- Civic Building



COMMUNITY CHARACTER



Town Center

The Chismar Property plan envisions a new, pedestrian-friendly neighborhood that integrates built form with the surrounding landscape. Walkable streets, a connected network of parks and trails, and well-designed public spaces—including plazas and community gathering areas—foster social interaction and a strong sense of place.

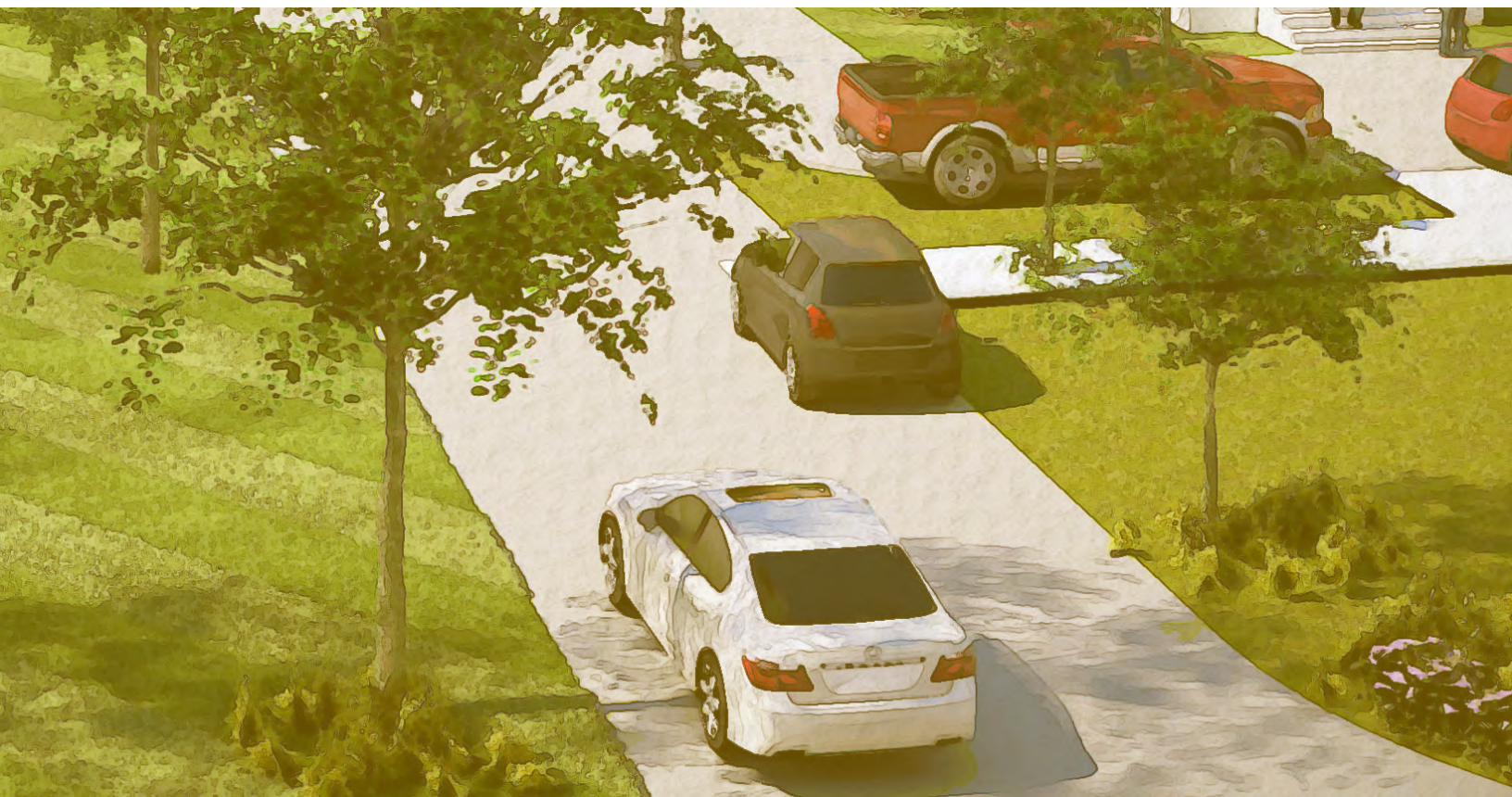
A mix of housing types, including live-work units, is supported by neighborhood-serving commercial uses, a town center, a school, and accessible parks. Together, these elements create a complete mixed-use neighborhood where daily needs can be met close to home, supporting economic vitality and long-term livability.

Green infrastructure—such as bioswales, native landscaping, and low-impact stormwater systems—enhances environmental resilience while shaping attractive, functional open spaces. These features reinforce the neighborhood’s relationship to the landscape and contribute to a high-quality public realm.

By linking existing neighborhoods with new opportunities for living, recreation, and work, the Chismar Property establishes a balanced and enduring model for greenfield growth that advances social, economic, and environmental sustainability.



WELD TRUST PROPERTY



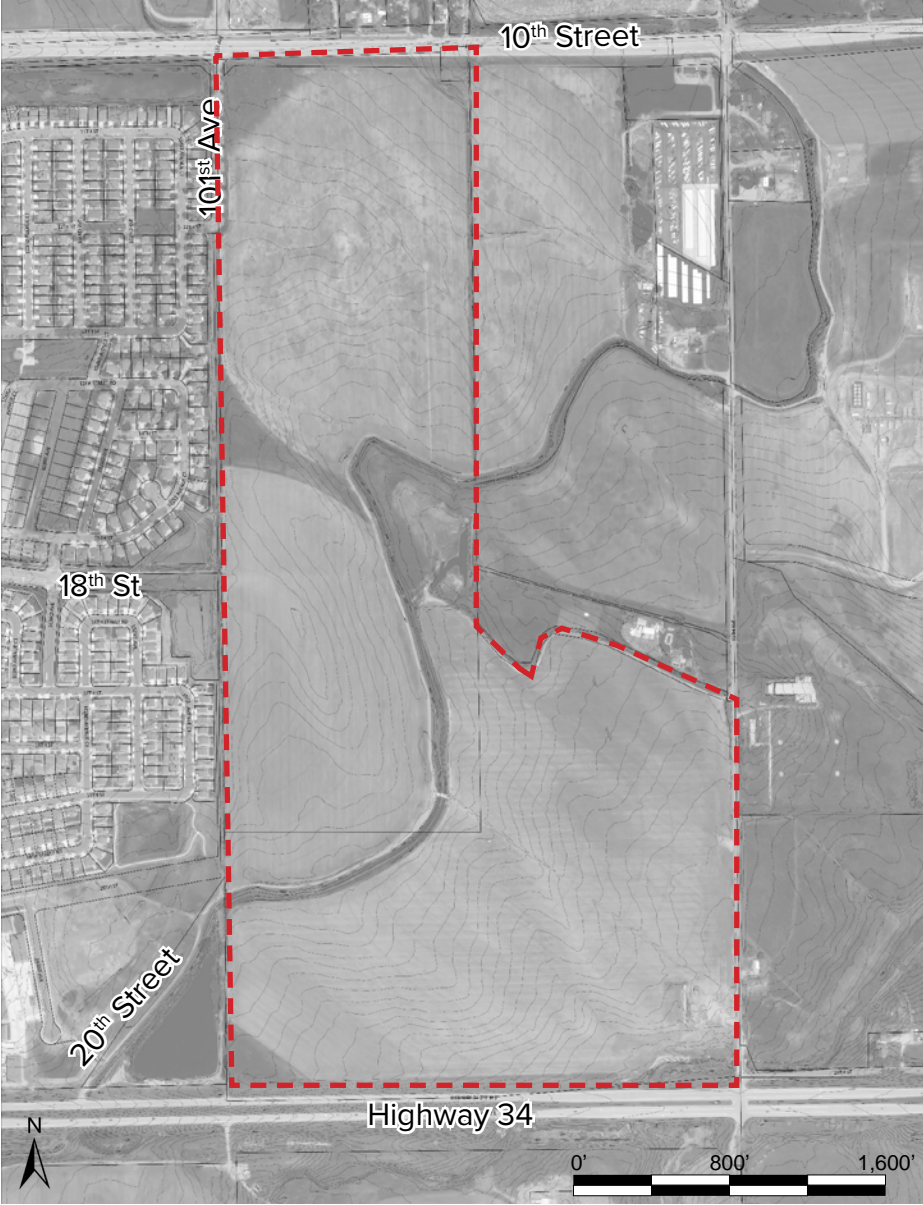
WELD TRUST PROPERTY

INTRODUCTION

The Weld Trust Property occupies approximately 214 acres between 10th Street and Highway 34, where the city’s residential neighborhoods meet open agricultural land.

The site is characterized by gently rolling topography, native grasslands, and intermittent drainage corridors that flow toward the regional wetland system to the north.

Existing access is provided by two-lane rural roads, with nearby connections to established residential areas, schools, and employment centers. Vegetation consists primarily of prairie species and scattered tree stands along drainage lines, offering opportunities for integrated open space and habitat preservation. While portions of the site have been historically used for limited agricultural activity, much of the land remains undeveloped, providing a flexible foundation for a new, compact, and environmentally sensitive neighborhood structure.



Existing Conditions

ILLUSTRATIVE PLAN

The proposed plan for the Weld Trust Property establishes a connected neighborhood framework centered on open space, walkability, and environmental balance.

A network of green corridors links parks, residential areas, and civic spaces, while compact blocks and clear neighborhood centers foster a strong sense of place. Streets are designed for safety and comfort, with tree-lined sidewalks, rear-access parking, and calmed traffic movement.

Natural drainage systems are integrated into the layout, preserving wetlands and enhancing ecological performance. Together, these features create a livable and sustainable community that blends seamlessly with Greeley’s urban fabric.



Illustrative Plan

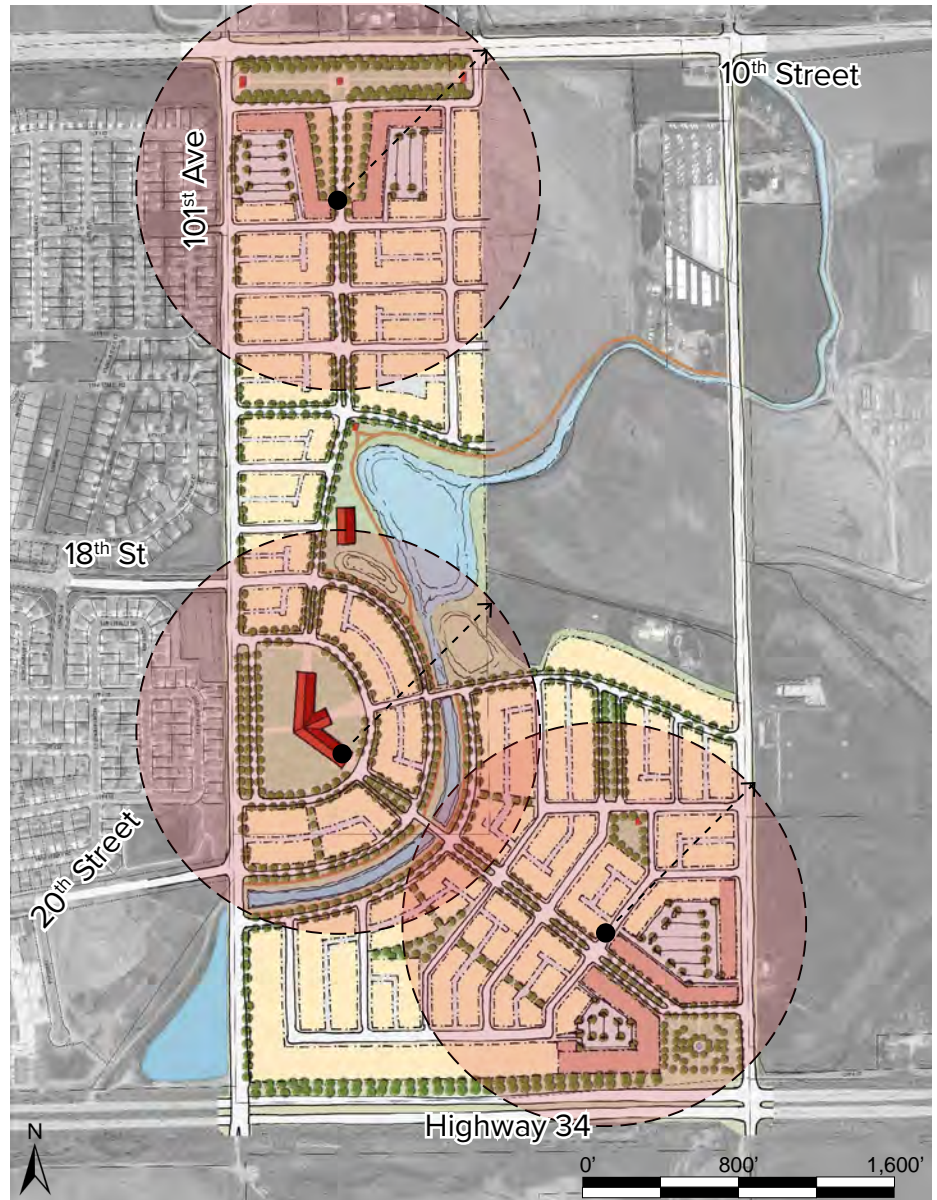
DESIGN FEATURES

- 1 Connected Green Network:** Parks, greens, and natural corridors link residential areas to community spaces and recreation.
- 2 Defined Neighborhood Structure:** Compact blocks and identifiable centers cultivate a strong community identity.
- 3 Civic Anchors:** Community buildings along greens or central spaces serve as visual and social focal points.
- 4 Rear-Loaded Parking:** Parking is located mid-block or via alleys in mixed-use areas to keep frontages active and pedestrian-friendly.
- 5 Edge Stewardship:** Natural edges act as buffers, preserving views and protecting ecological systems.
- 6 Future Connectivity:** Rights-of-way allow future street and trail connections to neighboring areas.

FIVE-MINUTE WALK

Walkability within the Weld Trust plan is guided by the principle of creating a safe, comfortable, and engaging environment for pedestrians. The design emphasizes close proximity to daily destinations, well-connected paths, and visual interest along every street.

Three neighborhoods are envisioned with two centers located on major streets and one center with a school (if needed) situated ceremonially in the middle of an open space.



Five-Minute Walk

WALKABILITY FEATURES

- **Interconnected routes:** Short, linked streets provide direct pedestrian access.
- **Comfortable paths:** Sidewalks, trees, and shade improve walk quality.
- **Accessible amenities:** Parks and civic spaces are a short walk from homes.
- **Safe crossings:** Narrow streets and defined intersections slow traffic.
- **Active edges:** Homes and shops face streets to create visibility and life.
- **Linked trails:** Paths connect open spaces, recreation, and green corridors.

TRANSECT PLAN

The transect framework organizes the community in a clear gradient, from a mixed-use neighborhood center to quieter residential areas and natural edges. This structure supports diverse building types, walkable streets, and open spaces scaled to each context.

Higher-intensity uses are located near the center with civic spaces and small-scale retail, transitioning outward to residential neighborhoods and preserved landscapes.

This balanced approach creates a cohesive, legible community form that reflects both urban vitality and environmental stewardship.

The Weld Trust Property emphasizes relationships—between buildings, streets, and public spaces—so that a public building like a school can be an integrated community landmark rather than an isolated institutional complex. The school’s compact form, clear frontage, and pedestrian-friendly orientation allow students to walk or bike from nearby homes, reinforcing accessibility and neighborhood connectivity.



Civic Park
 Civic Building

COMMUNITY CHARACTER

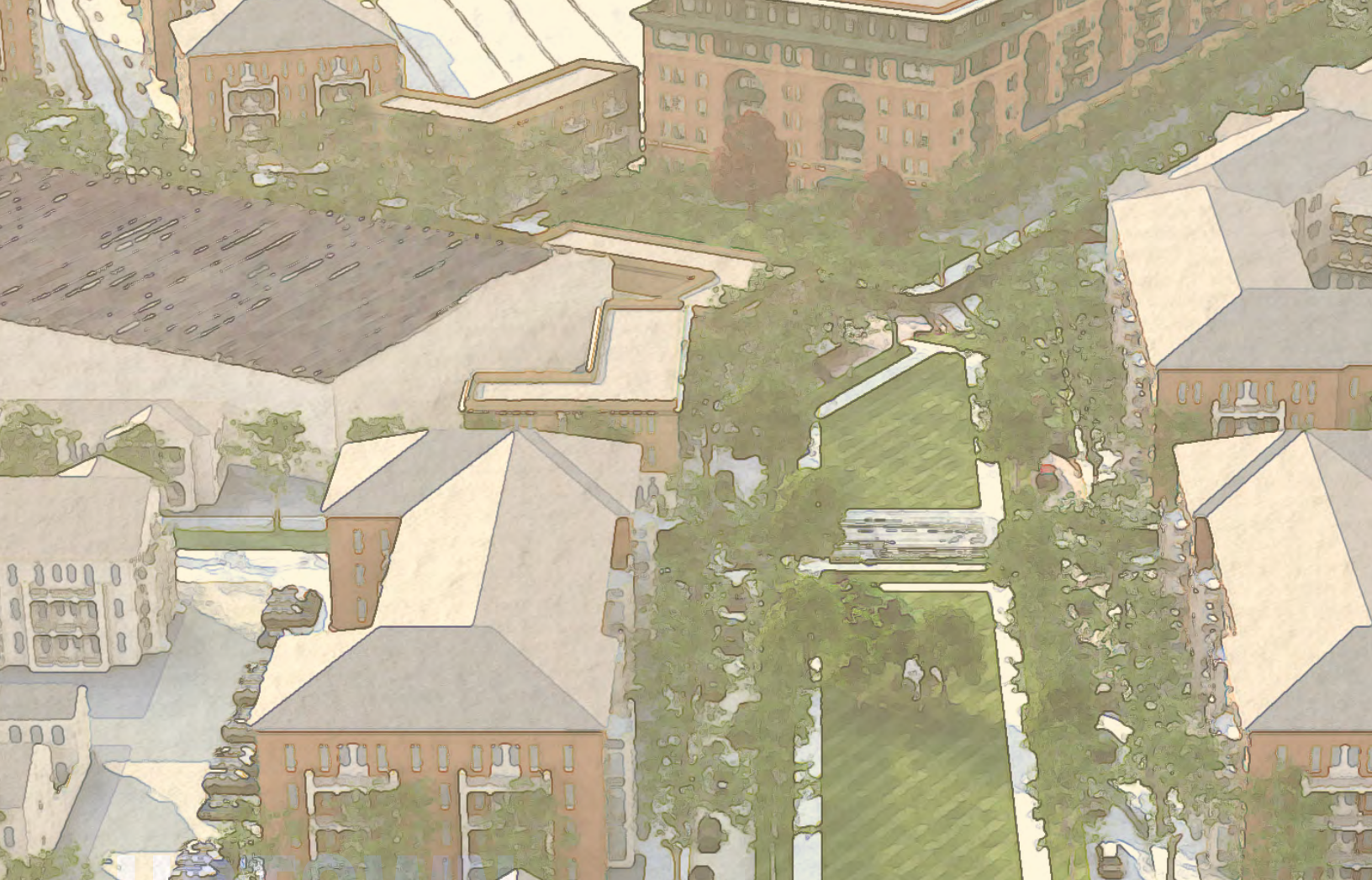


Center green leading to the neighborhood school

The Weld Trust Property emphasizes high-quality public spaces as the social backbone of the neighborhood. Parks, trails, and greenways connect homes with civic amenities, providing opportunities for recreation, gathering, and community events while reinforcing the area's natural character.

A mix of housing types, neighborhood-serving retail, small businesses, and flexible building forms supports long-term economic resilience. Placing these uses within walking distance of homes and public spaces fosters a vibrant, people-oriented environment active throughout the day.

Through its compact, walkable design, integrated green infrastructure, and community-focused amenities, the Weld Trust Property establishes a model for sustainable growth that strengthens the western edge of Greeley while enhancing the quality of life for residents.



UPTOWN



UPTOWN

INTRODUCTION



Existing Conditions

The Uptown site encompasses approximately 300 acres near Greeley’s central corridor, positioned between established neighborhoods to the south and future employment districts to the north. The gently undulating land, with existing tree cover and remnant drainage swales, offers opportunities for ecological restoration. Its proximity to major roadways and community amenities makes it well-suited for a higher-intensity, mixed-use neighborhood. Currently characterized by open pasture and a few access points, the site provides a flexible foundation for a compact, connected community within Greeley’s urban fabric.

EXISTING PLAN



Existing Development Plan

There is currently a plan for Uptown’s development, which organizes the site into distinct, single-use areas, with separate blocks for big-box and small-box retail, drive-thru commercial uses, hotel, medical offices, and senior housing. Land uses are largely isolated from one another, with surface parking and limited connectivity dominating the site. This separation of uses prioritizes vehicular access over pedestrian activity and limits opportunities for social interaction, walkability, and a cohesive public realm.

The Uptown plan proposed on the following pages transforms this segmented layout into a multi-story, mixed-use regional center that maintains the same range of uses while introducing new housing, maker spaces, and community amenities. By integrating these functions vertically and across compact blocks, the new plan creates a vibrant, walkable district where residents can live, work, shop, and recreate in close proximity. Streets, trails, parks, and plazas are woven throughout, replacing isolated parking lots and single-use blocks with an interconnected network of public spaces and pedestrian-friendly frontages.

PROPOSED PLAN

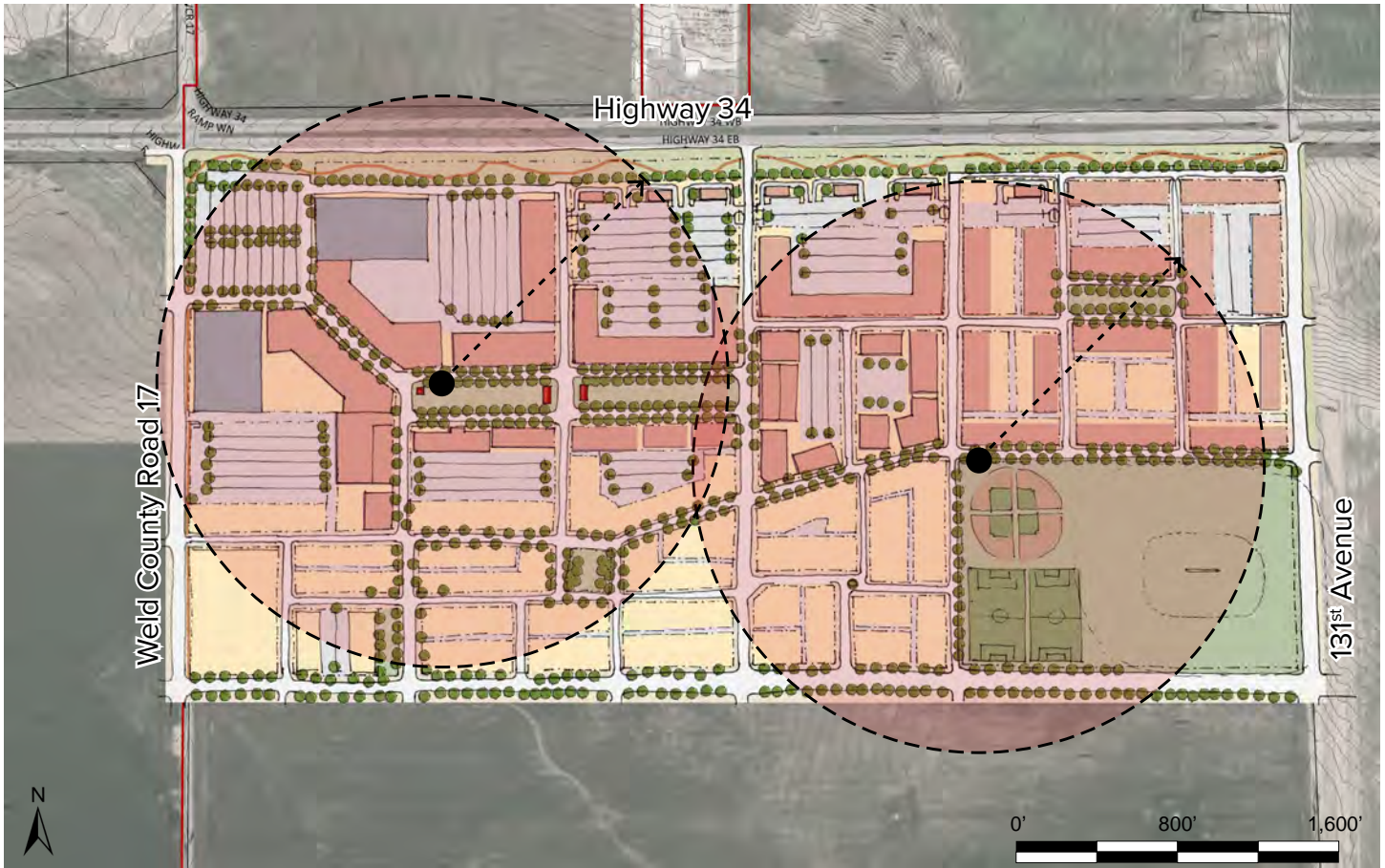


Illustrative Plan

DESIGN FEATURES

- 1 Regional Trail Connection:** A multi-use trail crosses the site, linking to the City’s trail system and connecting residents to parks and nearby neighborhoods.
- 2 Regional Park:** A central park offers recreation fields, natural areas, and community spaces accessible to all Greeley residents.
- 3 Senior Living Campus:** Housing for older adults is close to amenities, healthcare, and open spaces in a safe, walkable setting.
- 4 Multi-family Housing:** Higher-density residential buildings near the park and mixed-use areas support diverse living options and activate public spaces.
- 5 Industrial and Maker Spaces:** Light industrial buildings and flexible maker spaces along the southern edge encourage innovation, craft, and local entrepreneurship.
- 6 Complete Street Network:** Streets include sidewalks, planting strips, and street trees, promoting safe, comfortable movement for all users.
- 7 Integrated Water Systems:** Naturalized drainage corridors and wetlands manage runoff and enhance the ecological character of the site.

FIVE-MINUTE WALK

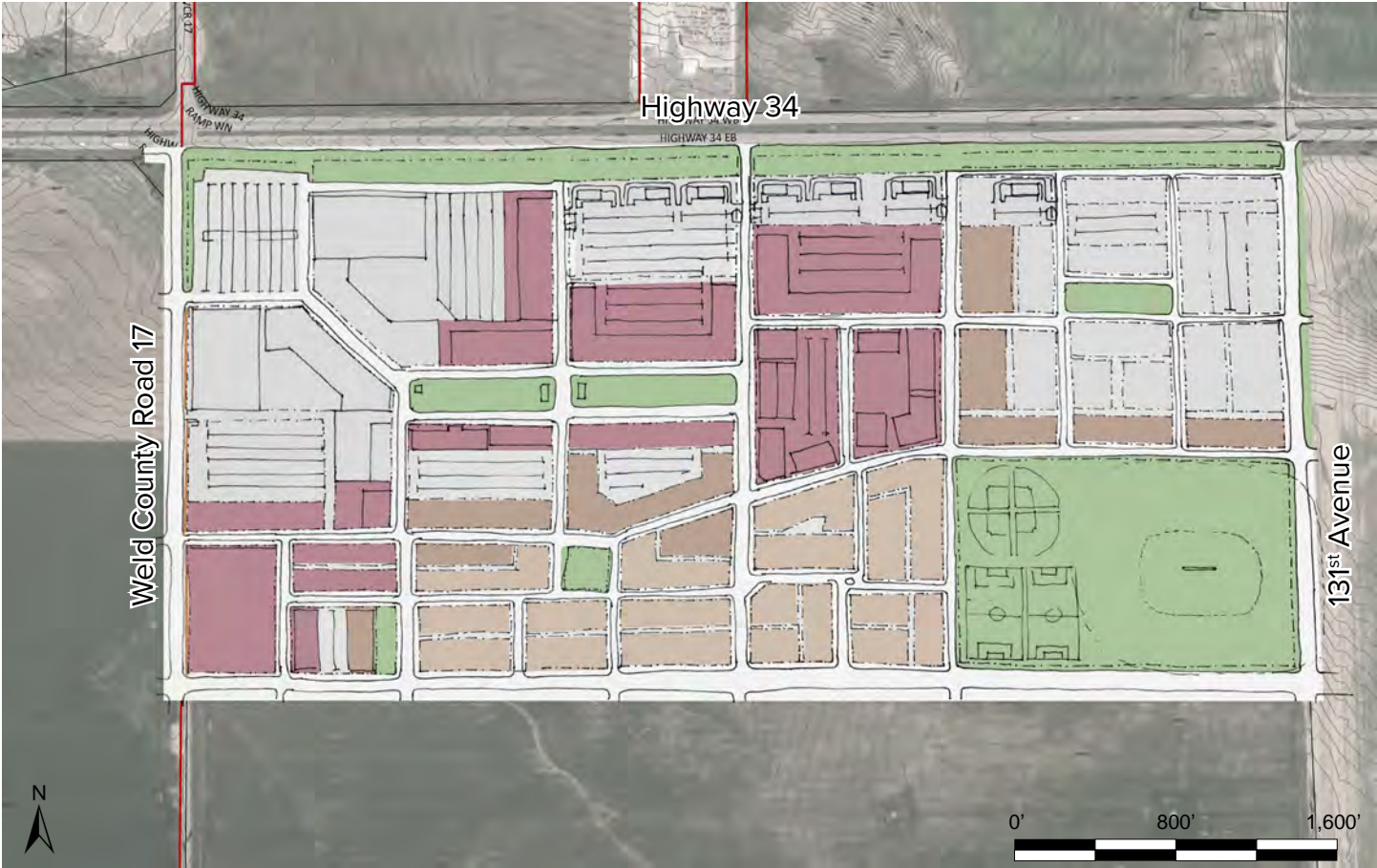


Five-Minute Walk

WALKABILITY FEATURES

- **Compact Block Structure:** Short blocks and frequent intersections reduce walking distances and create more direct, intuitive routes.
- **Mixed Land Uses:** Homes, shops, services, and jobs located close together give people a reason to walk between daily needs.
- **Human-Scaled Design:** Streets sized for people rather than oversized for cars feel safer, calmer, and more inviting to walk along.
- **Linked amenities:** Paths connect housing, civic uses, and mixed-use areas.
- **High-Quality Pedestrian Infrastructure:** Continuous sidewalks, safe crossings, shade, lighting, and seating support comfortable and accessible walking for all ages.
- **Ground-Floor Frontages:** Buildings with doors, windows, and street-facing activity create visual interest and a sense of safety for walkers.
- **Public Realm and Greenery:** Parks, plazas, trees, and landscaping add beauty, comfort, and social space, encouraging people to linger rather than pass through.






TRANSECT PLAN



Transect Plan

POTENTIAL BUILD OUT

A potential building program has been estimated for the Uptown site. Sites that use the optional Westside Form-Based Code Overlay have more development potential because requirements involving design elements, like lot coverage, setbacks, and parking, are more flexible in the interest of creating dense, intense, walkable centers of activity.

Transect	Acres	# Units	Retail/Office SF
 SD-W: Workplace	88 acres		550,000 SF Retail 500,000 SF Industrial
 T5: Mixed-Use Center	66 acres	2,000 units 300 hotel rooms	400,000 SF Medical 500,000 SF Office 500,000 SF Retail
 T4-O: General Open	25.5 acres	500 units	100,000 SF Retail
 T4-R: G- Restricted	45.5 acres	500 units	
 Civic	35 acres		
Total Developed	260 acres	3,000 units	2,550,000 SF Retail/office

COMMUNITY CHARACTER



Central green surrounded by mixed-use development

Conventional development patterns in Uptown have largely produced auto-oriented, single-use commercial areas that separate housing, employment, and services. While familiar to the development community, this model does not align with the City’s long-term goals for affordability, mobility, fiscal sustainability, or community character. These patterns limit walkability, reduce economic resilience, and constrain the creation of vibrant public spaces.

This Area Plan establishes a clear shift away from conventional development toward walkable, mixed-use neighborhoods as the new default form of growth in Uptown. Future development is intended to integrate housing, employment, retail, and civic uses within compact, connected environments that support daily needs, encourage active transportation, and foster a stronger sense of place.

Within commercial areas, redevelopment is expected to evolve from single-use retail and office formats into mixed-use districts that combine residential uses, employment, and active ground-floor spaces. Housing is a fundamental component of this long-term vision, supporting local businesses, enabling future transit service, and contributing to a public realm that remains active throughout the day and evening.

Together, these changes redefine community character in Uptown—shifting from isolated, auto-oriented development to complete, people-centered districts that support economic vitality, social interaction, and long-term livability.



DELANTERO CENTER



DELANTERO CENTER

INTRODUCTION

The Delantero Center site, located between Weld County Roads 54 and 56, encompasses gently rolling land with open fields, intermittent drainage swales, and scattered tree lines. The area benefits from strong regional access and proximity to existing residential neighborhoods and open spaces. Natural flow ways and views toward surrounding agricultural landscapes provide an opportunity to create a compact, connected, and context-sensitive community that transitions continuously between urban and rural environments. Half of the 300 acre site could be developed, leaving the other half preserved and used for agricultural purposes.

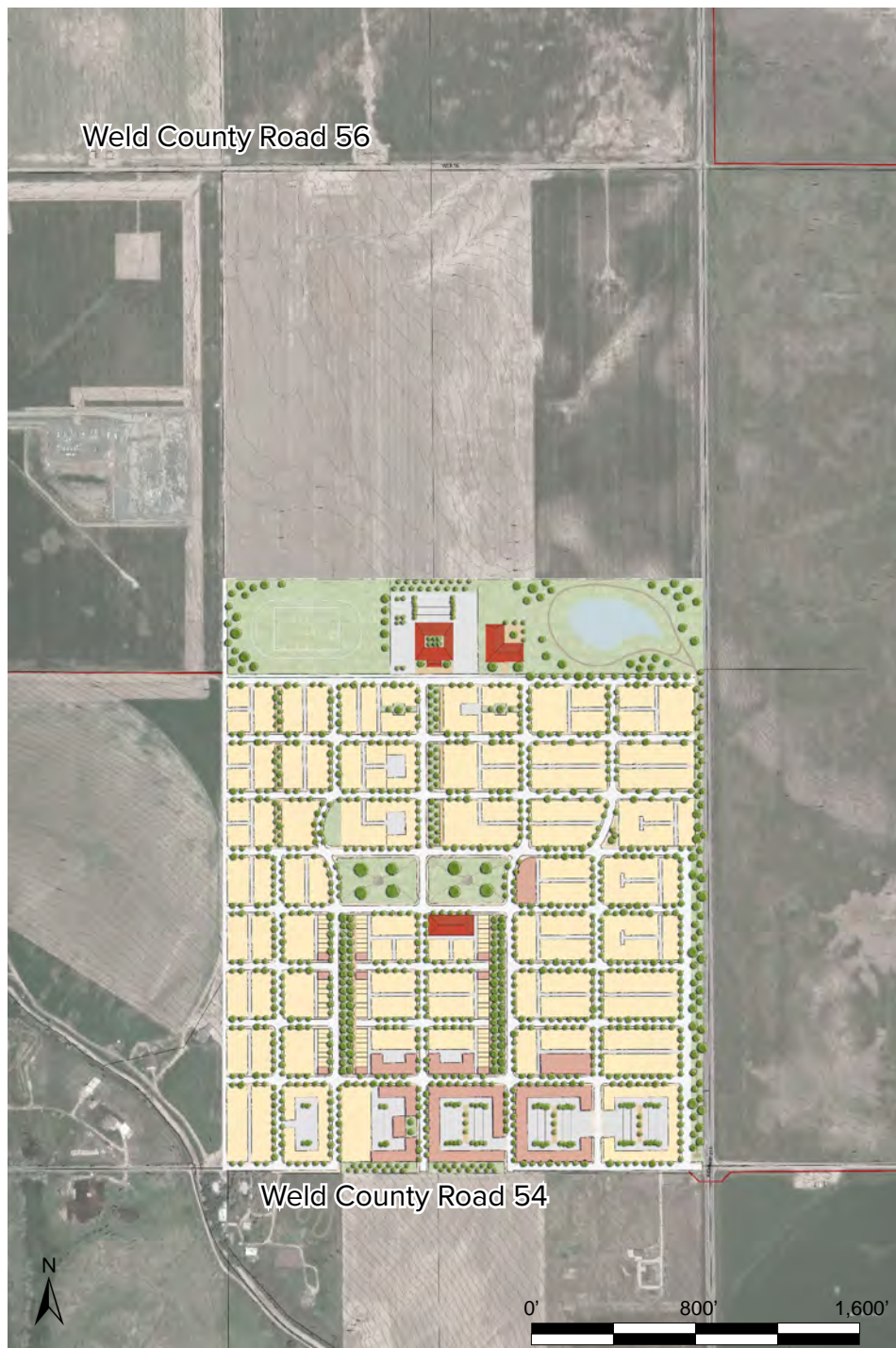


Existing Conditions

ILLUSTRATIVE PLAN

The plan for Delantero Center is founded on principles of connectivity, diversity, and environmental integration, creating a balanced urban structure that supports daily life within walking distance. Multiple entrances distribute traffic evenly and prevent congestion at the community's edges, while a clear hierarchy of streets offers accessibility and legibility. Neighborhood commercial development along the primary corridor provides daily conveniences and serves both residents and passersby.

Neighborhood blocks are designed for flexibility, supporting a range of housing types, from townhomes and cottages near the center to larger lots and open space at the edges. Parking is mid-block, accessed from alleys to maintain active street frontages. A civic or community building, located at the end of a north-south axis, serves as a visual anchor and local gathering place, framed by gardens and small greens. The design preserves existing wetlands and flow ways, connecting them to the open space network to enhance stormwater management and habitat value. Squares, greens, and tree-lined streets are proportioned to create comfortable outdoor "rooms," ensuring each area contributes to the community's sense of place and cohesion.



Illustrative Plan

ILLUSTRATIVE PLAN

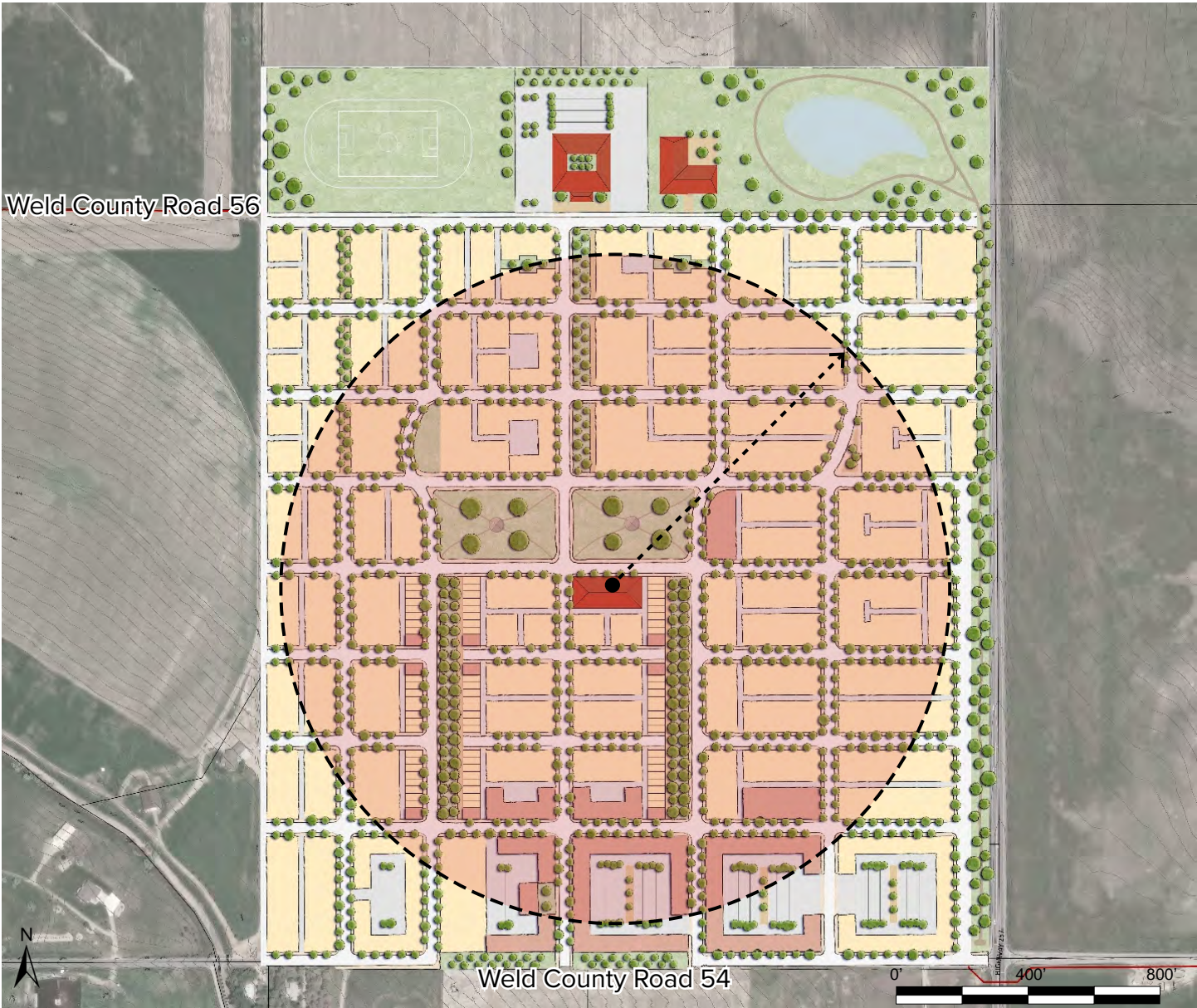


Illustrative Plan Detail

DESIGN FEATURES

- 1 Multiple Access Points:** A connected network improves circulation and reduces traffic.
- 2 Corner Store:** A local corner store offers accessible goods for residents and passersby.
- 3 Mid-Block Parking:** Garages accessed from alleys enable friendly street frontages.
- 4 Main Street Townhomes:** Varied housing types provide attainable housing options.
- 5 Civic Anchor:** A community building sits along the north-south axis and serves as a visual and social focal point.
- 6 Preserved Wetlands:** Wetlands and flow ways are maintained within the open space network.

FIVE-MINUTE WALK



Five-Minute Walk

WALKABILITY FEATURES

- **Short Blocks:** Compact blocks encourage pedestrian movement.
- **Street Trees:** Street trees add comfort, protection, and shade to pedestrian routes.
- **Linked Greens:** Parks, greens, and natural corridors connect residential areas to community spaces and civic areas.
- **Neighborhood Commercial Uses:** Daily needs are within a five-minute walk of most homes.

TRANSECT PLAN

The transect plan organizes the community from a mixed-use core along the main corridor to lower-density residential and preserved open spaces at its edges.

Civic buildings and neighborhood greens define focal points, while townhomes and small cottages frame streets near the center.

Outward from these areas, single-family homes and natural buffers form a soft transition to the rural landscape. This structure creates a balanced, walkable community that blends urban convenience with environmental stewardship.



Transect Plan



COMMUNITY CHARACTER



Main Street

Delantero Center is envisioned as a compact, walkable neighborhood that integrates diverse housing, civic uses, and open spaces within a human-scaled framework. With a significant portion of the site reserved as protected land, development is intentionally concentrated to balance growth with long-term ecological stewardship. Parks, greens, civic anchors, and preserved natural drainage systems shape a connected public realm that supports daily interaction and healthy mobility.

Mixed-use elements at the neighborhood edges provide convenient access to neighborhood-serving uses and help establish a clear identity while reinforcing walkability. Pedestrian-friendly streets, shaded walkways, safe crossings, and connections to regional trails support active transportation and strengthen ties to surrounding neighborhoods.

Flexible building types, neighborhood-serving retail, and adaptable community spaces allow the district to evolve over time while keeping daily needs, recreation, and social opportunities within easy reach. By pairing conservation with compact urban form, Delantero Center exemplifies inclusive, sustainable growth in Western Greeley.



JBS HEADQUARTERS



JBS HEADQUARTERS

INTRODUCTION



Existing Conditions

The JBS Headquarters site is located at the intersection of W. 10th Street and Highway 34, a prominent gateway corridor in Western Greeley. The site occupies gently sloping land between commercial and light industrial uses to the south and emerging residential areas to the north. Its strategic location provides regional visibility and access to major transportation routes, making it well-suited for a mixed employment and innovation district. Existing conditions—including open land, frontage roads, and utility connections—offer a flexible foundation for development.

ILLUSTRATIVE PLAN

JBS USA’s headquarters are located at Promontory Circle in Greeley. This address serves as the central hub for the global food company’s operations. The illustrative plan does not interfere with JBS’s current operations but imagines the company, and its surrounding tenants and neighbors, maximizing the value of the land by densifying with walkable mixed-use urbanism.

The proposed plan is designed to function as both a corporate campus and community landmark, blending employment, innovation, and everyday amenity within a cohesive landscape. The layout organizes office, research, and light industrial buildings around a central green spine that connects public plazas, parking areas, and pedestrian routes.



Illustrative Plan

DESIGN FEATURES

- 1 Central Green Spine:** A landscaped open space links buildings, plazas, and pedestrian paths across the site.
- 2 Integrated Water Management:** Bioswales and retention areas manage stormwater and enhance site ecology.
- 3 Unified Building Orientation:** Structures are aligned to define active frontages and reinforce a cohesive streetscape.
- 4 Gateway Character:** Landmark buildings and signage at key corners improve visibility and establish JBS Headquarters as a regional destination.
- 5 Existing Neighborhood Park:** A large neighborhood park anchors the eastern edge of the site and is connected by a continuous green spine to smaller neighborhood greens and plazas.
- 6 Mixed-Use Highway Frontage:** The Highway 34 edge features retail, dining, and service spaces, creating an active and visible mixed-use corridor.
- 7 Corporate Campus Layout:** Office, research, and light industrial buildings are organized around a primary landscaped boulevard and pedestrian greenway.

FIVE-MINUTE WALK

The JBS area makes a good candidate for a regional center and is large enough to support three complete neighborhoods with workplaces, shopping, and recreation. Walkability within the plan is central to its design, ensuring safe and convenient access between workplaces, amenities, and surrounding neighborhoods. Internal walkways connect office entries, open plazas, and parking areas, reducing reliance on vehicles for on-site movement. Landscaped paths, tree-lined sidewalks, and covered walkways lend comfort and shade, while active frontages invite informal interaction among employees and visitors.

The combination of connected pedestrian routes and nearby mixed-use services fosters a dynamic, campus-like environment that supports health, community, and collaboration.



Five-Minute Walk

WALKABILITY FEATURES

- **Continuous sidewalks:** Cohesive sidewalks provide direct, safe connections across the site.
- **Internal pathways:** A porous network links offices, parking areas, and outdoor plazas.
- **Cross-site connectivity:** Trails connect to regional bike and walking routes.
- **Comfortable streetscapes:** A furnished streetscape encourages outdoor meetings and casual use.
- **Active frontages:** Cafés and retail at the north edge enhance street activity.
- **Seating and shade zones:** A furnished streetscape encourages outdoor meetings and casual use.
- **Defined crossings:** Marked pedestrian routes ensure safety near access drives.
- **Open space access:** The central green spine doubles as a walking and recreation corridor.

TRANSECT PLAN

The transect framework transitions from a mixed-use employment core along W. 10th Street to lighter industrial and logistics uses toward the south. The northern edge integrates public-facing functions—cafés, showrooms, and training spaces—while internal blocks accommodate larger office and production buildings. Green corridors and pedestrian paths unify the site, linking workspaces with shared amenities and open areas. This structure enables a balanced relationship between activity, accessibility, and environmental performance, reinforcing the site’s role as a gateway to Greeley’s economic corridor.

Clear standards for frontage, building height, and public realm design help ensure that new development supports an active street environment, making walking both appealing and convenient. This approach also reduces barriers for mixed-use and small-scale projects—common forms of infill—that can fill vacant or underutilized parcels while strengthening connectivity.



Transect Plan

- Civic Park
- Civic Building



COMMUNITY CHARACTER



Offices are mixed with parks and residential uses.

The JBS Headquarters plan envisions a walkable, amenity-rich employment district that integrates office, light industrial, and community-oriented uses. Cafés, small shops, public plazas, and shaded seating areas encourage street activity and help transform a primarily corporate campus setting into a more connected, people-oriented place. Walkable connections, active open spaces, and a cohesive architectural language position the district as both a regional landmark and a catalyst for reinvestment along Highway 34.

This vision recognizes that much of the area is already developed and will evolve incrementally over time. Rather than requiring wholesale redevelopment, the plan is intended to guide future expansions, site reinvestment, and redevelopment of underutilized areas as opportunities arise. Property owners are encouraged to align new buildings, parking reconfigurations, and public-facing amenities with the illustrated framework to improve connectivity, accessibility, and overall district character.

To support this incremental, market-driven evolution, a Form-Based Code serves as the primary implementation tool. A Form-Based Code provides a clear, predictable pathway for implementation by allowing flexibility in use while setting expectations for building form, frontage, circulation, and open space. Projects that advance walkability, shared amenities, and mixed-use activity can achieve greater site efficiency and long-term value while supporting broader city goals.

Sustainable mobility and landscape strategies further reinforce this transition. Enhanced transit access, internal trails, pedestrian-priority streets, and green infrastructure—including native plantings, bioswales, and low-impact drainage systems—reduce reliance on single-occupancy vehicles and improve environmental performance. Through coordinated reinvestment and consistent application of the plan, the JBS Headquarters area can evolve into a forward-thinking employment district that contributes to economic vitality and a strong sense of place beyond the workplace.

WHAT COMES NEXT

The illustrative plans presented in this chapter are not fixed blueprints or development mandates. Rather, they depict potential outcomes, showing how a more connected, walkable, and mixed-use Western Greeley could evolve when guided by strong policy direction, coordinated public investment, and intentional design. These illustrations translate the Plan’s values into physical form, helping residents, decision-makers, and development partners visualize long-term possibilities.

While individual projects will respond to market conditions, ownership patterns, and infrastructure timing, the core principles illustrated in this chapter must guide all future development decisions. Walkability, compact and mixed-use neighborhoods, diverse housing types, human-scaled streets, and accessible neighborhood centers are not optional features—they are the defining qualities that distinguish a more resilient and livable Western Greeley from conventional, auto-dependent growth.

The Westside Form-Based Code (FBC) Overlay is the primary tool for translating this vision into on-the-ground outcomes. By emphasizing building form, street design, and public realm quality—rather than separating uses—the FBC provides flexibility for developers while establishing clear, predictable expectations. When applied consistently, it enables mixed-use development, incremental reinvestment, and interconnected street networks that align with the character envisioned in this Plan.

ILLUSTRATIVE PLANS

Vision & Possibility



Demonstrate potential outcomes and urban form

FORM-BASED CODE

Regulatory Framework



Shape development form and public realm

The Action Plan provides the framework needed to support implementation over time. Organized across ten focus areas, it identifies the policies, partnerships, infrastructure investments, and sequencing required to align public and private actions. Priorities include advancing compact, mixed-use development; coordinating land use with transit and multimodal infrastructure; expanding housing choice and affordability; protecting natural resources and open lands; and investing in parks, public spaces, and civic facilities that anchor neighborhood life.

Taken together, the illustrative plans, the Form-Based Code, and the Action Plan form a cohesive roadmap for Western Greeley’s future. The plans show what is possible, the code establishes how development should occur, and the Action Plan outlines the steps needed to move from vision to reality. This chapter is not an endpoint, but a call to coordinated action—inviting City leaders, staff, developers, and community partners to work together to realize a more connected, livable, and resilient Western Greeley.

ACTION PLAN

Policies & Investments

BUILT OUTCOMES

Walkable, Mixed-Use Western Greeley



Guide policies, infrastructure, and sequencing

Complete neighborhoods with housing choice & mixed use



05

ACTION PLAN

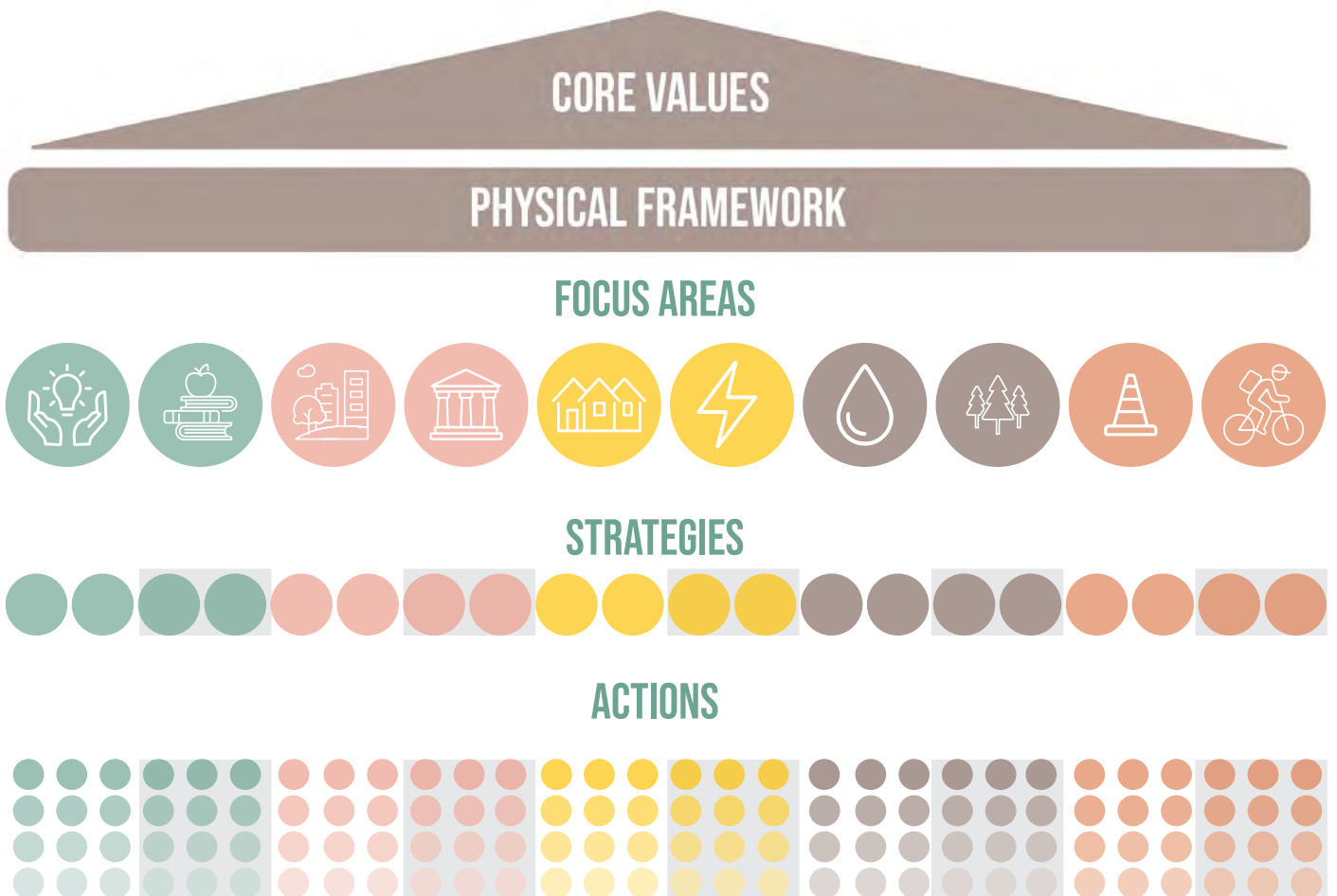
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ACTION PLAN OVERVIEW

The Action Plan translates the vision for Western Greeley into a coordinated set of policies, investments, and partnerships that will guide the area’s evolution over the next decade. Building on the existing conditions review and extensive community engagement, it outlines the concrete steps needed to shape a connected, sustainable, and prosperous Western Greeley.

The Action Plan provides a coordinated structure for future development, public infrastructure investment, and policy decisions across the City of Greeley, private developers, and community partners. It connects land use planning with economic development, environmental stewardship, and social well-being, ensuring that improvements in each sector reinforce progress in others.

Organized around ten key categories—Economic Health & Diversification; Education, Health, & Human Services; Growth & City Form; Historic & Cultural Resources; Housing; Infrastructure; Natural Resources & Open Lands; Parks & Recreation; Public Safety; and Transportation & Mobility—as discussed in Chapter 2, the Action Plan ensures that implementation touches every aspect of community life. Each category includes targeted strategies and actions that collectively strengthen neighborhoods, support local businesses, enhance mobility, protect natural systems, and expand opportunities for residents.



At its core, the Action Plan is designed to:

- **Advance compact, walkable, and human-scaled neighborhoods** consistent with the City’s form-based design principles.
- **Guide near- and long-term infrastructure investments**—including streets, trails, utilities, stormwater systems, and civic amenities—so they effectively support growth.
- **Promote economic vitality and diversification** through strategic infill development, new neighborhood centers, and support for local businesses and emerging industries.
- **Strengthen environmental resilience and stewardship** by expanding access to open space, integrating green infrastructure, and protecting natural resources.
- **Improve community well-being and equity** by enhancing housing choices, access to education and services, mobility options, and public safety.

By linking the City’s long-term vision to actionable, phased steps across ten categories, the Action Plan provides a clear, coordinated roadmap for making Western Greeley a more connected, livable, and resilient community.

STRATEGY

The “Strategy” column lists a goal that has been discussed in detail in this report. Each strategy represents a targeted approach designed to address the specific challenges identified in the preceding analysis. By aligning these goals with measurable outcomes and clear implementation steps, the City can ensure that progress can be effectively tracked and evaluated over time.

ACTION

Several “Actions” follow from the strategies. These actions outline the specific steps required to translate high-level goals into measurable results.

CONCEPTUAL COST

The Plan assigns one, two, or three dollar signs to each of the proposed actions. Actions with three dollar signs are the most resource intensive and will require significant capital planning and/or contributions from multiple levels of government and/or the private sector. Actions with one dollar sign may be accomplished with more limited resources. These cost estimates help assess the feasibility and prioritization of proposed initiatives. Understanding both financial and time commitments allows for better coordination between partners and mitigates risks associated with underfunding or delays.

RESPONSIBILITY

This column represents the legal entity responsible for approving and carrying out the activity listed. Such responsibility often falls on the City Council and staff, but other entities will need to be involved. These local governmental entities will track and refine proposed actions with a particular emphasis on how they align with existing regulations, policies, and community priorities before implementation begins. Depending on the nature of the activity, regional agencies, private stakeholders, or state and federal authorities might need to issue additional approvals for executing action items.

SEQUENCING

“Sequencing” refers to the time for initiating and completing the activity. This column may also be interpreted as specifying the expected frequency of an activity, including items like recurring meetings. Establishing clear sequencing enables tasks to be realized logically and efficiently, preventing overlaps, delays, or resource conflicts.



ECONOMIC HEALTH & DIVERSIFICATION

The traditional neighborhood model proposed in Chapter 4 locates places to live, work, eat, drink, learn, and recreate in close proximity. These mixed-use communities strategically incorporate retail or commercial uses in town centers and will help support a vibrant and diverse economic landscape in Western Greeley. Once these neighborhoods are built or under construction, stakeholders must strive to carry out the following goals and action items, enumerated in order of sequencing.

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 1: Make Western Greeley a region of dynamic walkable neighborhoods attractive to job creators.	EHD.1.1	Reinforce transit and develop innovative zoning to support transit-oriented development and transportation demand management strategies.	\$	Economic Development; Planning and Zoning; Public Works and Transportation	Short-term
	EHD.1.2	Promote a trail system that connects the region's neighborhoods with its scenic hinterlands.	\$	Economic Development and Urban Revitalization; Culture, Parks, and Recreation; Public Works and Transportation	Short-term
	EHD.1.3	Create an optional form-based code overlay that allows flexibility to respond to changes in market conditions related to housing types, kinds of destinations, and varieties of workplaces.	\$\$	Economic Development and Urban Revitalization; Planning and Zoning; Department of Housing and Homeless Solutions	Short- to Mid-term
	EHD.1.4	Partner with major regional employers in growing industries to identify potential opportunities for establishing a presence in Western Greeley.	\$	Economic Development and Urban Revitalization; Greeley Chamber of Commerce	Mid- to Long-term
	EHD.1.5	Attract and incubate small-scale manufacturers to and in Western Greeley, particularly in neighborhood centers.	\$\$	Economic Development and Urban Revitalization; Greeley Chamber of Commerce	Mid- to Long-term
STRATEGY 2: Use retail demand and industrial demand to support complete, innovative sustainable neighborhoods rather than auto-dependent destinations.	EHD.2.1	Encourage retail to cluster in community centers or near transit rather than along strips so each neighborhood has multimodal access to a core area of services.	\$\$-\$	Economic Development and Urban Revitalization; Planning and Zoning; Greeley Chamber of Commerce; private developers	Short- to Mid-term
	EHD.2.2	Promote regionally recognized restaurants and cafés that reflect Western Greeley's diverse cultural and ethnic heritage.	\$\$	Economic Development and Urban Revitalization	Mid-term
	EHD.2.3	Develop district-wide parking strategies in emerging walkable areas to allow more intense development on individual sites.	\$	Economic Development and Urban Revitalization; Greeley Chamber of Commerce; Planning and Zoning; local employers	Mid-term
	EHD.2.4	Create locations attractive to new industrial/technology employers in mixed-use and amenity-rich environments. Integrate into neighborhood industrial uses, such as specialized software and programming, industrial design, and small-scale manufacturing.	\$\$\$	Economic Development and Urban Revitalization; Planning and Zoning; Greeley Chamber of Commerce	Mid- to Long-term

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 3: Provide educational opportunities at all levels to allow children and adults to reach their fullest potential to improve the City's workforce skills.	EHD.3.1	Support programs that encourage the completion of high school, or its equivalent, for all residents of the City.	\$	Johnstown-Milliken School District RE-5; Windsor School District RE-4; Workforce Centers; City of Greeley	Short-term
	EHD.3.2	Aggressively promote and support high school retention programs throughout the City.	\$-\$	Johnstown-Milliken School District RE-5; Windsor School District RE-4; Colorado Department of Education, City of Greeley	Short- to Mid-term
	EHD.3.3	Support literacy programs, including the expansion of library resources.	\$-\$	High Plains Library District; City of Greeley; Aims Community College; UNC	Short- to Mid-term
	EHD.3.4	Support a wide variety of job skills training programs that provide stable, higher paying job opportunities to residents at different skill levels.	\$	Aims Community College; UNC; Economic Development and Urban Revitalization; local employers	Mid-term
	EHD.3.5	Encourage higher education institutions in Western Greeley to provide a variety of new educational programs.	\$-\$-\$	Aims Community College; UNC; Colorado Department of Higher Education	Mid- to Long-term
	EHD.3.6	Encourage and help to increase the training capabilities of local colleges and universities, including increasing the types of graduate programs and degrees offered.	\$	UNC; Aims Community College; Colorado Department of Higher Education	Long-term



EDUCATION, HEALTH, & HUMAN SERVICES

Westward growth in Greeley has initiated community conversations about designing neighborhoods that accommodate a mix of uses in walking distance to one another. This Westside Area Plan and Form-Based Code can further support coordinated approaches to developing neighborhoods and schools by pursuing the strategies and action items below.

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 1: Coordinate the development of neighborhoods, recreational facilities, and transportation infrastructure with the development of schools to meet the City's needs.	EHH.1.1	Align with the City of Greeley's school systems to phase development in a manner that maintains levels of service and offers safe environments for children to attend school.	\$	Economic Development and Urban Revitalization; Planning and Zoning; Johnstown-Milliken School District RE-5; Windsor School District RE-4	Short-term
	EHH.1.2	Coordinate development review with the City's school districts to optimize the efficiency of capital planning and improvements.	\$	Economic Development and Urban Revitalization; Planning and Zoning; Johnstown-Milliken School District RE-5; Windsor School District RE-4	Short-term
	EHH.1.3	Ensure that new school sites can be adequately served by existing and planned infrastructure (including streets, sidewalks, water/wastewater, and public safety facilities).	\$	Economic Development and Urban Revitalization; Planning and Zoning; Public Works and Transportation; Department of Water and Sewer; Johnstown-Milliken School District RE-5; Windsor School District RE-4	Short-term
	EHH.1.4	Locate schools, daycares, playgrounds, libraries, and other municipal amenities to be easily reached on foot or by bike to foster walkability and healthier lifestyles for children.	\$\$\$	Economic Development and Urban Revitalization; Planning and Zoning; Johnstown-Milliken School District RE-5; Windsor School District RE-4	Mid- to Long-term
STRATEGY 2: Improve the overall level of health in Western Greeley.	EHH.2.1	Increase access to safe water by continuing to monitor water for contaminants.	\$	Department of Water & Sewer; Weld County Department of Public Health & Environment; Colorado Department of Public Health & Environment (CDPHE)	Short-term
	EHH.2.2	Continue to allow farming as a commercial operation, however, seek to reduce exposure to endocrine disruptors and agro-toxins, such as pesticides, fertilizers, fumigants, and other by-products of non-organic agricultural practices.	\$-\$	Weld County Extension Services; Colorado Department of Agriculture; local farmers and agricultural associations; Planning and Zoning	Mid-term
	EHH.2.3	Conduct a regional health study and develop a strategic framework to improve health within the region.	\$	Economic Development and Urban Revitalization; Aims Community College; UNC	Mid-term

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 2: Improve the overall level of health in Western Greeley.	EHH.2.4	Initiate research, policies, and programs that increase food security, improve health outcomes, and create social and economic opportunities to attempt to ensure that every resident has access to fresh, healthy, and affordable food.	\$	Economic Development and Urban Revitalization; Greeley Chamber of Commerce; Weld County Public Health; local employers; local farmers; local hospital systems (Banner Health, UHealth)	Mid-term
	EHH.2.5	Improve access to medical care by distributing offices, clinics, laboratories, and hospitals amongst the various districts and neighborhoods that make up the City.	\$\$-\$\$\$	Economic Development and Urban Revitalization; local hospital systems (Banner Health, UHealth); Weld County Public Health; private healthcare providers	Mid- to Long-term
STRATEGY 3: Improve the overall physical and mental health of Western Greeley citizens by increasing quality of life in the region.	EHH.3.1	Encourage schools to provide open campuses. At a minimum, recreational fields should be open to the community after school hours and on weekends.	\$	Johnstown-Milliken School District RE-5; Windsor School District RE-4 Department of Culture, Parks, and Recreation	Short-term
	EHH.3.2	Encourage physical activity through the design of the built environment by adjusting land development regulations to make neighborhoods more complete and walkable and improve streetscapes so that traffic speeds are reduced and pedestrians' and cyclists' comfort and safety is increased.	\$\$-\$	Planning and Zoning; Public Works and Transportation	Short- to Mid-term
	EHH.3.3	Locate medical care providers and human services at transit nodes and designated Transit-Oriented Development sites.	\$\$	Planning and Zoning	Mid-term
	EHH.3.4	Reduce exposure to environmental risk factors, including risk of injury and fatality due to vehicular crashes, by building neighborhoods with lower design speeds on local roads, frequent crosswalks, narrow lanes, four-way stop signs (instead of signals), two-way traffic, and a network of safe routes to school.	\$\$-\$\$\$	Public Works and Transportation; Colorado Department of Transportation (CDOT)	Mid- to Long-term
	EHH.3.5	Increase pedestrian and cyclist connectivity across natural and man-made barriers, such as freeways, ravines, river beds, canyons, and arroyos. Provide a shorter and more convenient route for non-motorized traffic across barriers.	\$\$\$	Department of Culture, Parks and Recreation; Public Works and Transportation; CDOT; regional trail partners	Long-term



GROWTH & CITY FORM

Greeley’s current growth pattern has led to an inefficient use of infrastructure, limited transportation choices, and suburban sprawl. In response, the City aims to promote a compact urban form, mixed-use and transit-supportive development, neighborhood centers, and infill. The Westside Area Plan supports these goals through mixed-use, transect-based, traditional neighborhood design. The Plan envisions walkable blocks, diverse housing options, vibrant public spaces, and development patterns that strengthen community while reducing auto dependence. The goals and actions below can help create a more connected, livable, prosperous, and sustainable Western Greeley.

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 1: Strategically accommodate population growth in Western Greeley.	GCF.1.1	Designate the City department responsible for crafting a work program to manage and support this Westside Area Plan.	\$	Planning and Zoning	Short-term
	GCF.1.2	Prioritize the development of opportunity sites identified in Chapter 4 through developer incentives, streamlined permitting, and public-private partnerships.	\$\$	Economic Development and Urban Revitalization; Planning and Zoning	Short- to Mid-term
	GCF.1.3	Tap into public-private partnerships to fund development projects, particularly projects in keeping with the principles of mixed-use, walkable design.	\$\$	Economic Development and Urban Revitalization; Planning and Zoning	Short- to Mid-term
	GCF.1.4	Review and amend the City’s zoning and land development regulations when appropriate to encourage new neighborhoods to have greater interconnection of internal streets; provision of small parks and civic functions within neighborhoods; a greater variety of housing types within each neighborhood; and protection of natural features such as critical arroyos.	\$-\$\$	Planning and Zoning; Economic Development and Urban Revitalization; Department of Housing and Homeless Solutions; Department of Culture, Parks, and Recreation	Short- to Mid-term
	GCF.1.5	Investigate resources to remediate properties that have environmental hazards or other impediments to redevelopment.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Mid- to Long-term
STRATEGY 2: Invest in Western Greeley and ensure it is a vibrant, walkable place for all residents, visitors, employers, and employees to live, work, eat, and enjoy.	GCF.2.1	Champion a mix of residential housing types to welcome a diversity of ages and incomes. Housing should include a range of arrangements with both rental and owner-occupied offerings: studio units; 1-, 2-, and 3-bedroom units; townhouses; live-work spaces; duplexes; fourplexes; and single-family executive housing.	\$\$	Economic Development and Urban Revitalization; Planning and Zoning; Department of Housing and Homeless Solutions	Short- to Mid-term
	GCF.2.2	Design street spaces with the intention of creating prominent public spaces with a comfortable sense of enclosure.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 3: Encourage energy-independent and energy-efficient development.	GCF.3.1	Prioritize climate-responsive designs, increased building efficiency, and green building techniques, like LEED (Leadership in Energy and Environmental Design) architectural criteria and LEED ND criteria for neighborhood designs.	\$\$	Planning and Zoning	Short-term
	GCF.3.2	Incorporate “green infrastructure design” and similar light-imprint and low-impact principles for stormwater management and landscaping in streets.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
STRATEGY 4: Protect working farms in order to protect agricultural jobs, as well as the character of Western Greeley.	GCF.4.1	Provide a density bonus for development of formerly active farmland when at least 50 percent of the lot allows for small-scale farming.	\$	Planning and Zoning; Economic Development and Urban Revitalization	Short-term
	GCF.4.2	Discourage the rezoning of agricultural land, except where a percentage of small-scale farming has been reserved.	\$	Planning and Zoning	Short-term
	GCF.4.3	Protect farmland through park credits, the purchase of development rights, outright purchase of land, or other mechanisms available to the City and County.	\$\$-\$\$\$	Planning and Zoning; Greeley Planning Commission; Weld County; Land Trusts / Conservation Partners; Economic Development and Urban Revitalization	Mid- to Long-term



HISTORIC & CULTURAL RESOURCES

Once the heart of Northern Colorado, Greeley can reclaim such status by setting a precedent for walkable, human-scaled neighborhoods in Western Greeley. Within such neighborhoods, building forms can be situated within the unique context of Greeley’s historic built environment, and public spaces can be designed to invite all community members and foster a sense of belonging. Attaining the following goals and action items, depicted in order of sequencing, can help deliver a treasured, livable Western Greeley.

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 1: Reinforce Western Greeley’s identity by aligning new developments with Greeley’s cultural character and Historic Districts while also cultivating a unique style.	HCR.1.1	Consider using pattern books to address the various architectural styles characteristic of Greeley’s historic districts.	\$	Planning and Zoning; Department of Culture, Parks, and Recreation; Department of Historic Preservation	Short- to Mid-term
	HCR.1.2	Pursue climate-responsive architecture authentic to Greeley’s history.	\$\$	Planning and Zoning; Department of Culture, Parks, and Recreation; Department of Historic Preservation	Short- to Mid-term
STRATEGY 2: Create attractive, comfortable, and distinct public spaces that can draw residents, visitors, employees, and more.	HCR.2.1	Host community design workshops for residents and business owners to shape neighborhood character and identity.	\$	Planning and Zoning; Department of Communication and Engagement	Short-term
	HCR.2.2	Develop a distinct brand identity for Western Greeley that uses technology to promote awareness about the subarea’s resources.	\$\$	Department of Culture, Parks, and Recreation; Department of Communication and Engagement; Greeley Art Commission; Greeley Creative District	Short- to Mid-term
	HCR.2.3	Offer amenities that appeal to all ages, like multi-generational playgrounds and game tables.	\$\$	Planning and Zoning; Department of Culture, Parks, and Recreation; Greeley Art Commission; Greeley Creative District	Short- to Mid-term
	HCR.2.4	Invest in a wayfinding network (wayfinding signs, bicycle signs, banners, and more) that directs visitors to Western Greeley’s landmarks and popular destinations, most of which will be in walkable neighborhood cores.	\$\$	Planning and Zoning; Department of Culture, Parks, and Recreation; Department of Communication and Engagement; Greeley Art Commission; Greeley Creative District	Short- to Mid-term
	HCR.2.5	Maintain maintenance standards in Western Greeley’s public spaces.	\$\$\$	Department of Public Works and Transportation	Short- to Mid-term

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 2: Create attractive, comfortable, and distinct public spaces that can draw residents, visitors, employees, and more.	HCR.2.6	Continue funding public art with public and private investments.	\$\$	Planning and Zoning; Department of Culture, Parks, and Recreation; Greeley Art Commission; Greeley Creative District	Short- to Mid-term
	HCR.2.7	Equally distribute cultural resources throughout Western Greeley.	\$\$	Planning and Zoning; Department of Culture, Parks, and Recreation	Short- to Mid-term
	HCR.2.8	Facilitate events that celebrate the different cultures and heritages present in Western Greeley, ranging from its agricultural tradition to its growing Spanish-speaking population.	\$	Department of Culture, Parks, and Recreation; Department of Communication and Engagement; Greeley Art Commission; Greeley Creative District	Short- to Mid-term
	HCR.2.9	Create dog park facilities at small sites.	\$\$	Planning and Zoning; Department of Culture, Parks, and Recreation	Mid-term
	HCR.2.10	Leverage public art or streetscape features to mark key gateways to Western Greeley, like 83 rd Avenue and 10 th Street.	\$\$\$	Planning and Zoning; Department of Culture, Parks, and Recreation; Department of Public Works and Transportation; Department of Communication and Engagement; Greeley Art Commission; Greeley Creative District	Mid- to Long-term



HOUSING

Greeley is in a critical moment of growth, and building mixed-income housing constitutes a citywide priority. The Plan encourages the development of a range of housing types that will satisfy the requirements of developers to build projects that are economically viable and that welcome the diverse and varied set of residents who are and will be attracted to living in Greeley.

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 1: Provide housing in Western Greeley through complete, connected neighborhoods containing quality, affordable, and accessible choices to serve all income levels and age groups.	H.1.1	Maintain a sustainable and efficient housing supply for all residents of Western Greeley by striving to distribute a variety of housing types throughout the City to expand choices available to meet the financial, lifestyle, and cultural needs of Greeley’s diverse population.	\$	Planning and Zoning; Department of Public Works and Transportation; Department of Housing and Homeless Solutions	Short-term
	H.1.2	Strongly encourage housing types that take into account non-traditional households and multi-generational families.	\$	Department of Communication and Engagement; Planning and Zoning; Department of Housing and Homeless Solutions	Short-term
	H.1.3	Allow, by-right, a variety of residential lot sizes and housing types within existing and new neighborhoods.	\$	Department of Planning and Zoning; Department of Housing and Homeless Solutions	Short-term
	H.1.4	Encourage mixed-use developments that allow people to live without requiring everyday use of an automobiles.	\$\$	Planning and Zoning; Department of Housing and Homeless Solutions	Short- to Mid-term
	H.1.5	Work with developers to make sure that affordable housing is part of every new large subdivision. In general, affordable housing includes either public housing (deed-restricted housing involving public subsidies) or workforce housing (market rate housing that is attainable to the average household). Specifically, development in Western Greeley should comply with the City’s adopted housing plans.	\$\$-\$	Planning and Zoning; Department of Housing and Homeless Solutions; private developers	Short- to Mid-term
	H.1.6	Support higher density housing in designated compact neighborhoods on the Land Use Guidance Map.	\$\$	Planning and Zoning; Department of Housing and Homeless Solutions	Mid- to Long-term
	H.1.7	Work with non-profit affordable housing providers to develop publicly owned or publicly subsidized housing in accordance with the City’s adopted housing plans.	\$\$-\$\$\$	Department of Housing and Homeless Solutions; Non-profit Housing Developers (e.g., Habitat for Humanity, local CDCs); state & federal housing agencies (e.g., CHFA, HUD)	Mid- to Long-term

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 2: Expand and diversify the housing stock within neighborhoods.	H.2.1	Pursue funding sources for affordable and mixed-income housing, including public-private partnerships, Community Development Block Grants (CDBG), and HOME investment partnership funds.	\$\$	Department of Housing and Homeless Solutions; Planning and Zoning; Economic Development and Urban Revitalization	Short- to Mid-term
	H.2.2	Champion a mix of residential housing types to welcome a diversity of ages and incomes. Housing should include a range of arrangements with both rental and owner-occupied offerings: studio units; 1-, 2-, and 3-bedroom units; townhouses; live-work spaces; duplexes; fourplexes; and single-family executive housing.	\$\$	Department of Housing and Homeless Solutions; Planning and Zoning; Economic Development and Urban Revitalization	Short- to Mid-term
	H.2.3	Encourage Accessory Dwelling Units (ADUs).	\$\$	Planning and Zoning; Department of Housing and Homeless Solutions	Short- to Mid-term
	H.2.4	Partner with local non-profits and private developers to support the development of assisted living facilities and continuing care communities.	\$	Department of Housing and Homeless Solutions; Planning and Zoning; United Way of Weld County; private developers; local non-profits	Mid-term
	H.2.5	Support housing strategies tailored towards vulnerable groups, from seniors to low-income households to disabled people.	\$	Department of Housing and Homeless Solutions; Planning and Zoning	Mid-term



INFRASTRUCTURE

Accommodating Greeley’s anticipated 50 percent population growth requires significant infrastructure investments over the coming decade. To meet the needs of new developments, the City must thoughtfully assess the adequacy of public facilities and proactively build infrastructure to support future development by committing to the following goals and action items.

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 1: Anticipate growing demand for utilities in Western Greeley.	I.1.1	Continue to fund the upkeep of existing infrastructure.	\$	Department of Water and Sewer; Department of Public Works and Transportation	Short-term
	I.1.2	Continue aligning infrastructure projects with new development projects by including all relevant parties in the development review process.	\$	Planning and Zoning, Department of Water and Sewer; Department of Public Works and Transportation; private utility providers (to an extent)	Short-term
	I.1.3	Expand and utility infrastructure to serve anticipated residential and commercial growth.	\$\$\$+	Department of Water and Sewer; Department of Public Works and Transportation; Planning and Zoning; private utility providers	Short- to Long-term
STRATEGY 2: Continue enacting policies that promote water conservation, reclamation, stormwater management, and access to a sustainable water supply.	I.2.1	Invest in renewable energy infrastructure at new municipal facilities to increase resilience and reduce utility costs.	\$\$\$	Department of Public Works and Transportation; Planning and Zoning; Xcel Energy	Long-term



NATURAL RESOURCES & OPEN LANDS

Stakeholders ranging from farmers to landowners to City staff have recognized the historic value of Greeley’s natural areas. Understanding that new development in Western Greeley could threaten the area’s natural resources and open space, this Plan strives to protect as much natural area as possible with the following goals and action items.

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 1: Protect and enhance ecologically sensitive areas, the Poudre River, other waterbodies, and environmentally important or sensitive habitats.	NROL.1.1	Continue deploying tools like conservation easements, community separators, and land trusts to encourage the protection of sensitive or undisturbed lands.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	NROL.1.2	Enact regulatory and reclamation standards to protect the Poudre River.	\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	NROL.1.3	Adhere to Colorado’s statutes and support conservation practices in construction and landscaping.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	NROL.1.4	Permit dense land uses and developments that protect ecologically sensitive areas.	\$	Planning and Zoning	Short- to Mid-term
	NROL.1.5	During site development, preserve as many arroyos, ponds, streams, trees, and other significant natural areas as possible.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	NROL.1.6	Plan new parks and open spaces around the preservation of ecologically sensitive areas.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	NROL.1.7	Develop trails and greenways that simultaneously support recreation and ecological protection.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	NROL.1.8	Encourage the use of native plant landscaping and street trees.	\$\$	Department of Public Works and Transportation	Short- to Mid-term
	NROL.1.9	Establish a tree planting and maintenance campaign throughout Western Greeley.	\$\$\$	Department of Public Works and Transportation	Short- to Mid-term



PARKS AND RECREATION

A truly walkable neighborhood features places to live, work, shop, dine, and recreate. The City of Greeley has already taken impressive steps to build and maintain parks, recreational facilities, and trails. This momentum can continued in Western Greeley.

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 1: Create safe, accessible, sustainable parks and public open spaces.	PR.1.1	Levy a variety of tools (e.g., land trusts, development standards for civic spaces, community-separator regulations) to secure land for protected and recreational use.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	PR.1.2	Develop new community parks, plazas, squares, and pocket parks so that most residents can easily access them on foot.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	PR.1.3	Require subdivisions and new communities to provide designed open spaces and, where possible, trail access.	\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	PR.1.4	In line with best urban design practices, shape parks with strong edges that balance permeability and security.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	PR.1.5	Design open spaces to host multi-use, environmentally friendly recreation activities.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	PR.1.6	Encourage the use and maintenance of native trees and ground cover to improve transparency and natural surveillance at park edges.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	PR.1.7	Where appropriate, integrate green infrastructure to improve water quality and groundwater recharge.	\$\$	Planning and Zoning; Department of Public Works and Transportation; Department of Water and Sewer	Short- to Mid-term
	PR.1.8	Site new civic buildings so that they have prominent locations in the city, especially as the terminated view at the end of streets, on axes across greens and parks, or in the center of greens and parks.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	PR.1.9	Distribute and incorporate public buildings within the new regional and neighborhood fabric of the Western Greeley, particularly within regional and neighborhood centers.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	PR.1.10	Locate public buildings within walkable, bikeable districts.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	PR.1.11	Design a new publicly accessible regional park for Western Greeley.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Mid-term



PUBLIC SAFETY

Any new development in Western Greeley must be adequately accessible to public safety services, like fire and police. Designated station placements requires an analysis of prime response times. The Westside Area Plan project presents a pivotal opportunity for the City's Fire and Police Departments to partner with Planning and Zoning and execute several goals and action items to ensure Western Greeley is safe for all.

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 1: Support responsive fire and law enforcement services that efficiently enhance public safety.	PS.1.1	Include representatives of emergency service providers in the development review process.	\$	Planning and Zoning; Fire Department; Police Department	Short-term
	PS.1.2	Continue to discuss the benefits of good urban design as it pertains to fire safety with fire safety officials.	\$	Planning and Zoning; Fire Department; Police Department	Short-term
	PS.1.3	Champion porous, two-way street networks that minimize response times.	\$\$	Planning and Zoning; Fire Department; Police Department	Short- to Mid-term
	PS.1.4	Continue siting emergency service stations at target locations of optimal response times.	\$\$	Planning and Zoning; Fire Department; Police Department	Mid-term



TRANSPORTATION & MOBILITY

As a primarily auto-centric environment, Greeley largely lacks the infrastructure required for walking to be useful, safe, comfortable, and interesting—and thus, as appealing as driving. The Westside Area Plan centers walkability and expanding multimodal options to cultivate a more connected, livable, human-scale city. The City can arrive at this version of Western Greeley by taking up specific goals and action items.

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 1: Create a multimodal transportation system that supports, complements, and fulfills the needs of different urban contexts throughout Western Greeley.	TM.1.1	Prioritize walkability through right-sized streets with 10-foot travel lanes, wide sidewalks, street trees, on-street parking, proper crosswalks, and stop signs instead of signals (where possible).	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	TM.1.2	Champion porous, two-way street networks that allow for a high intersection density.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	TM.1.3	Coordinate transit planning with land use: transit must be in proximity to high density, and high density must be in proximity to transit.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Mid-term
	TM.1.4	Construct transit stops that are safe, accessible, shaded, and seamlessly integrated into walkable streetscapes.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Mid-term
	TM.1.5	Focus on improving east-west connectivity between Western Greeley and Downtown through bicycle and transit infrastructure.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Mid- to Long-term

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
STRATEGY 2: Develop complete streets that form a safe, well-connected network for users—people driving, walking, and biking, with attention to vulnerable populations, including people with disabilities, those using mobility devices, the young, and the elderly.	TM.2.1	Enforce street design standards that provide safe, accessible, and meaningful travel choices for driving, walking, and bicycling.	\$	Planning and Zoning; Department of Public Works and Transportation	Short-term
	TM.2.2	Require new residential, commercial, and mixed-use developments to include a multi-modal network and maximize connectivity wherever possible.	\$	Planning and Zoning; Department of Public Works and Transportation	Short-term
	TM.2.3	Design streets as human-scale public spaces, with climate-appropriate street trees and landscaping.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	TM.2.4	Design complete streets with multimodal amenities suitable for roadway type and context.	\$\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	TM.2.5	Encourage small block size and connected streets and eliminate cul-de-sacs and dead ends in new residential developments.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	TM.2.6	Where optimal street connectivity cannot be provided, add pedestrian trails and shared-use paths to decrease walking and bicycling trip lengths.	\$\$	Planning and Zoning; Department of Public Works and Transportation; Department of Culture, Parks, and Recreation	Short- to Mid-term
STRATEGY 3: Offer safe, convenient infrastructure for people walking and biking.	TM.3.1	Create safe and convenient pedestrian and bicycle facilities that are universally accessible, adequately lit, and designed to reduce conflicts with motor vehicles.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	TM.3.2	Apply best practices in bikeway design when planning facilities.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Short- to Mid-term
	TM.3.3	Support infrastructure that allows students to walk or bike safely to school.	\$\$	Planning and Zoning; Department of Public Works and Transportation	Mid-term

STRATEGY	NO.	ACTION	CONCEPTUAL COST	RESPONSIBILITY	SEQUENCING
<p>STRATEGY 4: Secure a safe, convenient, and economically viable public transit system that optimizes personal mobility, strengthens community character and economic vitality, and seamlessly integrates with other travel modes.</p>	TM.4.1	Require major commercial and residential developments to provide adequate sidewalks and suitable areas for bus stops with bicycle storage.	\$-\$-\$	Planning and Zoning; Department of Public Works and Transportation; private developers	Short-term
	TM.4.2	Construct safe, comfortable, and attractive bus stops and transit stations. Non-motorized connections, such as sidewalks and bicycle routes/trails, will be the most important connections to stops and stations.	\$\$-\$\$\$	Department of Public Works and Transportation; Greeley Evans Transit (GET); CDOT (for state roads); private developers (when adjacent to stops)	Mid- to Long-term
	TM.4.3	Make transit-oriented development a priority along high-capacity transportation corridors to leverage transit investment to create mixed-use and income livable communities.	\$\$-\$\$\$	Planning and Zoning; Department of Public Works and Transportation; Economic Development and Urban Revitalization; Greeley Evans Transit (GET); CDOT; private developers	Long-term

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APPENDIX

ONLINE COMMUNITY SURVEY

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APPENDIX: ONLINE COMMUNITY SURVEY

ONLINE SURVEY

To help shape the Westside Area Plan, the City launched an online survey through Speak Up Greeley. The survey, which was active between July 26 and August 24, 2025, invited residents, community organizations, and businesses to provide input on growth priorities, neighborhood character, mobility needs, and infrastructure investments across Greeley's expanding western edge. The survey was available in both English and Spanish, ensuring accessibility for a broad range of respondents.

More than 1,235 participants viewed the Westside Area Plan project page, 231 of whom clicked on the survey page. 148 submitted responses to the survey. This high level of public interest demonstrates a community eager to participate in decisions that will shape Greeley's future. The survey's interactive format also allowed users to view resources, explore maps, and learn about planning concepts through videos and project documents, supporting informed participation.

The survey responses offered critical insights into how residents envision a growing Western Greeley. Key themes that emerged included strong support for diverse housing options, improved trail and pedestrian connectivity, enhanced parks and recreational amenities, and greater access to everyday services and neighborhood-scale destinations. Respondents also emphasized the importance of maintaining community identity, creating walkable mixed-use neighborhoods, and making development more sustainable and resilient.

Findings from the online survey directly influenced the Plan's land use strategies, placetypes, mobility priorities, economic development opportunities, and implementation actions. The appendix that follows features a detailed summary of survey results, illustrating how community feedback helped guide the Plan.

Project Report

26 July 2025 - 24 August 2025

Speak Up Greeley

Shape the Future of Greeley: West Subarea Plan



Visitors Summary



Highlights

TOTAL VISITS	MAX VISITORS PER DAY	
1.4 k	303	
NEW REGISTRATIONS		
4		
ENGAGED VISITORS	INFORMED VISITORS	AWARE VISITORS
148	231	1.2 k

Aware Participants	1,235	Engaged Participants	148		
Aware Actions Performed	Participants	Engaged Actions Performed	Registered	Unverified	Anonymous
Visited a Project or Tool Page	1,235	Contributed on Forums	0	0	0
Informed Participants	231	Participated in Surveys	6	0	142
Informed Actions Performed	Participants	Contributed to Newsfeeds	0	0	0
Viewed a video	6	Participated in Quick Polls	0	0	0
Viewed a photo	0	Posted on Guestbooks	0	0	0
Downloaded a document	14	Contributed to Stories	0	0	0
Visited the Key Dates page	0	Asked Questions	0	0	0
Visited an FAQ/Help Page	24	Placed Pins on Places	0	0	0
Visited Instagram Page	0	Contributed to Ideas	0	0	0
Visited Multiple Project Pages	85				
Contributed to a tool (engaged)	148				

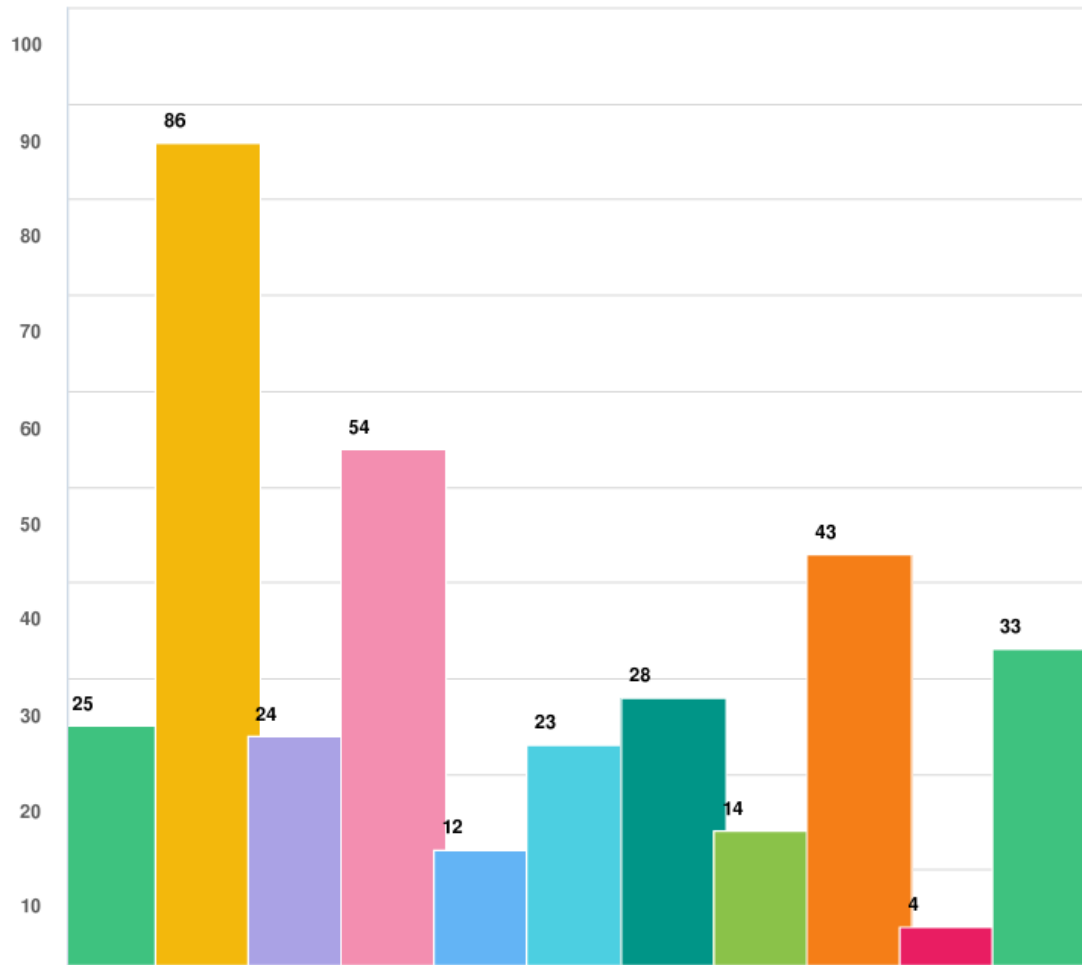
Speak Up Greeley : Summary Report for 26 July 2025 to 24 August 2025

ENGAGEMENT TOOL: SURVEY TOOL

Community Survey Questions for Subarea West Planning

Visitors 1046	Contributors 148	CONTRIBUTIONS 150
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Which of the following aspects of living in Greeley do you value most? Please select your top three.



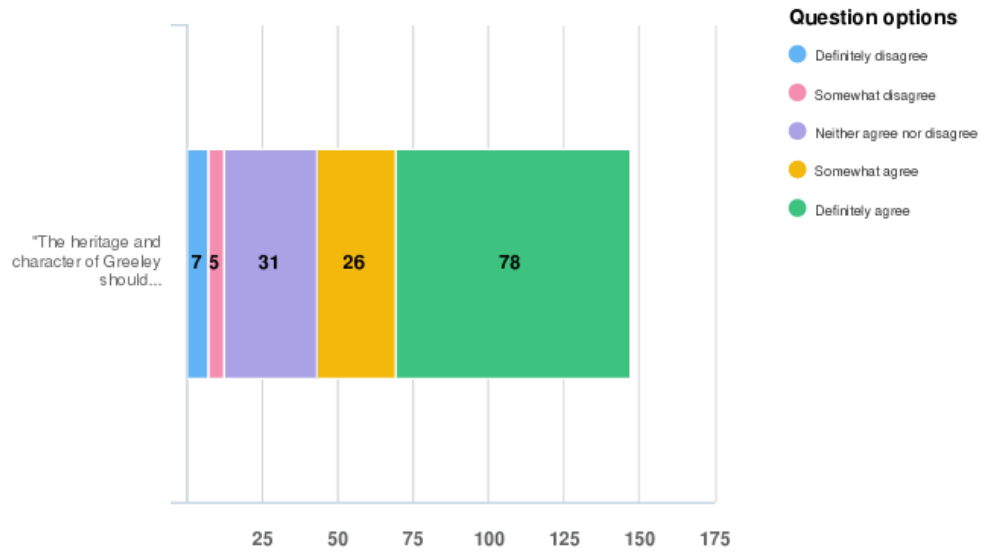
Question options

- Community Engagement
- Natural beauty, parks, green spaces, and trails
- Cultural diversity
- Access to amenities – shopping, retail, grocery, dining
- Transportation access
- Entertainment
- Outdoor recreation
- Water parks, pools and splash pads
- Access to health care
- Available childcare
- Housing options and affordability

Optional question (137 response(s), 13 skipped)

Question type: Checkbox Question

To what extent do you agree with the following statement:

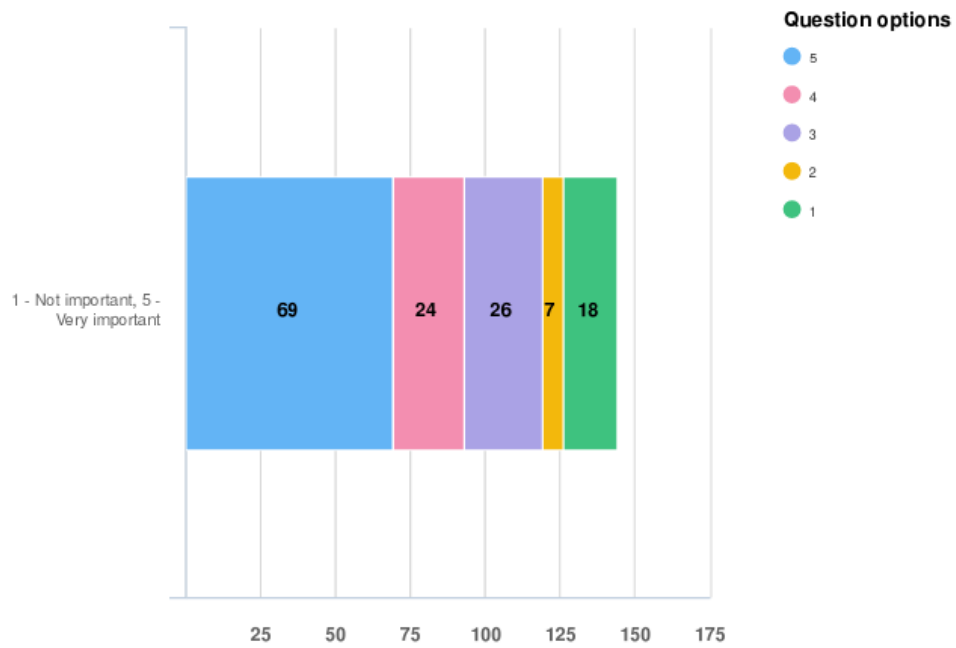


Optional question (147 response(s), 3 skipped)

Question type: Likert Question

Speak Up Greeley : Summary Report for 26 July 2025 to 24 August 2025

On a scale of 1 to 5, how strongly do you feel about celebrating western Greeley's agricultural roots?

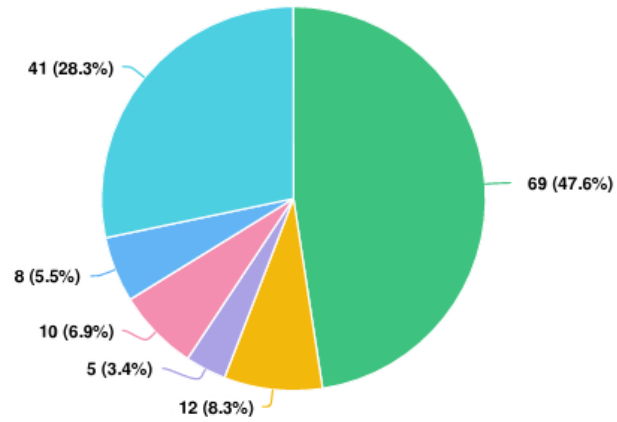


Optional question (144 response(s), 6 skipped)

Question type: Likert Question

Speak Up Greeley : Summary Report for 26 July 2025 to 24 August 2025

What types of housing do you believe are most needed in western Greeley?



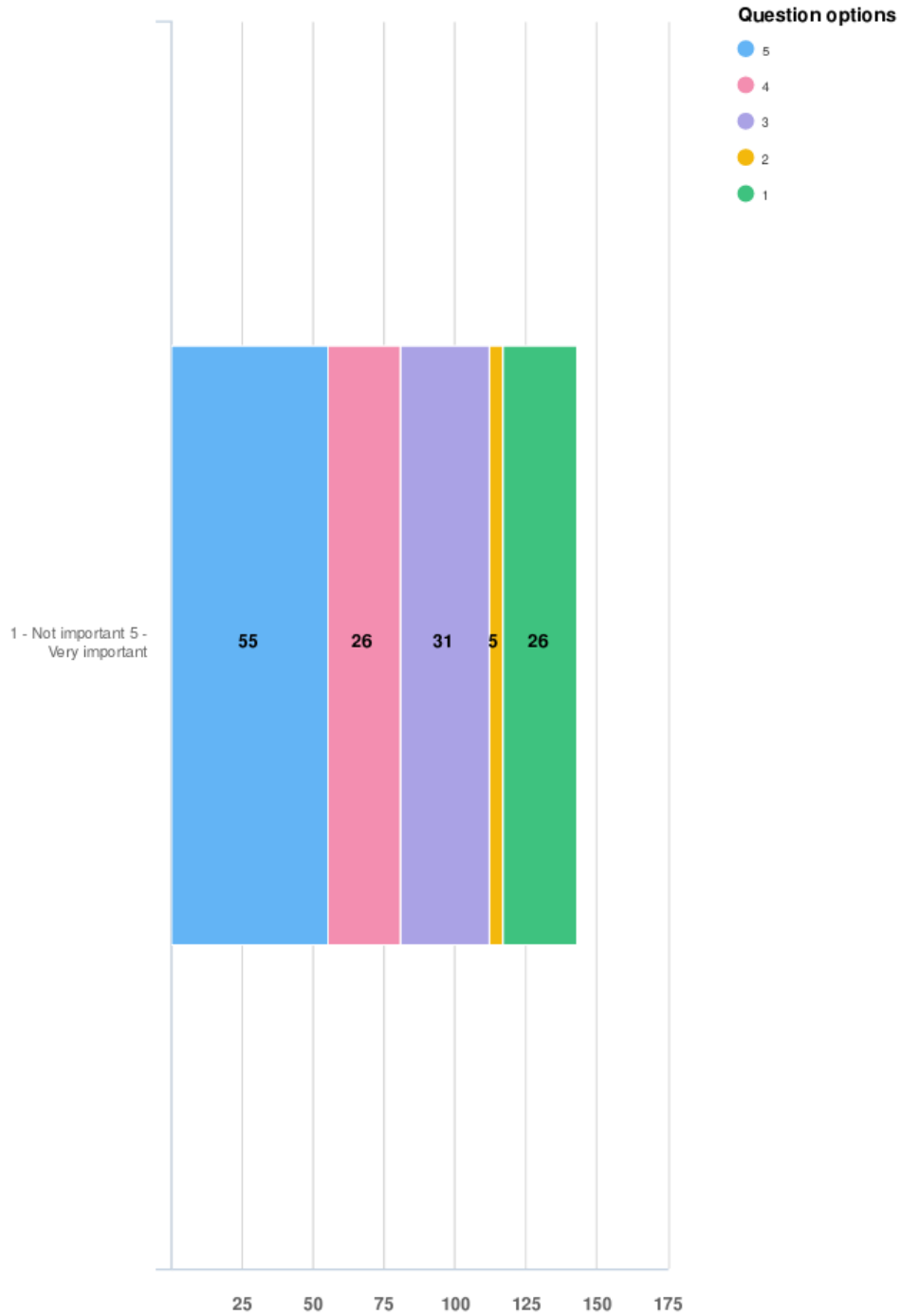
Question options

- Other (please specify)
- Executive level housing
- Duplex/Triplex/Fourplex
- Apartments
- Townhomes
- Single-family detached homes

Optional question (145 response(s), 5 skipped)

Question type: Radio Button Question

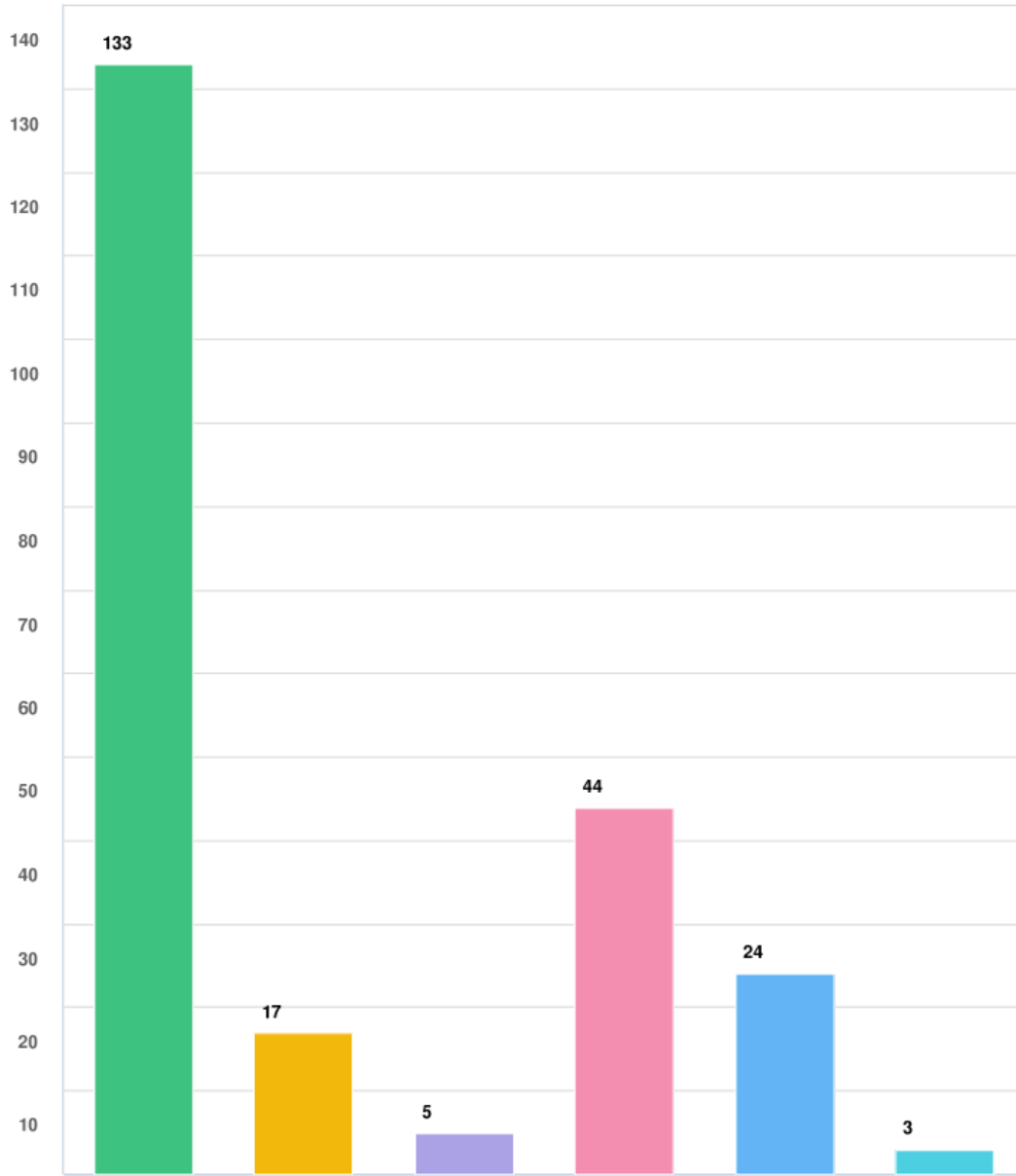
On a scale of 1 to 5, how important is it that new housing is located within walking distance of parks, shops, and schools?



Optional question (143 response(s), 7 skipped)

Question type: Likert Question

How do you normally get around Greeley? (Select all that apply)



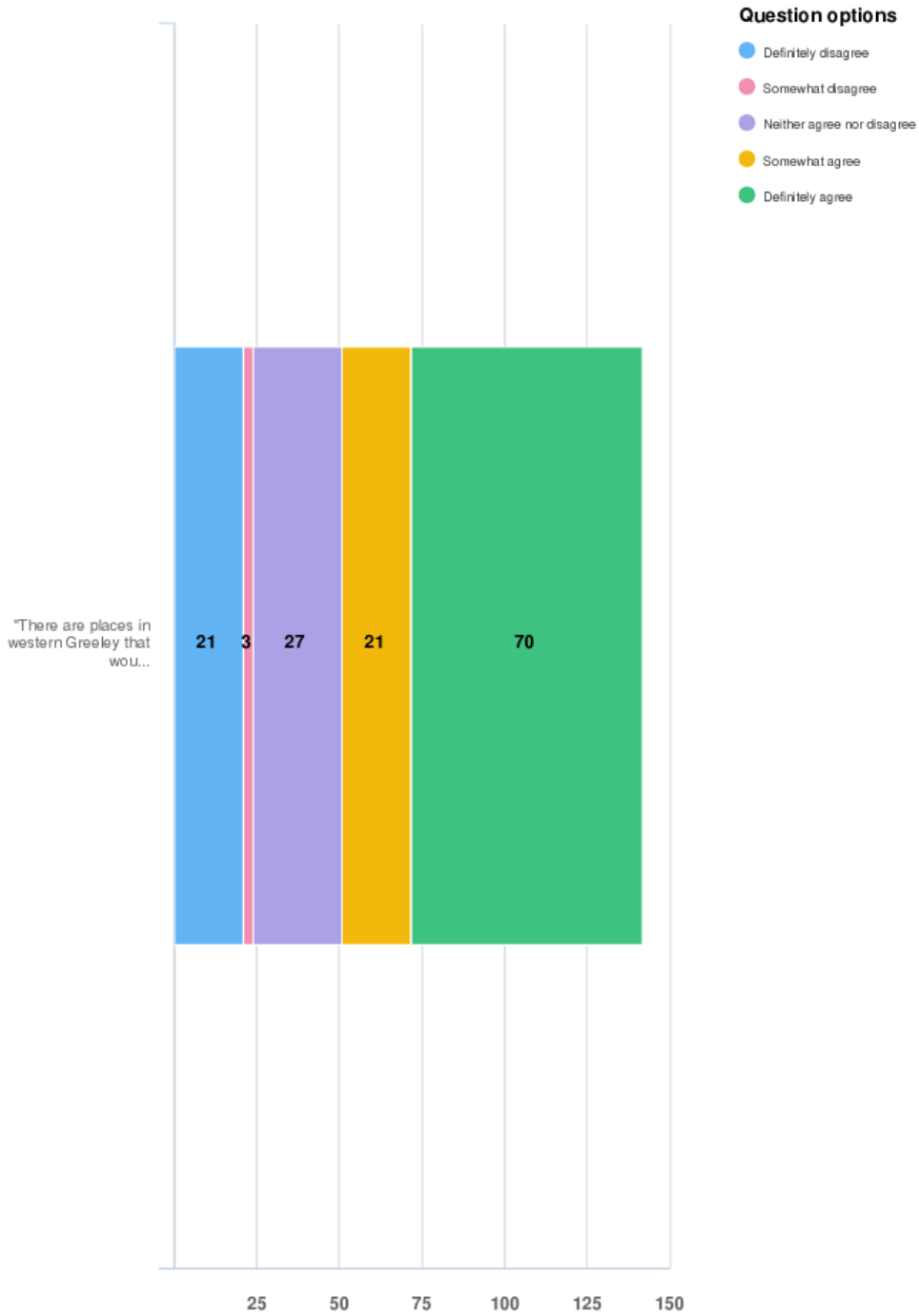
Question options

- Other (please specify)
- Bike
- Walk
- Public transport
- Carpool
- Drive alone

Optional question (146 response(s), 4 skipped)

Question type: Checkbox Question

Please indicate how strongly you agree with the following statement:



Greeley, CO

GREELEY WESTSIDE FORM-BASED CODE

January 2026

(Draft)



CREDITS

Prepared for:



Planning Team:



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DIVISION 1: GENERAL PROVISIONS

This Code's purpose, applicability and rules of interpretation are laid out in this Division.



SECTION 1.1 PURPOSE

The Greeley Westside Form-Based Code (FBC) is a form-based code that implements the mixed-use vision for Greeley Westside Area by:

- A. Creating the Greeley Westside Form-Based Code Zoning District.
- B. Providing the standards and review procedures necessary to ensure that the Greeley Westside development program established in the Greeley Westside Plan is achievable.
- C. Providing for standards of development through the implementation of the form-based code.
- D. Providing for the organization of development through the establishment of Transect Zones.

SECTION 1.2 CREATION OF THE GREELEY WESTSIDE FORM-BASED CODE

- A. The Greeley Westside Form-Based Code is hereby created and shall apply to the Greeley Westside Area as defined by the Figure 2-1: Placetypes Plan - Regulatory map for the FBC.
- B. The FBC will provide a compact, pedestrian-oriented development and a predictable built environment with a mix of commercial and residential uses.
- C. By providing a compact, walkable, mixed-use community, the FBC will help in meeting the varied housing needs of the Greeley Westside Area and allow arroyos and other open space areas to be preserved.
- D. Traditional Neighborhood Design (TND) will be used in the FBC to achieve efficient transportation corridors, walkable Streets, and an interconnected trail system that provides connectivity between natural areas, open space parks, civic spaces and neighborhood, and workplace areas.

SECTION 1.3 APPLICABILITY

- A. All properties within the Greeley Westside FBC boundary as shown in Figure 2-1: Placetypes Plan - Regulatory map for the FBC may utilize the Greeley Westside FBC.
- B. Rezoning is not required to develop or redevelop according to the Greeley Westside FBC if the land is within the Greeley Westside Area as designated by Figure 2-1: Placetypes Plan - Regulatory Map of this FBC and if the proposed development complies with all regulations in this code.
 - 1. Compliance of an application and its supporting documentation will be confirmed by issuance of development approvals using the process described in Article 7. The Approval Authority may authorize administrative deviations in accordance with this FBC and Division 7.
- C. Once a property has utilized, or opted into using the Greeley Westside FBC, no building or structure shall be erected, altered, or used for any purpose except as provided for within this FBC.
- D. All development within the Greeley Westside Area shall also comply with underlying Land Development Regulations (LDR) unless an exception to the requirements of those codes is expressly provided. To the extent that a conflict occurs between this FBC and other LDR provisions, the provisions of this FBC shall control.

SECTION 1.4 RULES OF INTERPRETATION

- A. The words “must”, “shall” and “will” are mandatory.
- B. The words “may” and “should” are permissive.
- C. Capitalized terms used in this FBC are defined in Division 8, Glossary of Terms. Words or phrases that are not capitalized and defined in Division 8, Glossary of Terms, shall be construed according to their customary meaning.
- D. In the event of a conflict between numerical metrics (i.e., measurements) and graphic metrics (i.e., pictures), the numerical metrics shall control.
- E. Where the provisions of this FBC require calculations to determine applicable requirements, any fractional results shall be rounded up if the number being rounded is followed by 5, 6, 7, 8, or 9. If the number being rounded is followed by 0, 1, 2, 3, or 4, the fractional result shall be rounded down.

DIVISION 2: PLACETYPE STANDARDS

Different combinations of Transect Zones shape a wide range of placetypes, from dense, mixed-use regional centers to compact town centers, local main streets, and small rural crossroads. A higher proportion of urban transects (T5 and SD-Workplace) supports taller buildings, transit, and diverse activity, while a mix of general urban and suburban transects (T3–T5) creates walkable town centers and neighborhood main streets. When rural and edge transects (T1–T3) dominate, the result is a more dispersed, landscape-oriented setting typical of village crossroads or rural hamlets. The balance of transects thus determines each place’s scale, intensity, and character.

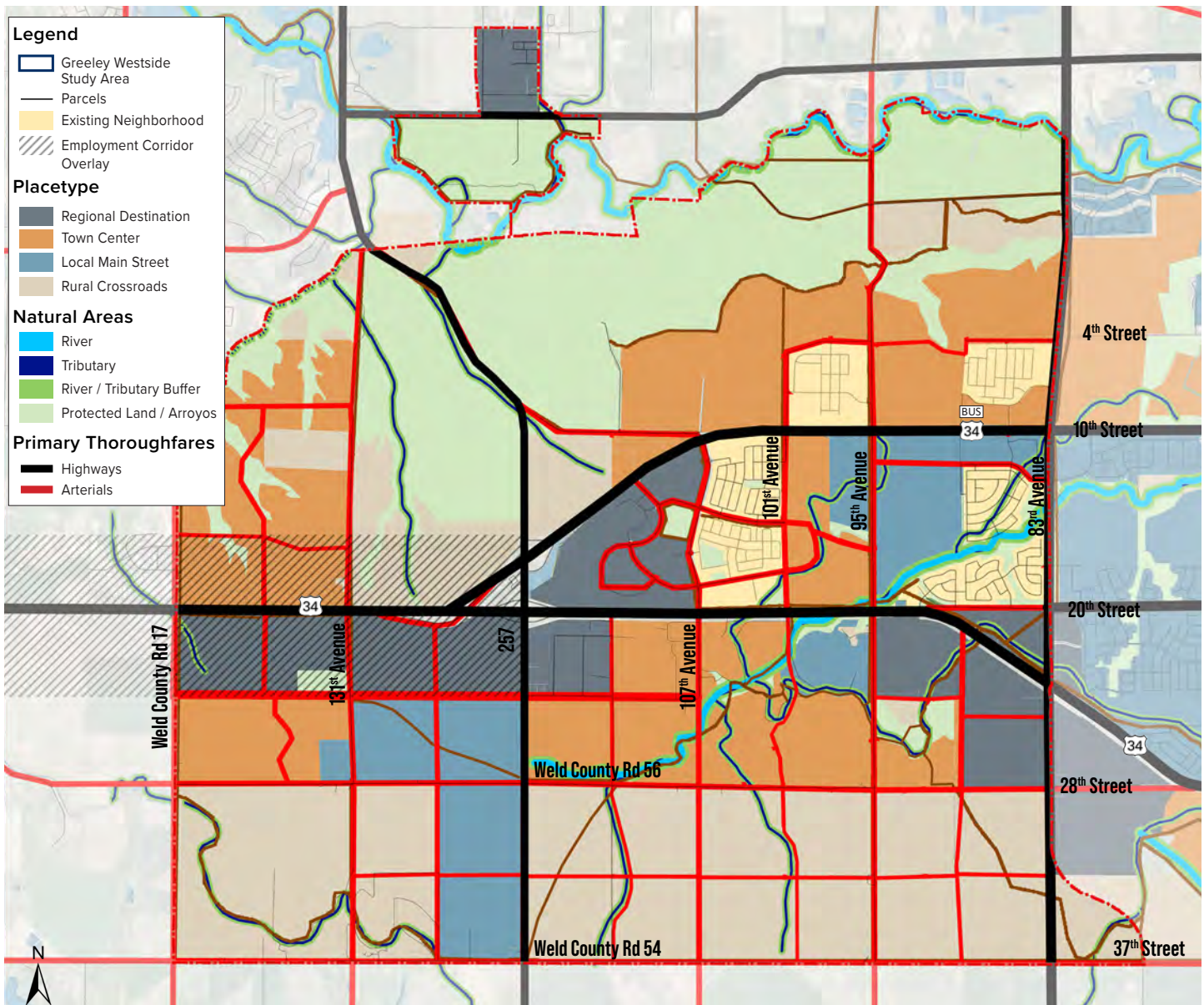
Eight Transect Zones have been created for use within the Greeley Westside Subarea, each with varying urban characteristics. All are calibrated to align with the envisioned future context of mixed-use walkable urbanism.



SECTION 2.1 PLACETYPES PLAN

A. Placetypes Regulating Plan

1. Development planning within the Greeley Westside Subarea begins with the creation of the Placetypes Plan (Figure 2-1). Development within the Greeley Westside Subarea shall be consistent with the Placetypes Plan.
2. The Placetypes Plan establishes the general location, size and type of neighborhoods, as well as other important elements that define the overall structure of The Greeley Westside Subarea.
3. The Placetypes Plan illustrates the following:
 - a. Placetypes general boundaries;
 - b. Arroyo and natural preserve areas;
 - c. Primary Thoroughfares.
4. The Placetypes boundaries shown on the Placetypes Plan and the location of primary Thoroughfares are subject to adjustment pursuant to applicable amendment provisions.



Not to Scale

Figure 2-1: Placetypes Plan - Regulatory map for the FBC.

SECTION 2.2 GENERAL NEIGHBORHOOD STANDARDS

B. Placetypes

1. The following Placetypes are hereby established: Regional Center, Town Center, Local Main Street, and Rural Crossroads.
2. Each Master Site Plan submitted shall identify its placetype and contain a mix of Transect Zones, as established in Table 2-1 Placetype Standards, corresponding to its designated Placetypes in Figure 2-1, with the following exceptions:
 - a. The allocation of Transect Zones standards, with the exception of the Open Space requirement, do not apply to Master Site Plans less than five (5) acres in size.
 - b. Any Master Site Plan may consist of 100 percent T1, T2, or C Transect Zones, or a combination thereof.

C. Adjustments

1. Placetype boundaries may be adjusted up to 150 feet through administrative approval.

- A. Each Placetype shall assign at least 10 percent of its area to Civic Open Space and/or T1.
- B. Figure 2-1 contains recommendations on how large, contiguous Open Spaces can be created across independently owned tracts of land.
- C. Property owners should work with adjacent property owners and the City to create the larger parks.
- D. Each Placetype shall contain at least one Primary Civic Open Space (not a Playground), and that Civic Open Space shall be within 800 feet of the geographic center of the Neighborhood.
- E. Each Placetype shall have at least one Playground.

	Regional Center	Town Center	Local Main Street	Rural Crossroads
TABLE 2-1: PLACETYPE STANDARDS				
General Standards				
Neighborhood Size	45 - 80 acres	60 - 160 acres	50 - 160 acres	80 - 180 acres
Max. Average Block Perimeter Per Master Site Plan	3,000 ft	2,000 ft	2,000 ft	2,400 ft
Allocation of Transect Zones Per Neighborhood (Gross Area)				
T1: Natural	no min.	no min.	no min.	no min.
T2: Rural	no min.	no min.	no min.	25% min.
T3: Neighborhood Edge	no min.	5 - 30%	10 - 40%	20% min.
T4-R: Neighborhood General Restricted	5 - 25% (Mix of R / O)	30 - 60% (Mix of R / O)	20 - 40%	0 - 10%
T4-O: Neighborhood General Open			10 - 30%	0 - 10%
T5: Mixed-Use Center	5 -25%	10 - 50% ¹	0 - 15%	not permitted
SD-W: Special District - Workplace	80% maximum	not permitted	not permitted	not permitted
C: Civic	10%	10%	10%	10%

Notes:

1. At least 500 linear feet of Frontage shall be ground floor Office or Retail use with a Shopfront Frontage Type per Placetype
2. Land with a T1 Transect Zone designation may contribute to the Civic Open Space requirement.
3. Allocation of Transect Zones standards (except for Open Space) do not apply to Master Site Plans less than five (5) acres in size.

SECTION 2.3 EMPLOYMENT CORRIDOR (EC) OVERLAY

A. Purpose and Intent

The Employment Corridor (EC) Overlay is established to implement the City's long-term economic development and intergovernmental planning objectives by preserving areas intended primarily for employment-generating uses. The EC Overlay is intended to:

1. Ensure that development within designated Employment Corridor areas maintains a predominantly employment-based land use pattern over time;
2. Support high-quality workplace, research, institutional, and industrial environments that benefit from coordinated infrastructure and regional access;
3. Allow limited, complementary commercial and mixed-use development that supports employment uses without undermining the primary employment function; and
4. Provide additional standards that work in conjunction with the underlying Placetype and Transect standards, without replacing or duplicating them.

B. Applicability

1. The EC Overlay shall apply only to properties shown within the Employment Corridor Overlay boundary on the Placetypes Regulating Plan.
2. The EC Overlay applies in addition to the underlying Placetype and Transect Zone standards.
3. Where the provisions of the EC Overlay conflict with the underlying Placetype or Transect standards, the provisions of the EC Overlay shall control.

C. Relationship to Placetypes

1. Properties within the EC Overlay may continue to utilize any Placetype otherwise permitted by the Placetypes Regulating Plan, including Regional Center and Special District – Workplace (SD-W).
2. The EC Overlay does not alter the permitted Transect Zones or their dimensional or form standards, except as expressly modified by this section or by referenced provisions of this Code.

D. Employment Primacy Requirement

1. Development within the EC Overlay shall maintain a predominance of employment-generating uses.
2. Each Regulating Plan, Master Site Plan, or equivalent development approval within the EC Overlay shall demonstrate that no less than sixty-five percent (65%) of the total gross floor area is devoted to employment-generating uses, including but not limited to:
 - a. Office and research uses;
 - b. Institutional and civic employment uses;
 - c. Manufacturing, production, and industrial uses; and
 - d. Other non-residential uses that generate primary employment.
3. Residential uses, retail, food and beverage, lodging, and entertainment uses may be permitted within the EC Overlay only as allowed by the underlying Placetype and Transect standards and subject to the limitations of this section.

E. Limitations on Non-Employment Uses

1. Retail, food and beverage, and personal service uses within the EC Overlay shall be:
 - a. Accessory to, or supportive of, employment uses; and
 - b. Limited to a maximum of thirty percent (30%) of total gross floor area within a Regulating Plan or Master Site Plan.
2. Residential uses, where permitted by the underlying Placetype, shall not exceed thirty percent (30%) of total gross floor area within a Regulating Plan or Master Site Plan, unless otherwise approved by Warrant pursuant to Division 7.
3. Specific use prohibitions or additional use limitations applicable within the EC Overlay are set forth in Division 3 (Transect Standards and Permitted Uses).

SECTION 2.4 SECONDARY COOPERATIVE LAND USE ANNEXATION AND UTILITY AREA

F. Corridor-Oriented Site Planning

1. Development within the EC Overlay shall be arranged to support a cohesive corridor pattern, including:
 - a. Buildings oriented to public or private streets rather than parking areas;
 - b. Shared access and internal circulation between adjacent parcels where feasible; and
 - c. Placement of surface parking, loading, and service functions to the side or rear of buildings.
2. Additional access management, frontage, or thoroughfare requirements applicable within the EC Overlay are established in Division 6 (Thoroughfares).

G. Regulating Plan Requirements

1. All Regulating Plans within the EC Overlay shall include an Employment Mix Summary demonstrating compliance with the employment percentage requirements of this section.
2. The Regulating Plan may include a Special Requirements Plan pursuant to Section 2.5 to further refine corridor-specific frontage, access, or building placement standards.
3. Approval of a Regulating Plan within the EC Overlay shall constitute a determination that the proposed development pattern satisfies the intent of the Employment Corridor.

H. No Expansion of Permitted Uses

Nothing in this section shall be construed to expand the list of permitted uses beyond those otherwise allowed by the underlying Placetype, Transect Zone, or Division 3 of this Code.

I. Long-Term Applicability

The EC Overlay is intended to provide long-term protection of employment-focused areas. The overlay boundary may be amended only through a legislative amendment to the Placetypes Regulating Plan, consistent with adopted plans and policies.

- A. Properties located within the Secondary Cooperative Land Use Annexation and Utility Area, as established by intergovernmental agreement, are intended to accommodate a broader mix of residential, commercial, mixed-use, and employment-supportive development than areas designated as the Primary Cooperative Land Use Annexation and Utility Area. Development within the Secondary Cooperative Area shall be governed by the underlying Placetypes and Transect standards of this Code. No additional overlay or special district standards apply to the Secondary Cooperative Area unless expressly adopted. Annexation, utility extension, and development approvals within the Secondary Cooperative Area shall be coordinated and reviewed in a manner consistent with the applicable intergovernmental agreement.

SECTION 2.5 REGULATING PLANS

- A. A Regulating Plan is a site plan that describes the varying character of land within a Placetype or Neighborhood development, or fragment thereof, within the Greeley Westside Subarea. Regulating Plans designate a Transect Zone for all development parcels within the Greeley Westside Subarea and Street Types that describe the design of neighborhood Streets. The Transect Zones and Street Types correspond with standards in other code sections.
- B. Regulating Plans identify the assignment of Transect Zones and the Street/Block structure, along with the subdivision of Lots according to the Transect Standards (Division 3) and identification of specific Street Types (Division 6).

C. Purpose of Regulating Plans

1. Regulating Plans define with precision the nature of allowable development of land. Regulating Plans are prepared by landowners in accordance with Division 7 and submitted to the Approval Authority through the approval processes described in that Section.

D. Regulating Plan Requirements

Submittals to obtain approval of a Regulating Plan must meet the following criteria:

1. The Regulating Plan shall demonstrate conformance to all provisions of the Placetype (Section 2.1 and Section 2.2).










2. The Regulating Plan must show future Blocks, Transect Zones, proposed Lot Lines, Streets and public spaces as follows:

- a. **New Blocks configured on the site, in accordance with the requirements of the Placetype** (Section 2.1 and Section 2.2).
- b. **Proposed Lot Lines for all developable Lots, in accordance with the Transect Standards** (Division 3). Lots may be legally subdivided for the purposes of demonstrating conformance to the standards in this code.
- c. **The assignment of a Transect Zone to all proposed Lots on the site.** The assignment of Transect Zones shall meet the provisions of the Placetype (Section 2.1). All proposed Lot area must be assigned to one of the eight Transect Zones permitted: T1, T2, T3, T4-R, T4-O, T5, SD-W, and C; no proposed Lot may be assigned two or more Transect Zones. Transect Zone boundaries should follow proposed Lot Lines. Proposed development on individual Lots must be able to meet the assigned Transect Standards.

d. **The location of all new and existing Rights-of-Way in Streets and Alleys/Rear Lanes.**

New Streets and Alleys/Rear Lanes shall be dedicated to the City and publicly owned. The plan must indicate a specific Street Type; all new and improved Streets must adhere to the dimensional standards of the Street Types in Division 6. Street types must be allowed within the Transect Zones through which they pass. To the extent that a conflict occurs between Division 6 of this FBC and the City's subdivision regulations, along with other LDRs, the provisions of this FBC shall control.

- 3. The Regulating Plan shall identify the residential density per Transect Zone and demonstrate adherence to the density requirements of Table 3-1.
- 4. The Regulating Plan may show site-specific standards of Section 2.5 Special Requirements Plan that apply to the site, including build-to lines, mandatory storefront areas, and/or Terminated Vistas.

Legend	
	Property Line / Placetype Boundary
Transect Zones	
	T5 Mixed-Use Center
	T4-O Neighborhood General Open
	T4-R Neighborhood General Restricted
	T3 Neighborhood Edge
	T2 Rural
	T1 Natural
	SD-W Special District - Workplace
	C Civic

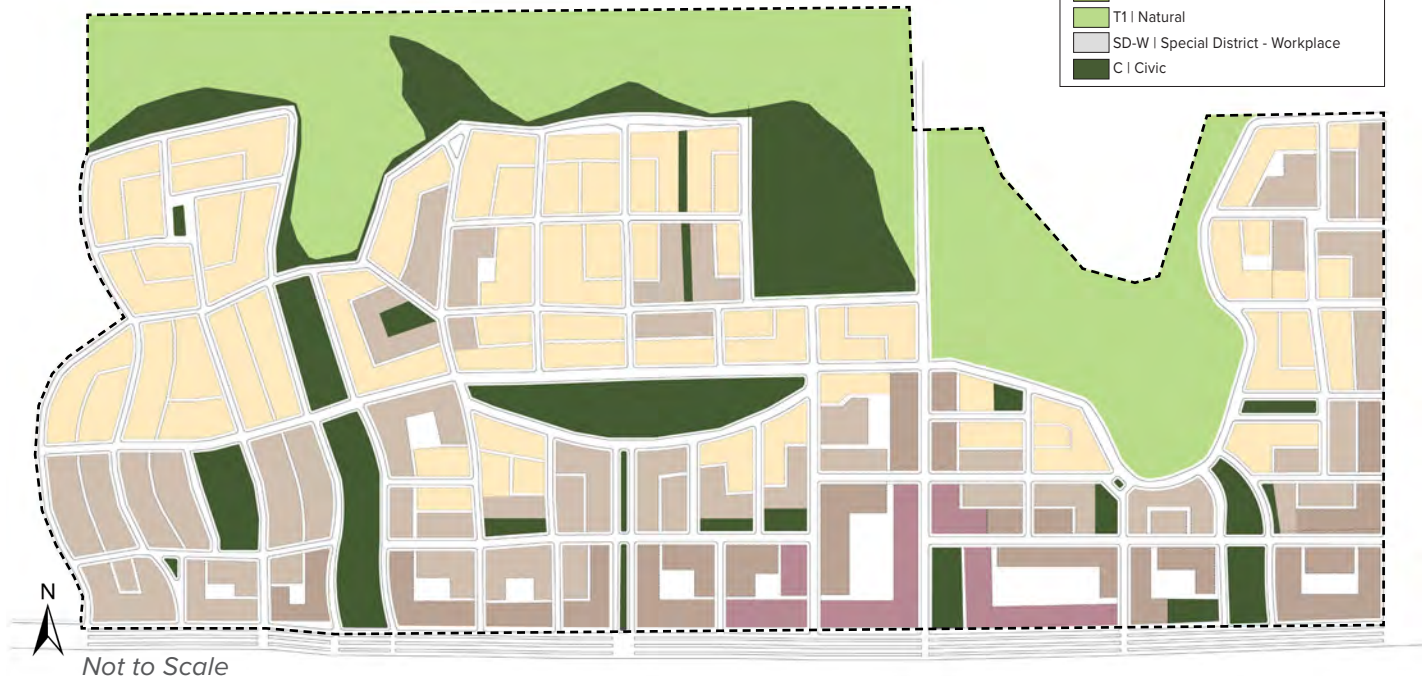


Figure 2-2: Sample Regulating Plan for Town Center Placetype

- 5. The level of detail and graphic format of the Regulating Plan must show individual Lot Lines and identify Street types. The plan should be produced at a scale and sheet size that allows all elements of the plan to be clearly legible. All related submittals must be provided at the same scale to facilitate review. The Regulating Plan must also be provided in a digital format acceptable to the Approval Authority.
- 6. Approved Regulating Plans must be kept on file with the City’s Zoning Administrator.

SECTION 2.6 BLOCK STRUCTURE

- A. To facilitate connectivity and pedestrian accessibility, the Blocks within the Greeley Westside Subarea shall conform to the following standards:
 - 1. Maximum Block sizes shall not exceed the maximum Block perimeter established for each Transect Zone.
 - 2. The average perimeter of all Blocks in a Master Site Plan shall not exceed the maximum average Block perimeter established in Table 2-1 Placetype Standards for the designated Placetype.
 - 3. Any Block face within the T5, T4-O, T4-R or T3 Transect Zones that exceeds 600 feet in length shall have a mid-block pedestrian access of at least 12 feet in width.

SECTION 2.7 SPECIAL REQUIREMENTS PLAN

A. Special Requirements Plan

A Special Requirements Plan is an optional site plan that provides further refinement of the Regulating Plan by requiring or recommending particular regulations in site-specific locations.

B. Special Requirements Plan Elements

A Special Requirements Plan within the Greeley Westside Subarea may designate any of the following Special Requirements:

- 1. **Differentiation of Thoroughfares:** A differentiation of the Thoroughfares as A-Grid and B-Grid. Buildings along the A-Grid shall be held to the highest standard of this Code in support of pedestrian activity. Buildings along the B-Grid may be more readily considered for Warrants allowing automobile-oriented standards. The Frontages assigned to the B-Grid shall not exceed 30% of the total length of Frontages within a Neighborhood Unit.
- 2. **Build-to-Line:** A Build-to-Line designates a specified distance from the front Property Line that the building’s Front Façade shall be built upon in order to create a uniform line of buildings along the Street. The Build-to-Line marked on the Regulating Plan shall take priority over the more general Build-to-Zone defined in Division 3.
- 3. **Mandatory Shopfront:** Designations for Mandatory and/or recommended Shopfront Frontage, requiring or advising that a building provide a Shopfront at sidewalk level along its Lot Frontage. See requirements for storefronts in Division 5 Lot & Building Standards.
- 4. **Gallery/Arcade Frontage:** Designations for Mandatory and/or Recommended Gallery/Arcade Frontage, requiring or advising that a building provide a permanent cover over the sidewalk, either cantilevered or supported by columns. The Gallery Frontage designation may be combined with a Retail Frontage designation.
- 5. **Terminated Vista:** Designations for mandatory and/or recommended Terminated Vista locations, requiring or advising that the building be provided with architectural articulation of a type and character to mark an important view, assist with wayfinding, and add to sense of place. See requirements in Division 5 Lot & Building Standards.
- 6. **Cross Block Passages:** A designation for cross Block Passages, requiring that a minimum 12-foot-wide pedestrian access be reserved between buildings.

DIVISION 2

PLACETYPE STANDARDS

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DIVISION 3: TRANSECT STANDARDS

The Transect Standards specify the desired character and development forms found along Streets and public spaces and prescribe the physical attributes of new development. Standards of this section are mapped on Regulating Plans as described in Division 2.



SECTION 3.1 PURPOSE, INTENT AND GENERAL STANDARDS

- A. This section establishes Transect Zones that are applied to property within the Greeley Westside Subarea, as illustrated on the Placetypes Regulating Plan (Division 2, Figure 2-1).
- B. The Transect is a planning and zoning tool that organizes zones in a continuum from rural to urban, referred to as T1, T2, T3, T4-R, T4-O, and T5. Two additional zones are the Special District - Workplace (SD-W) and Civic zones (Open Space and Buildings), which cover building types and uses that do not fit into any of the previous categories. Each Transect Zone has a different set of characteristics that correspond with building placement, building form, and Frontage standards, all of which influence the placetype. Transect Zones are applied at the Master Plan level and remain consistent throughout development planning.
- C. Within each Placetype, each T3, T4-R, T4-O, and T5 Transect Zone shall contain at least three different permitted residential building types. The permitted residential building types for each Transect Zone are established in Table 3-2 in Section 3.13 Permitted Uses.
- D. For those portions of the Greeley Westside Subarea that are not within a Neighborhood Unit, only the T1, T2, and Civic Transect Zones can be applied.

E. General Standards

- 1. Precedent images are for illustrative purposes only to demonstrate the intent of the standards. They are provided as examples and shall not imply that every element in the image is permitted.
- 2. The allocation of Transect Zones is based on net area allocated Transect Zones within each Master Site Plan.
- 3. The percentage of Open Space is derived using the gross developable area of a Master Site Plan and does not include undevelopable areas.
- 4. Residential density is measured for the total net acres allocated to each Transect Zone within each Neighborhood Unit. The net acre calculation does not include Right-of-Ways.
- 5. Accessory Dwelling Units do not constitute a separate unit for the purpose of calculating residential density.

SECTION 3.2 TRANSECT ZONES

- A. Standards for the Placetypes are provided in Table 2-1 in Division 2.
- B. Standards for each of the Transect Zones are shown for comparison in Table 3-1. Each zone is further described in Sections 3.4 through 3.11.

SECTION 3.3 NATURAL AREAS

- A. The T1 Natural Zone within the flood plains and arroyos and their buffers shall be protected as Open Space and transferred to the City of Greeley. The buffers shall extend at least 100 feet from the outer edge of the 100-year floodplain. For tributaries, the buffer shall be at least 50 feet. Development within this buffer shall be limited to trail and Street crossings or bridges, irrigation infrastructure, and where necessary, those improvements necessary for life safety or emergency purposes.

TABLE 3-1: TRANSECT STANDARDS SUMMARY

	T1	T2	T3	T4-R	T5	SD-W	C
				T4-O			
Residential Density							
Minimum, By-right ^{1,2}	n/a	1 unit / 20 ac.	6 units / ac.	12 units / ac.	24 units / ac.	n/a	n/a
Maximum, By-right ^{1,2}	n/a	1 unit / 20 ac.	8 units / ac.	24 units / ac. (R)	36 units / ac.	48 units / ac.	n/a
Building Placement							
Front Build-to-Zone, or Setback	n/a	20' min.	20' min., 48' max.	6' min., 18' max.	0' min., 10' max.	6' min., 40' max.	0' min.
Front Street Frontage Buildout	n/a	n/a	40% min.	60% min.	80% min.	25% min.	By Warrant
Side Street Frontage Buildout	n/a	n/a	n/a	30% min.	40% min.	20% min.	By Warrant
Side Street Build-to-Zone	n/a	20' min.	12' min.	6' min. to 18' max.	0' min., 10' max.	6' min., 40' max.	0' min.
Interior Side Property Line Setback	n/a	30' min.	8' min.	0' (Attached) 5' (Detached)	0' min.	5' min. ⁶	0' min.
Rear Setback (Lot or Alley)	n/a	30' min.	12' min.	5' min.	5' min.	5' min. ⁶	0' min.
Lot and Block Standards							
Maximum Block Perimeter	n/a	By Warrant	2,400 linear ft	2,000 linear ft	2,000 linear ft ³	3,000 linear ft	n/a
Lot Width	n/a	By Warrant	50' min.	18' min., 100' max.	18' min., 180' max.	No min., No max.	n/a
Lot Depth	n/a	By Warrant	110' min.	80' min.	30' min.	No min., No max.	n/a
Lot Coverage	n/a	By Warrant	60% max.	70% max.	90% max.	80% max.	By Warrant
Building Heights							
Principal Building	n/a	2.5 Stories max.	2.5 Stories max.	3 Stories max.	5 Stories max.	6 Stories max.	By Warrant
Ground Floor Elev. Above Sidewalk ⁴	n/a	0' min.	24" min.	6" max. (Non-Res.) 24" min. (Res.)	6" max. (Non-Res.) 24" min. (Res.)	n/a (Non-Res.) 24" min. (Res.)	By Warrant
Ground Floor Ceiling Height	n/a	9' min.	9' min.	12' min. (Non-Res.) 9' min. (Res.)	12' min. (Non-Res.) 9' min. (Res.)	14' min.	By Warrant
Upper Floor(s) Ceiling Height	n/a	9' min.	9' min.	9' min.	9' min.	14' min. (Non-Res.) 9' min. (Res.)	By Warrant
Parking Location⁵							
Front Setback	By Warrant	30' min.	30' min.	30' min.	30' min.	40' min.	By Warrant
Side Street Setback	By Warrant	12' min.	12' min.	6' min.	5' min.	20' min.	By Warrant
Interior Side Property Line Setback	By Warrant	12' min.	8' min.	0' (Attached) 5' (Detached)	5' min. 0' min. (When Adjacent to Parking)	10' min. 0' min. (When Adjacent to Parking)	By Warrant
Rear Setback	By Warrant	30' min.	5' min. 0' min. (When Adjacent to Alley)	5' min. 0' min. (When Adjacent to Alley)	5' min. 0' min. (When Adjacent to Alley)	5' min.	By Warrant
Allowed Frontages & Encroachments							
Allowed Frontage Types	n/a	Common Yard, Porch	Common Yard, Porch	T4-O only: Shopfront, Forecourt, Arcade, Gallery T4-O & T4-R: Porch, Stoop, Lightwell, Common Yard	Shopfront, Forecourt, Gallery, Arcade, Stoop, Lightwell, Porch	n/a	n/a
Other Allowed Encroachments	n/a	n/a	Balconies, Bay Windows, Awnings, and Other Frontage Elements			n/a	n/a
Front Setback	n/a	n/a	8' max.	6' T4-O / 3' T4-R		8' max.	n/a
Side Street Setback	n/a	n/a	8' max.	3' max.		4' max.	n/a
Rear Setback	n/a	n/a	3' max.	3' max.		0' max.	n/a

Notes:

1. See Section 3.1.D for more information about density requirements.
2. Civic Buildings and Open Space are provided additional flexibility. Standards for this Transect Zone can be found in Sections 3.11.
3. 3,000 linear ft max. with parking structure
4. Lobbies for multi-family residential buildings shall have a 6" max. ground floor elevation above sidewalk or finished grade.
5. Parking Location applies to location of garage, surface parking lot, and parking structure.
6. When abutting Lots with Transect Zones other than SD-W, the minimum rear or side Setback shall be 15'.

T5

SECTION 3.4 (T5) MIXED-USE CENTER TRANSECT ZONE STANDARDS

A. Overview

This district forms the center of most walkable mixed-use neighborhoods; priority is placed here on optimizing the physical characteristics of the built environment for increased walkability. This Transect Zone permits the highest intensity and mix of uses with buildings located close to the sidewalk, plentiful shade for pedestrians, and parking lots screened from public view. Building Heights are permitted to be the tallest here to create landmark defining features and terminate important vistas.



Figure 3-1: Illustrative example of buildings and site arrangement in the T5 Mixed-use Center Zone.

B. Examples



Mixed-use buildings and apartments up to four stories can be located in this zone.



Smaller scale, two-story mixed-use buildings can occupy the corners in a neighborhood center.



The shallow build-to-zone locates buildings at the sidewalk.



Larger and more prominent buildings can be located closer to streets and civic spaces.



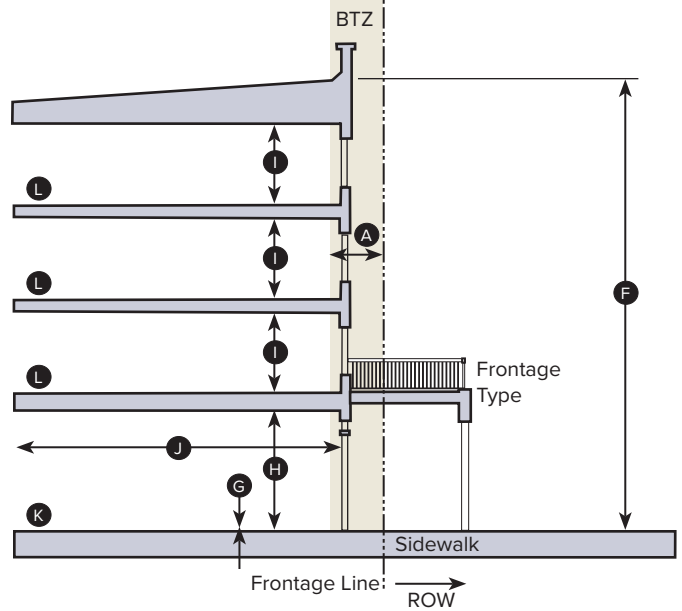
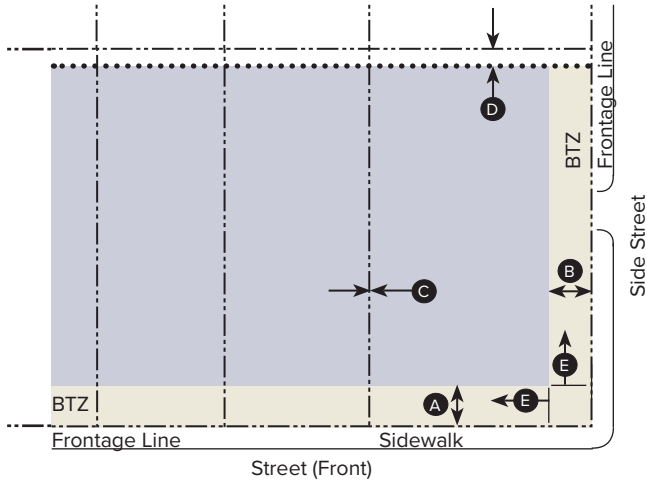
Large shopfront windows provide interesting views for pedestrians.



Two and three-story mixed-use buildings transition to residential uses in neighborhood centers.

T5

C. Building Form



Key
 - - - - - Frontage/Property Line Setback Line
 ■ Build-to-Zone (BTZ) ■ Potential Building Area (in addition to BTZ)

Key
 - - - - - Frontage Line ■ Building
 ■ Build-to-Zone (BTZ)

a. Building Placement

Setbacks

Front Build-to-Zone	0' min., 10' max.	A
Side Street Build-to-Zone	0' min., 10' max.	B
Interior Side Property Line Setback	0' min.	C
Rear Setback	5' min.	D

Frontage Buildout

Building Façade within Build-to-Zone

Front Street Frontage	80% min.
Side Street Frontage	40% min.

Street Façades must be built to the BTZ for the first 30' at a corner. **E**

b. Lot and Block Standards

Maximum Block Perimeter	2,000 linear feet max.
Lot Width	18' min., 180' max.
Lot Depth	30' min.
Lot Coverage	90% max.

c. Building Form

Height

Main Building	5 Stories max. ¹	F
Ground Floor Elev. Above Sidewalk	6" max. (Non-Residential) 24" min. ² (Residential)	G
Ground Floor Ceiling Height	12' min. (Non-Residential) 9' min. (Residential)	H
Upper Floor(s) Ceiling Height	9' min.	I

¹ See Division 5 Lot & Building Standards for more information

² Lobbies for multi-family residential buildings shall have a 6" max. ground floor elevation above sidewalk or finished grade.

Footprint

Depth, ground floor commercial space:	15' min.	J
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d. Allowed Frontage Types

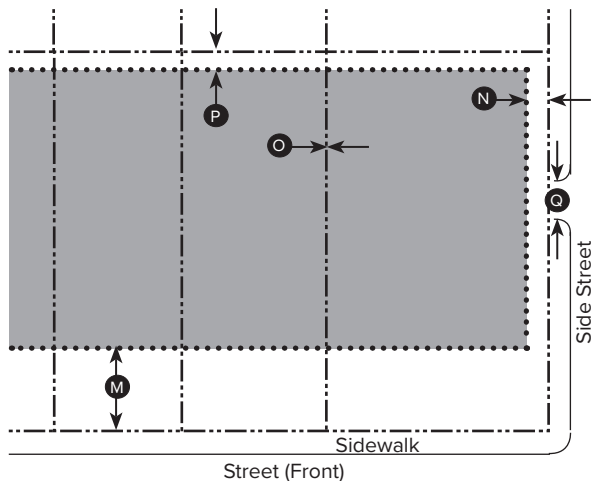
Common Yard	■ Stoop
■ Porch	■ Storefront
■ Terrace or Lightwell	■ Gallery/ Colonnade
■ Forecourt	■ Arcade

*See Division 5 Lot & Building Standards for Frontage details.

e. Allowed Use Types

Ground Floor	All Permitted Uses Allowed	K
All Floors Otherwise	All Permitted Uses Allowed	L

D. Parking



Key
 - - - - - Frontage/Property Line ······ Setback Line
 █ Parking Area

a. Parking Location¹ (Distance from Property Line)

Front Setback	30' min.	M
Side Street Setback	5' min.	N
Side Setback	5' min.	O
	0' min. (When Adjacent to Parking)	
Rear Setback	5' min.	P
	0' min. (When Adjacent to Alley)	

¹Parking Location applies to location of garage or parking lot

b. District Specific Parking Requirements

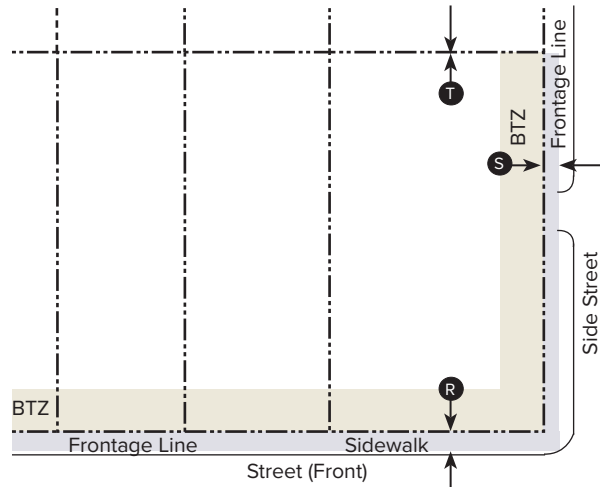
Parking shall be provided as established in Section 4.1

Parking shall be located behind the Front Façade of buildings and accessed from rear Alleys or Side Streets whenever possible.

Streetscreens, Garden Walls, fences, or hedges are required along all un-built Street Right-of-Ways adjacent to parking.

Parking / Driveway Curb Cut	24' max. (2-way)	Q
Width	12' max. (1-way)	

E. Encroachments



Key
 - - - - - Frontage/Property Line ······ Setback Line
 █ Build-to-Zone (BTZ) █ Encroachment Area

a. Allowed Encroachments

Balconies, Bay Windows, Awnings, and Other Frontage Elements

Front	8' max.	R
Side Street	4' max.	S
Rear	0' max.	T

Note: When permitted, Frontage Elements may Encroach forward of the Build-to-Zone and/or into the Right-of-Way, barring any additional restrictions by the public entity that has control over the public Right-of-Way. A 6-foot minimum sidewalk clear zone must be maintained. Frontage Elements shall maintain a minimum 2-foot Setback from the curb face.

b. Miscellaneous

All buildings must have a Principal Entrance along the Front Façade.

Where a building Façade steps back or is absent from the maximum Setback Line, the Setback Line should be defined by a Streetscreen. Loading docks, overhead doors, and other service entries shall not be located on Façades facing Streets or across from, or adjacent to, Civic Building Frontages or Civic Open Spaces, and should instead be located in rear service areas.

T4-O

SECTION 3.5 (T4-O) NEIGHBORHOOD GENERAL - OPEN TRANSECT ZONE STANDARDS

A. Overview

In the Neighborhood General Transect Zone, buildings are required to be Street-oriented, and may be attached or detached with Front Façades located close to the sidewalk. This is generally the area of the neighborhood with the greatest diversity of building types. Limited increments of non-residential uses are permitted, such as Home Occupation, small mixed-use buildings, and Live-Work Units.

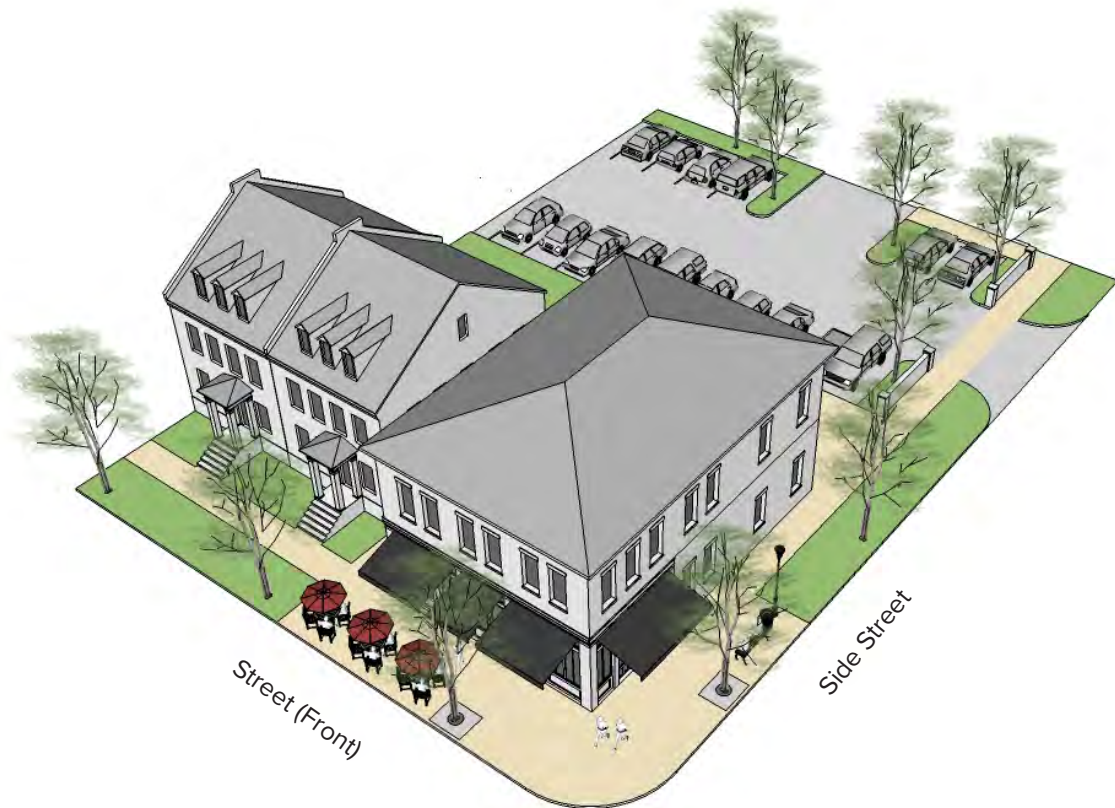


Figure 3-2: Illustrative example of buildings and site arrangement in the T4 General Zone.

T4-O

B. Examples



A Cottage Court consisting of several smaller single-family homes located around a shared green space.



A small garden area and Porch are a common Frontage for homes in this Transect Zone.



Live-Work Units offer a unique housing and commercial ownership option.



With garages accessed from the rear Alley, the Frontages are uninterrupted by driveways.



Townhouses add to the variety of housing choices within the neighborhoods.



Storefronts with shallow Setbacks transition from the neighborhood center towards the residential area.

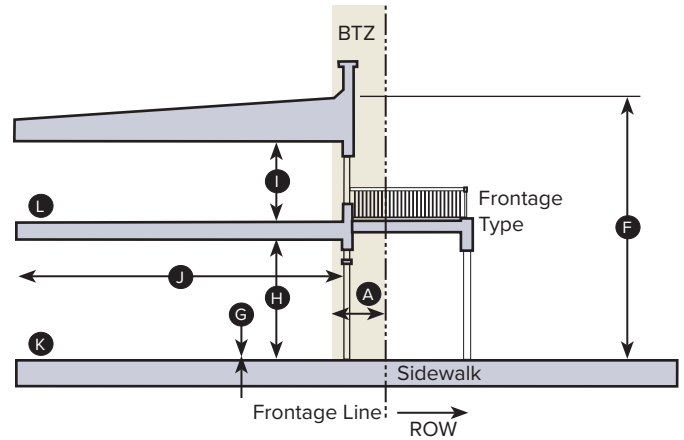
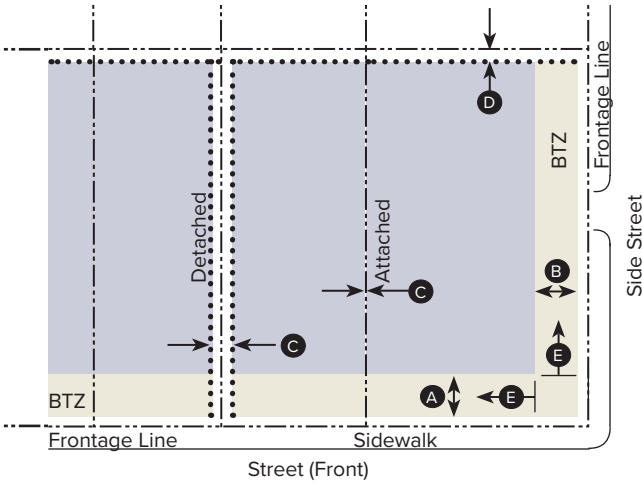
DIVISION 3

(Draft)

TRANSECT STANDARDS

T4-O

C. Building Form



Key
 - - - - - Frontage/Property Line Setback Line
 ■ Build-to-Zone (BTZ) ■ Potential Building Area (in addition to BTZ)

Key
 - - - - - Frontage Line ■ Building
 ■ Build-to-Zone (BTZ)

a. Building Placement

Setbacks		
Front Build-to-Zone	6' min., 18' max.	A
Side Street Build-to-Zone	6' min., 18' max.	B
Interior Side Property Line Setback	0' min. (attached) 5' min. (detached)	C
Rear Setback	5' min.	D

Frontage Buildout

Building Façade within Build-to-Zone	
Front Street Frontage	60% min.
Side Street Frontage	30% min.

Street Façades must be built to the BTZ for the first 30' on a corner. **E**

b. Lot and Block Standards

Maximum Block Perimeter	2,000 linear feet max.
Lot Width	18' min., 100' max.
Lot Depth	80' min.
Lot Coverage	70% max.

c. Building Form

Height		
Main Building	3 Stories max. ¹	F
Ground Floor Elev. Above Sidewalk	6" max. (Non-Residential) 24" min. ² (Residential)	G
Ground Floor Ceiling Height	12' min. (Non-Residential) 9' min. (Residential)	H
Upper Floor(s) Ceiling Height	9' min.	I

¹ See Division 5 Lot & Building Standards for more information
² Lobbies for multi-family residential buildings shall have a 6" max. ground floor elevation above sidewalk or finished grade.

Footprint

Depth, ground floor commercial space:	15' min.	J
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d. Allowed Frontage Types

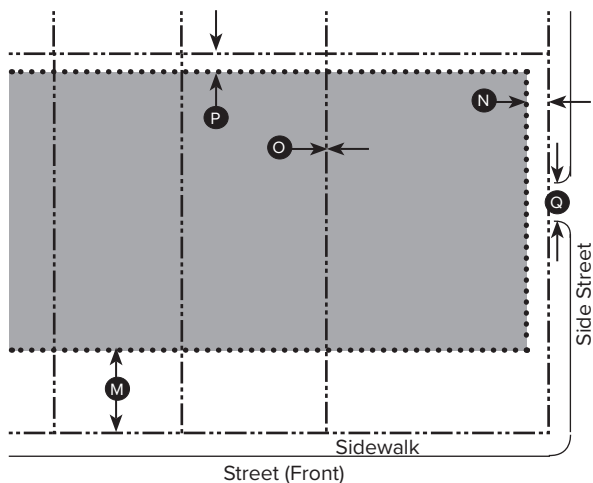
■ Common Yard	■ Stoop
■ Porch	■ Storefront
■ Terrace or Lightwell	■ Gallery/ Colonnade
■ Forecourt	■ Arcade

*See Division 5 Lot & Building Standards for Frontage details.

e. Allowed Use Types

Ground Floor	All Permitted Uses Allowed	K
All Floors Otherwise	All Permitted Uses Allowed	L

D. Parking



Key

----- Frontage/Property Line Setback Line
█ Parking Area	

a. Parking Location¹ (Distance from Property Line)

Front Setback	30' min.	M
Side Street Setback	6' min.	N
Side Setback	0' min. (attached) 5' min. (detached)	O
Rear Setback	5' min. 0' min. (When Adjacent to Alley)	P

¹Parking Location applies to location of garage or parking lot

b. District Specific Parking Requirements

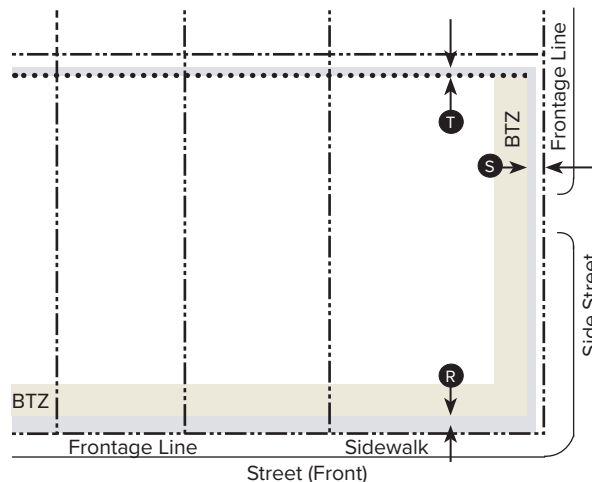
Parking shall be provided as established in Section 4.1

Parking shall be located behind the Front Façade of buildings and accessed from rear Alleys or Side Streets whenever possible.

Streetscreens, Garden Walls, fences, or hedges are required along all un-built Street Right-of-Ways adjacent to parking.

Parking / Driveway Curb Cut Width	24' max. (2-way) 12' max. (1-way)	Q
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E. Encroachments



Key

----- Frontage/Property Line Setback Line
█ Build-to-Zone (BTZ)	█ Encroachment Area

a. Allowed Encroachments

Balconies, Bay Windows, Awnings, and Other Frontage Elements

Front	6' max.	R
Side Street	3' max.	S
Rear	3' max.	T

Note: When permitted, Frontage Elements may Encroach forward of the Build-to-Zone and/or into the Right-of-Way, barring any additional restrictions by the public entity that has control over the public Right-of-Way. A 6-foot minimum sidewalk clear zone must be maintained. Frontage Elements shall maintain a minimum 2-foot Setback from the curb face.

b. Miscellaneous

All buildings must have a Principal Entrance along the Front Façade.

Where a building Façade steps back or is absent from the maximum Setback Line, the Setback Line should be defined by a Streetscreen.

Loading docks, overhead doors, and other service entries shall not be located on Façades facing Streets or across from, or adjacent to, Civic Building Frontages or Civic Open Spaces, and should instead be located in rear service areas.

T4-R

SECTION 3.6 (T4-R) NEIGHBORHOOD GENERAL - RESTRICTED TRANSECT ZONE STANDARDS

A. Overview

This Transect Zone shares the same form and building characteristics as the T4-O zone, but the uses are restricted to residential and Home Occupations. When combined, the T4-O and T4-R zones are generally the largest area of the neighborhood.



Figure 3-3: Illustrative example of buildings and site arrangement in the T4 General Zone.

B. Examples



Porches provide spaces to enjoy the outdoors.



Houses front onto a Green Street with garages and driveways accessed from a rear Alley.



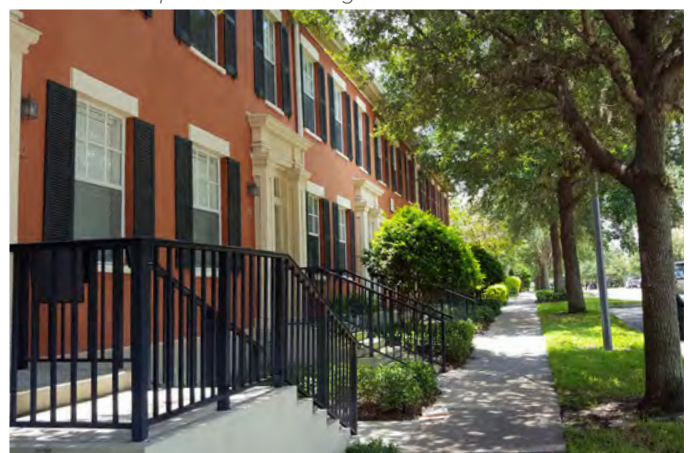
This Transect Zone has the greatest diversity of building types.



Single-family homes on small Lots with Front Façades close to the Street are part of the housing mix in this zone.



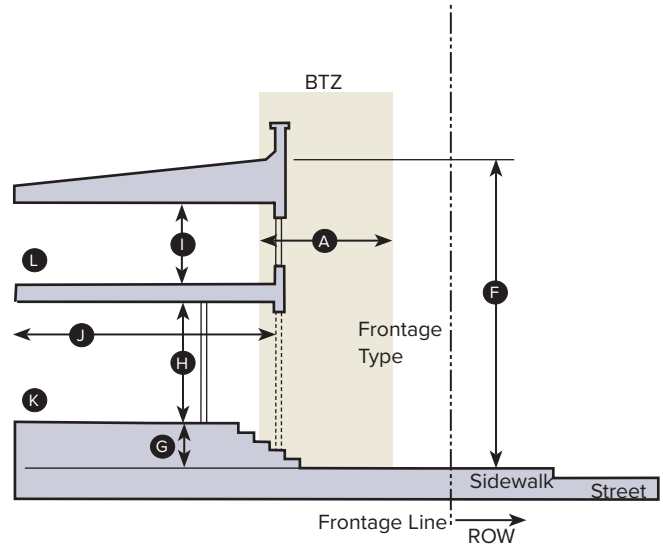
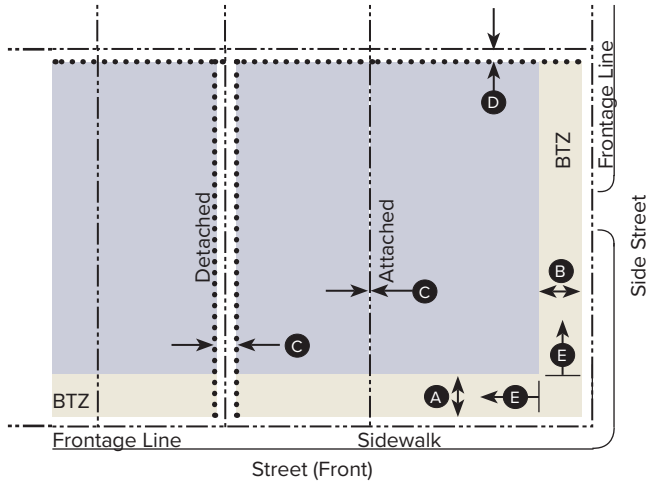
Larger homes are also an important housing type in this Transect Zone.



Townhouses with stoops and a narrow Setback.

T4-R

C. Building Form



Key

--- Frontage/Property Line Setback Line
■ Build-to-Zone (BTZ)	■ Potential Building Area (in addition to BTZ)

a. Building Placement

Setbacks		
Front Build-to-Zone	6' min., 18' max.	A
Side Street Build-to-Zone	6' min., 18' max.	B
Interior Side Property Line Setback	0' min. (attached) 5' min. (detached)	C
Rear Setback	5' min.	D

Frontage Buildout

Building Façade within Build-to-Zone	
Front Street Frontage	60% min.
Side Street Frontage	30% min.

Street Façades must be built to the BTZ for the first 30' on a corner. E

b. Lot and Block Standards

Maximum Block Perimeter	2,000 linear feet max.
Lot Width	18' min., 100' max.
Lot Depth	80' min.
Lot Coverage	70% max.

Key

--- Frontage Line	■ Building
■ Build-to-Zone (BTZ)	

c. Building Form

Height		
Main Building	3 Stories max. ¹	F
Ground Floor Elev. Above Sidewalk	6" max. (Non-Residential) 24" min. ² (Residential)	G
Ground Floor Ceiling Height	12' min. (Non-Residential) 9' min. (Residential)	H
Upper Floor(s) Ceiling Height	9' min. clear	I

¹ See Division 5 Lot & Building Standards for more information
² Lobbies for multi-family residential buildings shall have a 6" max. ground floor elevation above sidewalk or finished grade.

Footprint

Depth, ground floor commercial space:	15' min.	J
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d. Allowed Frontage Types

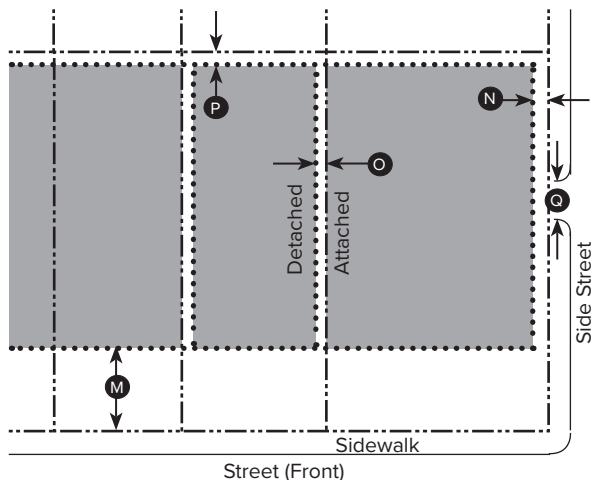
■ Common Yard	■ Stoop
■ Porch	■ Storefront
■ Terrace or Lightwell	■ Gallery/ Colonnade
■ Forecourt	■ Arcade

*See Division 5 Lot & Building Standards for Frontage details.

e. Allowed Use Types

Ground Floor	All Permitted Uses Allowed	K
All Floors Otherwise	All Permitted Uses Allowed	L

D. Parking



Key

----- Frontage/Property Line Setback Line
█ Parking Area	

a. Parking Location¹ (Distance from Property Line)

Front Setback	30' min.	M
Side Street Setback	6' min.	N
Side Setback	0' min. (attached) 5' min. (detached)	O
Rear Setback	5' min. 0' min. (When Adjacent to Alley)	P

¹Parking Location applies to location of garage or parking lot

b. District Specific Parking Requirements

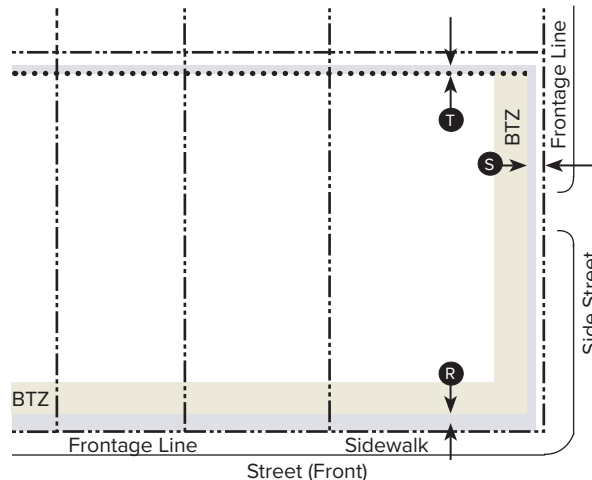
Parking shall be provided as established in Section 4.1

Parking shall be located behind the Front Façade of buildings and accessed from rear Alleys or Side Streets whenever possible.

Streetscreens, Garden Walls, fences, or hedges are required along all un-built Street Right-of-Ways adjacent to parking.

Parking/Driveway Curb Cut Width	12' max.	Q
---------------------------------	----------	----------

E. Encroachments



Key

----- Frontage/Property Line Setback Line
█ Build-to-Zone (BTZ)	█ Encroachment Area

a. Allowed Encroachments

Balconies, Bay Windows, Awnings, Galleries, Stoops, and Other Frontage Elements

Front	3' max. ¹	R
Side Street	3' max. ¹	S
Rear	3' max.	T

Note: Frontage Elements shall not Encroach into the Right-of-Way.

¹ Stairs may Encroach beyond this maximum to the Frontage Line. Frontage Elements shall not Encroach into the Right-of-Way

b. Miscellaneous

All buildings must have a Principal Entrance along the Front Façade.

Loading docks, overhead doors, and other service entries shall not be located on Façades facing Streets or across from, or adjacent to, Civic Building Frontages or Civic Open Spaces, and should instead be located in rear service areas.

T3

SECTION 3.7 (T3) NEIGHBORHOOD EDGE TRANSECT ZONE STANDARDS

A. Overview

In the Neighborhood Edge Transect Zone, buildings are required to be Street-oriented and are typically detached. The intent of this zone is to facilitate a transition between the Traditional Neighborhood Development areas and lower intensity development in surrounding single-family neighborhoods and agricultural lands. Among other details, buildings in this zone are predominantly residential and are set further back from the Street on larger Lots.



Figure 3-4: Illustrative example of buildings and site arrangement in the T3 Edge Zone.

T3

B. Examples



This Transect Zone primarily consists of larger homes on larger Lots.



Building materials can reflect the local character of the neighborhood and city.



Buildings are Street-oriented but set further back than in the T4 and T5 Transect Zones.



A large single-family detached house with Porch. A picket fence marks the Frontage Line.



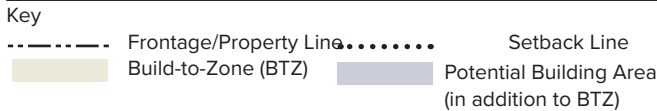
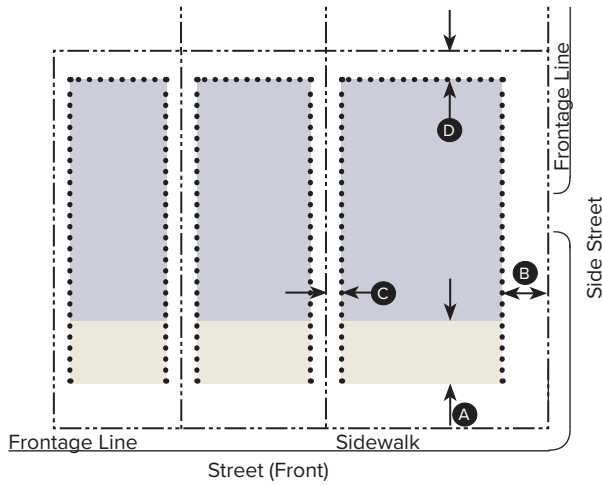
This Transect Zone can help transition to surrounding existing lower-density residential neighborhoods.



Parking and garages are located towards the rear of the Lots, typically behind the Principal Building.

T3

C. Building Form



a. Building Placement

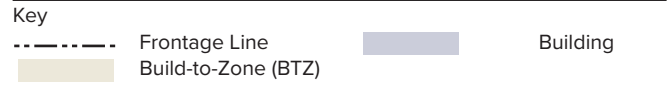
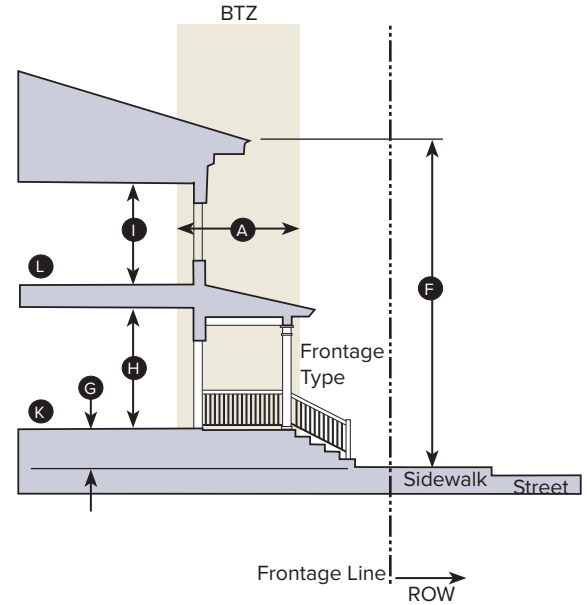
Setbacks		
Front Build-to-Zone	20' min., 48' max.	A
Side Street Setback	12' min.	B
Interior Side Property Line Setback	8' min.	C
Rear Setback	12' min.	D

Frontage Buildout

Building Façade within Build-to-Zone	
Front Street Frontage	40% min.
Side Street Frontage	N/A

b. Lot and Block Standards

Maximum Block Perimeter	2,400 linear feet max.
Lot Width	50' min.
Lot Depth	110' min.
Lot Coverage	60% max.



c. Building Form

Height		
Main Building	2.5 Stories max. ¹	F
Ground Floor Elev. Above Sidewalk	24" min.	G
Ground Floor Ceiling Height	9' min. clear	H
Upper Floor(s) Ceiling Height	9' min. clear	I

¹See Division 5 Lot & Building Standards for more information

d. Allowed Frontage Types

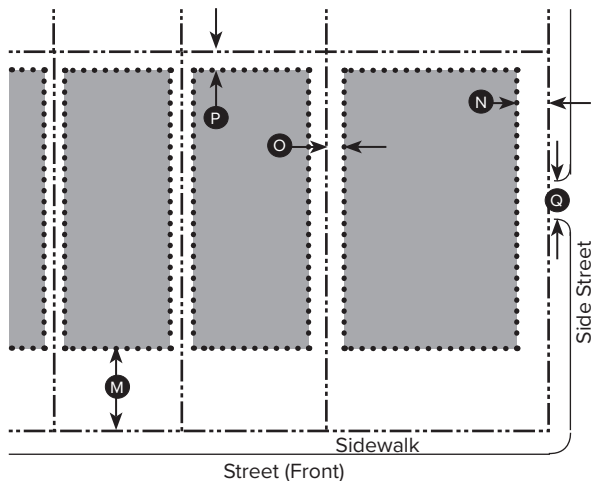
■ Common Yard	■ Stoop
■ Porch	Storefront
Terrace or Lightwell	Gallery/ Colonnade
Forecourt	Arcade

*See Division 5 Lot & Building Standards for Frontage details.

e. Allowed Use Types

Ground Floor	All Permitted Uses Allowed	K
All Floors Otherwise	All Permitted Uses Allowed	L

D. Parking



Key

-----	Frontage/Property Line	Setback Line
■	Parking Area		

a. Parking Location¹ (Distance from Property Line)

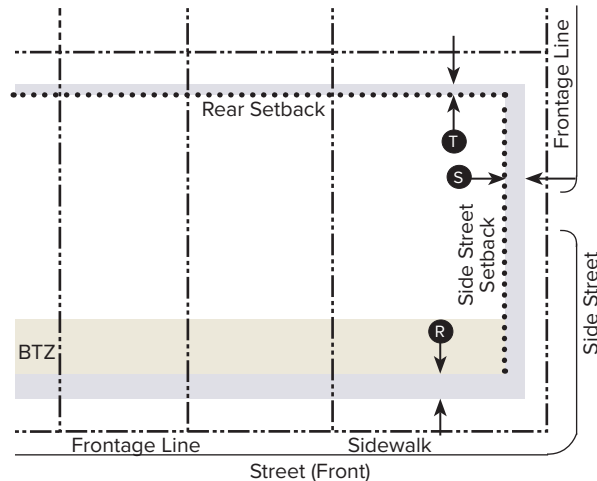
Front Setback	30' min.	M
Side Street Setback	12' min.	N
Side Setback	8' min.	O
Rear Setback	5' min. 0' min. (When Adjacent to Alley)	P

¹ Parking Location applies to location of garage

b. District Specific Parking Requirements

- Parking shall be provided as established in Section 4.1
- Parking shall be located behind the Front Façade of buildings and accessed from rear Alleys or Side Streets whenever possible.
- Streetscreens, Garden Walls, fences, or hedges are required along all un-built Street Right-of-Ways adjacent to parking.
- Shared driveways between adjacent Lots is encouraged to reduce curb cuts.
- Curb cuts are not permitted if a rear Alley is present.
- Parking Curb Cut Width 15' max. **Q**

E. Encroachments



Key

-----	Frontage/Property Line	Setback Line
■	Build-to-Zone (BTZ)	■	Encroachment Area

a. Allowed Encroachments

Balconies, Bay Windows, Awnings, Stoops, and Other Frontage Elements

Front	8' max. ¹	R
Side Street	8' max.	S
Rear	3' max.	T

¹ Stairs may Encroach beyond this maximum an additional 4 feet. Frontage Elements shall not Encroach into the Right-of-Way

b. Miscellaneous

All buildings must have a Principal Entrance along the Front Façade.

T2

SECTION 3.8 (T2) RURAL TRANSECT ZONE STANDARDS

A. Overview

The Rural Transect Zone reflects the working agricultural character of Greeley, supporting uses such as crop production, livestock operations, agribusiness facilities, and complementary Open Space. Recreation, equestrian activities, and limited rural residential uses are also compatible. Compared to other Transect Zones, it is less formal and allows greater flexibility in building placement to accommodate barns, greenhouses, silos, and other farm-related structures. Development that supports or complements agricultural activity is permitted within this Transect Zone.

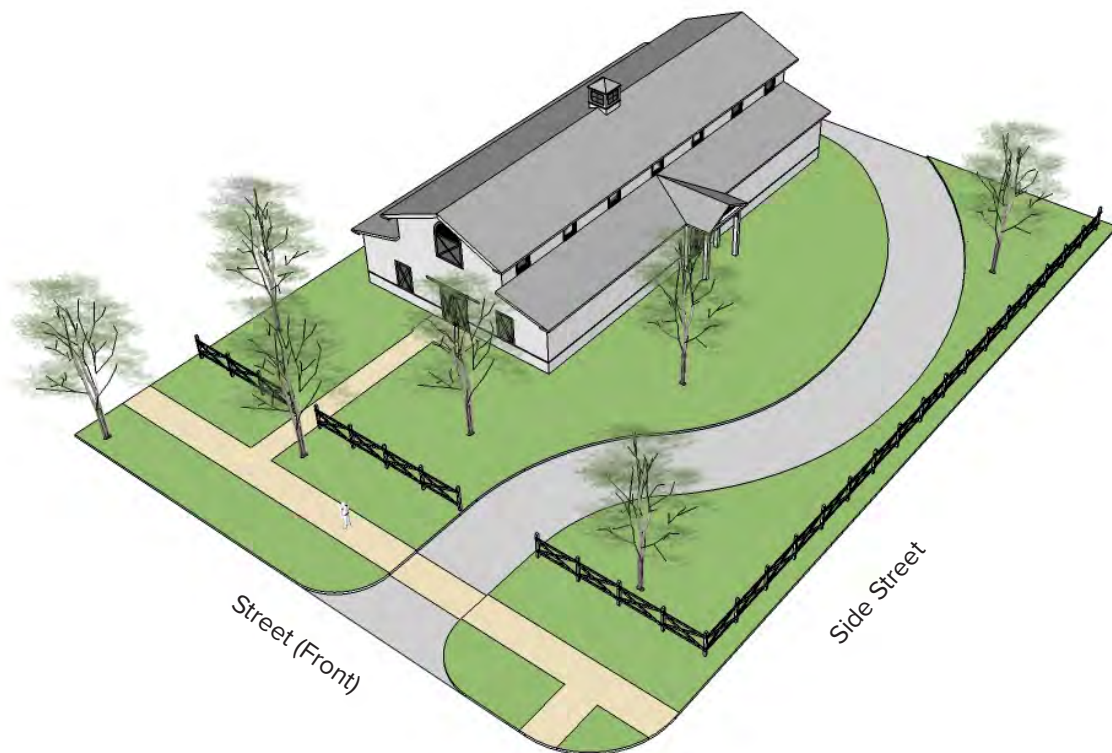


Figure 3-5: Illustrative example of buildings and site arrangement in the T2 Rural Zone.

B. Examples



Buildings and other structures in this zone support agricultural and recreational uses.



Agricultural facilities may be used for hosting events and community gatherings.



Farmers' Markets are part of the mix in this Transect Zone.



Agricultural Uses may include cattle, corn, sugar beets, or hay production.



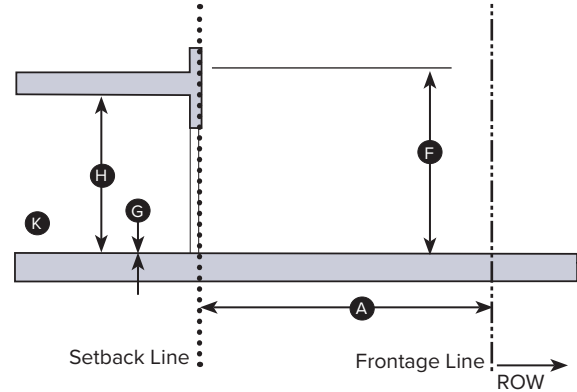
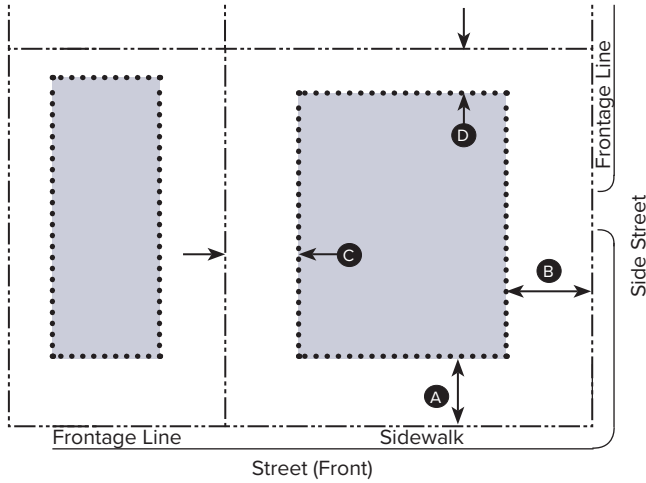
Less formal than the other Transect Zones, more flexibility is permitted in building placement.



Community Supported Agriculture (CSA) Farms offer residents an opportunity for local food production.

T2

C. Building Form



Key
 - - - - - Frontage/Property Line ······ Setback Line
 ■ Potential Building Area

Key
 - - - - - Frontage Line ······ Setback Line
 ■ Building

a. Building Placement		
Setbacks		
Front Setback	20' min.	A
Side Street Setback	20' min.	B
Interior Side Property Line Setback	30' min.	C
Rear Setback	30' min.	D

Frontage Buildout	
Building Façade along:	
Front Street Frontage	N/A
Side Street Frontage	N/A

b. Lot and Block Standards	
Maximum Block Perimeter	By Warrant
Lot Width	By Warrant
Lot Depth	By Warrant
Lot Coverage	By Warrant

c. Building Form		
Height		
Main Building	2.5 Story max. ^{1,2}	F
Ground Floor Elev. Above Sidewalk	0' min.	G
Ground Floor Ceiling Height	9' min.	H
Upper Floor(s) Ceiling Height	9' min.	I

¹ See Division 5 Lot & Building Standards for more information

² Permitted Agriculture Uses may exceed the 2 Story maximum height limit with approval from the zoning administrator of the local jurisdiction.

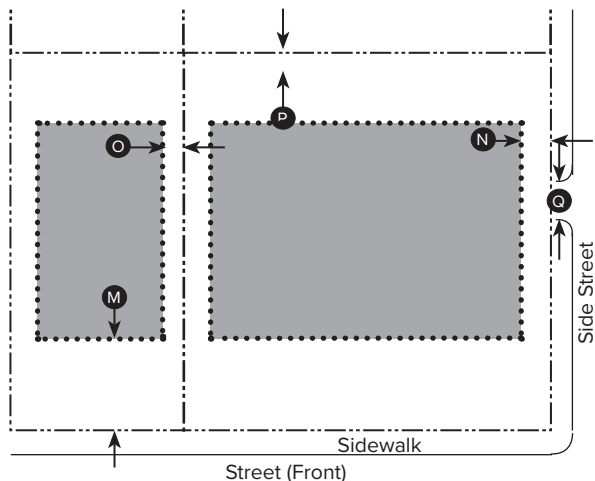
d. Allowed Frontage Types	
■ Common Yard	Stoop
■ Porch	Storefront
Terrace or Lightwell	Gallery/ Colonnade
Forecourt	Arcade

*See Division 5 Lot & Building Standards for Frontage details.

e. Allowed Use Types	
Ground Floor	All Permitted Uses Allowed K

D. Parking

E. Encroachments



Key
 - - - - - Frontage/Property Line Setback Line
 ■ Parking Area

Key
 - - - - - Frontage/Property Line Setback Line
 ■ Encroachment Area

a. Parking

Parking Location¹ (Distance from Property Line)

Front Setback	30' min.	M
Side Street Setback	12' min.	N
Side Setback	12' min.	O
Rear Setback	30' min.	P

¹Parking Location applies to location of garage or parking lot

District Specific Parking Requirements

Parking shall be provided as established in Section 4.1

Parking shall be located behind the Front Façade of buildings and accessed from Side Streets whenever possible.

Streetscreens, Garden Walls, fences, or hedges are required along all un-built Street Right-of-Ways adjacent to parking.

Parking / Driveway Curb Cut Width	24' max.	Q
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a. Allowed Encroachments

Encroachments are not permitted

T1

SECTION 3.9 (T1) NATURAL TRANSECT ZONE STANDARDS

A. Overview

The Natural Transect Zone consists of property that is nature preserve. Development within this area is limited to protect the natural habitat. Limited boardwalks, trails, trailheads, and associated parking may be developed in order to access the area for recreational purposes.



Figure 3-6: Illustrative example of T1 Natural Zone.

B. Examples and Standards



Samples of recreational trail boardwalks and wayfinding through preserved area.

SD-W

SECTION 3.10 (SD-W) SPECIAL DISTRICT - WORKPLACE TRANSECT ZONE STANDARDS

A. Overview

Lot, Building, and Street dimensions within the Workplace District Transect Zone vary based on the functional requirements of the use type, but the goal of compact, walkable urban form remains. The focus of this area is on industrial, office, and research functions.

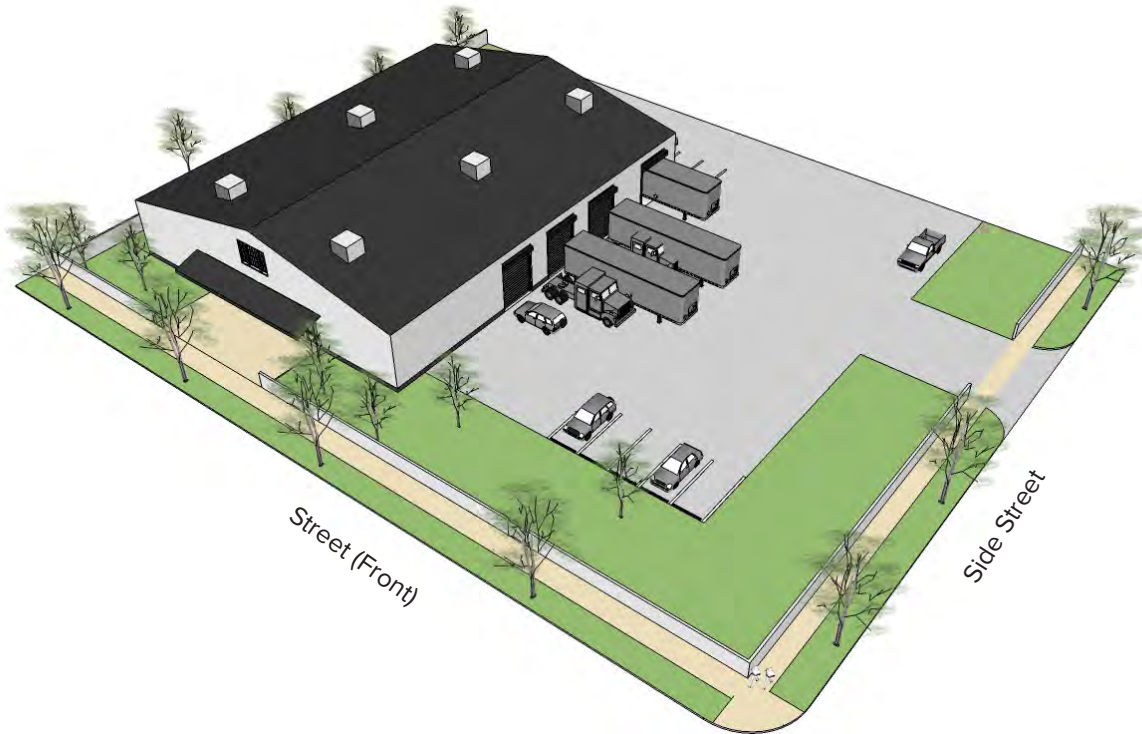


Figure 3-7: Illustrative example of building and site arrangement in the SD-W Special Workplace District.

B. Examples



Office and research buildings create a walkable urban form.



Buildings can be brought close to the Street.



Functional green spaces and plazas are an integral part of this Transect District.



Tall ceiling heights can accommodate a wide range of uses and meet the demands for modern workplaces.



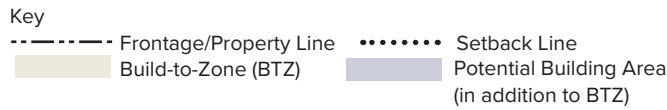
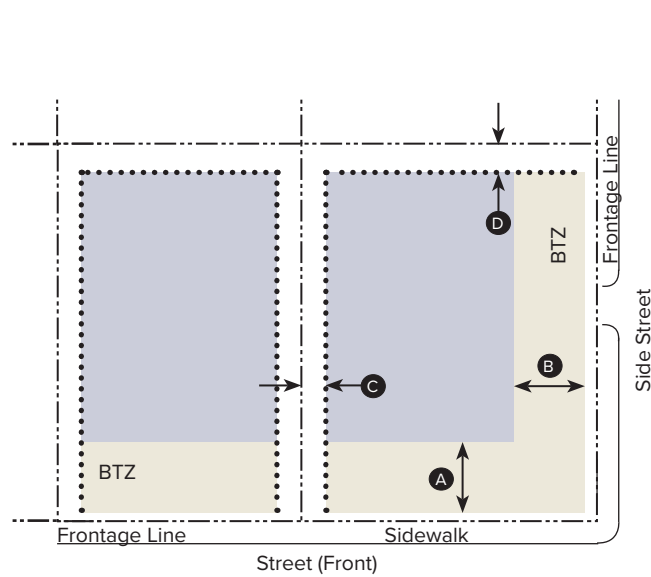
Industrial buildings and uses are also a part of the Workplace District.



The Workplace District is the location for industrial, research, and institutional uses.

SD-W

C. Building Form



a. Building Placement

Setbacks		
Front Build-to-Zone	6' min., 40' max.	A
Side Street Build-to-Zone	6' min., 40' max.	B
Interior Side Property Line Setback	5' min. ¹	C
Rear Setback	5' min. ¹	D

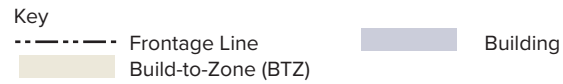
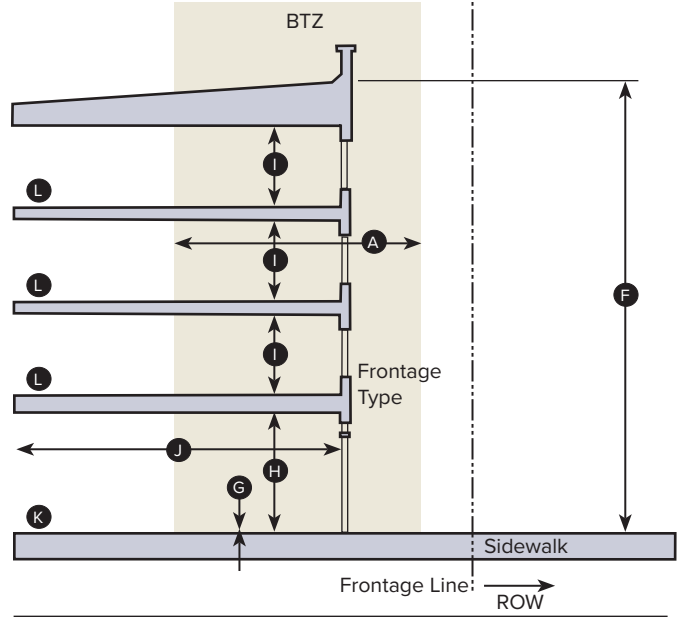
¹ When abutting Lots with Transect Zones other than SD-W, the minimum rear or side Setback shall be 15'.

Frontage Buildout

Building Façade within Build-to-Zone	
Front Street Frontage	25% min.
Side Street Frontage	20% min.

b. Lot and Block Standards

Maximum Block Perimeter	3,000 linear feet max.
Lot Width	No min., No max.
Lot Depth	No min., No max.
Lot Coverage	80% max.



c. Building Form

Height		
Main Building	6 Stories max. ²	F
Ground Floor Elev. Above Sidewalk	N/A (Non-Residential) 24" min. (Residential)	G
Ground Floor Ceiling Height	14' min.	H
Upper Floor(s) Ceiling Height	14' min. (Non-Residential) 9' min. (Residential)	I

¹ See Division 5 Lot & Building Standards for more information

d. Allowed Frontage Types

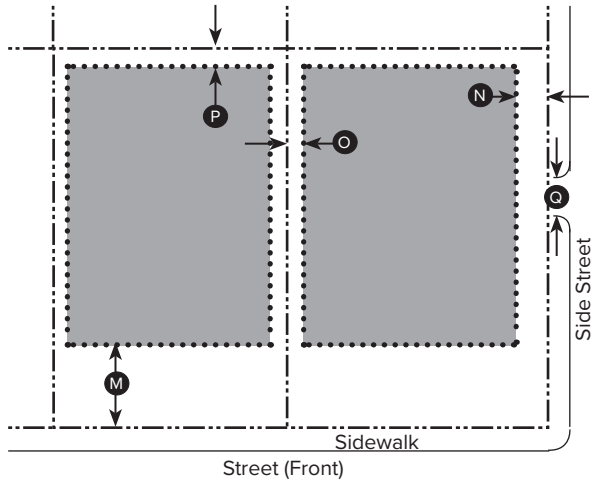
Common Yard	Stoop
Porch	■ Storefront
Terrace or Lightwell	■ Gallery/ Colonnade
■ Forecourt	■ Arcade

*See Division 5 Lot & Building Standards for Frontage details.

e. Allowed Use Types

Ground Floor	All Permitted Uses Allowed	K
All Floors Otherwise	All Permitted Uses Allowed	L

D. Parking



Key
 - - - - - Frontage/Property Line ······ Setback Line
 █ Parking Area

a. Parking Location¹ (Distance from Property Line)

Front Setback	40' min.	M
Side Street Setback	20' min.	N
Side Setback	10' min. 0' min. (When Adjacent to Parking)	O
Rear Setback	5' min.	P

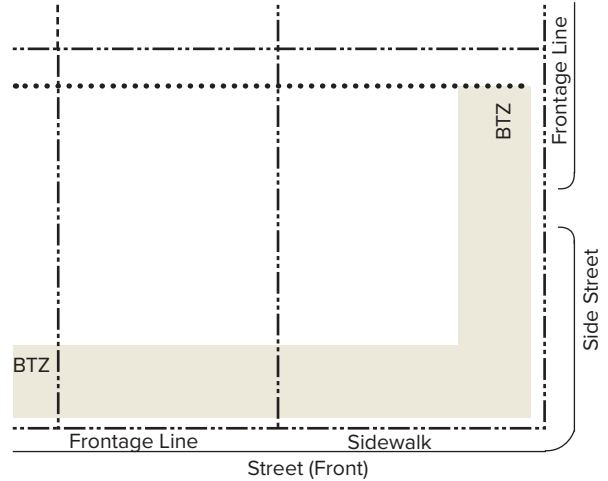
¹Parking Location applies to location of garage or parking lot

b. District Specific Parking Requirements

Parking shall be provided as established in Section 4.1
 Parking shall be located behind the Front Façade of buildings and accessed from rear Alleys or Side Streets whenever possible.
 Streetscreens, Garden Walls, fences, or hedges are required along all un-built Street Right-of-Ways adjacent to parking.

Parking / Driveway Curb Cut Width	24' max. (2-way) 12' max. (1-way)	Q
-----------------------------------	--------------------------------------	----------

E. Encroachments



Key
 - - - - - Frontage/Property Line ······ Setback Line
 █ Build-to-Zone (BTZ) █ Encroachment Area

a. Allowed Encroachments

Encroachments are not permitted

b. Miscellaneous

All buildings must have a Principal Entrance along the Front Façade.
 Where a building Façade steps back or is absent from the maximum Setback Line, the Setback Line should be defined by a Streetscreen.
 Loading docks, overhead doors, and other service entries shall not be located on the Front Façade.



SECTION 3.11 (C) CIVIC TRANSECT ZONE STANDARDS

A. Overview

The Civic Zone encompasses sites and structures dedicated to public purposes and the enhancement of community well-being. This includes Civic Buildings that provide for arts, culture, education (public and private schools), recreation, government, transit, and emergency services. Because the specific uses and forms of Civic Buildings may evolve over time, these sites are granted greater design flexibility while remaining subject to a higher level of design review to ensure quality and compatibility with community character.

The Civic Zone also includes Civic Open Spaces—lands provided or preserved for public use, environmental protection, or cultural value. These may serve park and recreational functions, conserve natural resources, or highlight historic and scenic assets. Examples include parks, plazas, community gardens, schoolyards, Playgrounds, squares, and conservation areas. Supporting structures and buildings that enhance or enable these Open Space functions are permitted within this zone.

Together, Civic Buildings and Civic Open Spaces form the essential framework of community life, offering places for gathering, learning, governance, recreation, and environmental stewardship.

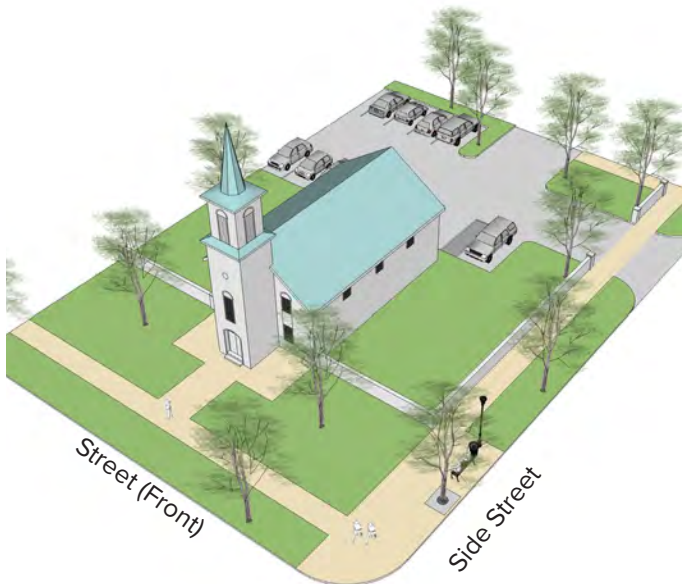


Figure 3-8: Illustrative example of buildings and site arrangement in the Civic Transect Zone.

B. Civic Building Examples



A school that students can walk to located within walking distance of several neighborhoods.



Civic Buildings can take on a variety of sizes to serve the needs of the community.



The tower, Setback, and materials of this fire station distinguish it from nearby commercial buildings.



C. Civic Building Standards

1. General

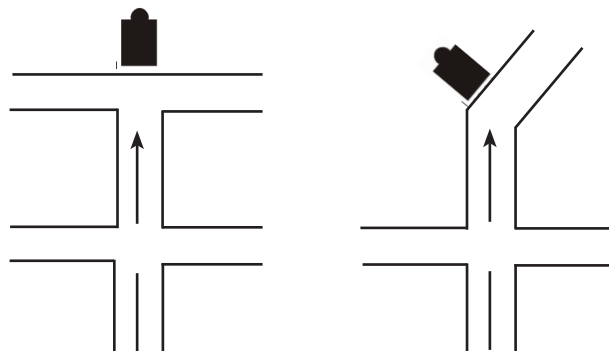
Civic Buildings may include, but are not limited to, municipal buildings, libraries, schools, recreation facilities, fire stations, and places of assembly. The design and construction of Civic Buildings shall reflect the importance of these buildings within the community and with their function as landmarks in mind.

2. Building Siting

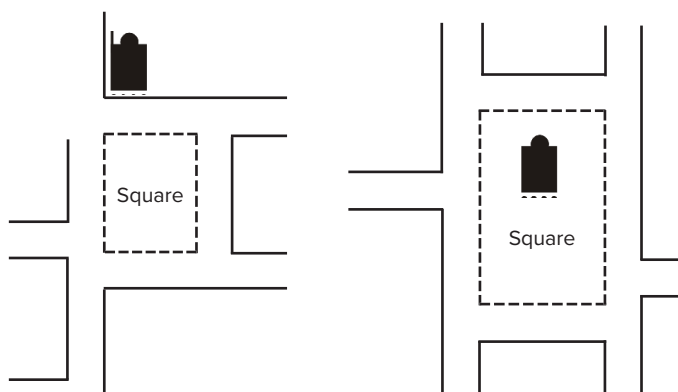
Civic Buildings should be sited in locations of particular geometric importance, such as anchoring a major Open Space or terminating a Street vista. Flexibility in building placement allows Civic Buildings to be distinguished from surrounding residential and commercial buildings and be a prominent landmark in the community. (See Figure 3-9)

3. Building Design Guidelines

- a. The scale of Civic Buildings should typically be larger than surrounding buildings in order to be more prominent and visible across greater distances.
- b. Floor-to-floor heights and architectural details should be proportionately larger than those of private buildings that exist or are anticipated within adjacent Blocks.
- c. Prominent roof forms and additive elements such as cupolas can visually extend the height of the building. See Division 5 Building Design Standards for more information.



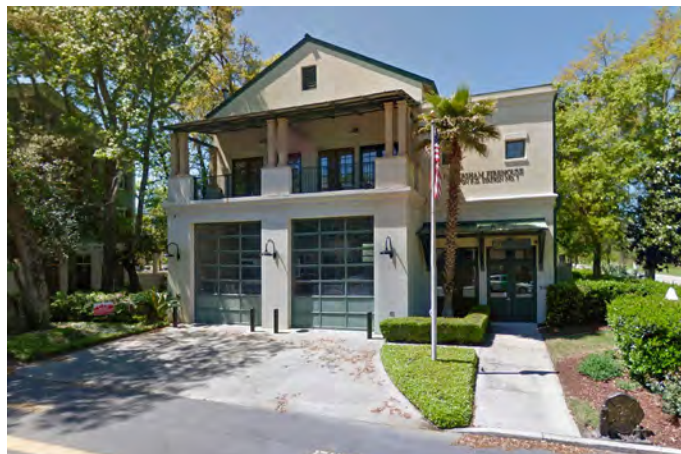
The Civic Building terminates the view of a Street.



The Civic Building anchors the square at a prominent corner.

The Civic Building anchors the space from within the square.

Figure 3-9: Civic Building siting diagrams



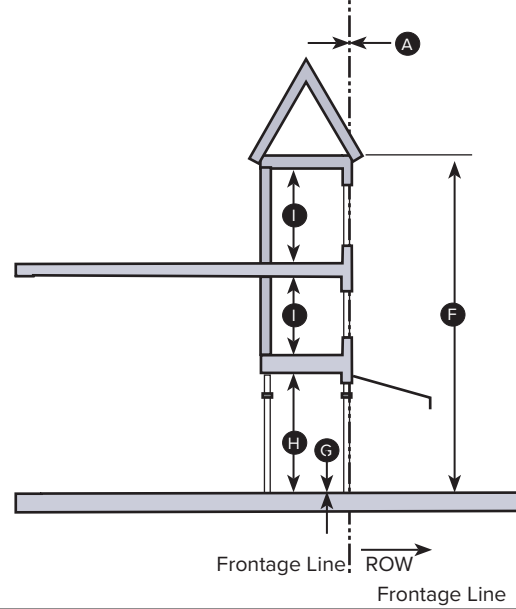
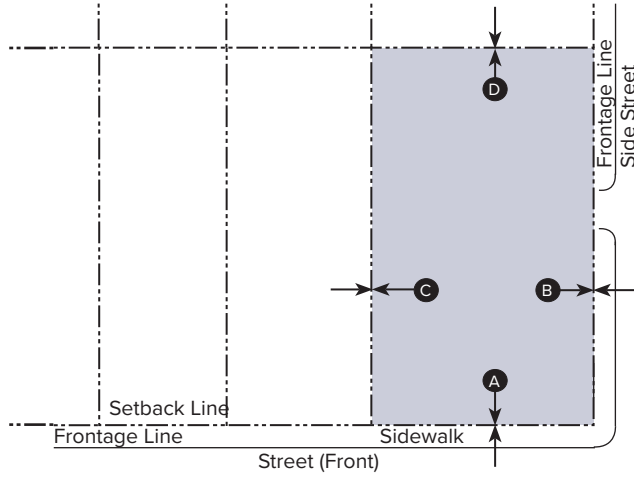
Greater design flexibility allows for buildings to accommodate a wide range of functions and uses.



Civic Uses can include neighborhood amenity areas and function as focal buildings.



D. Civic Building Form & Height



Key
 - - - - - Frontage/Property Line ······ Setback Line
 ■ Potential Building Area

Key
 - - - - - Frontage Line ■ Building

a. Building Placement

Setbacks

Front Setback	0' min.	A
Side Street Setback	0' min.	B
Interior Side Property Line Setback	0' min.	C
Rear Setback	0' min.	D

Frontage Buildout

Building Façade along:	
Front Street Frontage	By Warrant
Side Street Frontage	By Warrant

b. Lot and Block Standards

Maximum Block Perimeter	N/A
Lot Width	N/A
Lot Depth	N/A
Lot Coverage	By Warrant

c. Building Form

Height

Main Building	By Warrant ¹	F
Ground Floor Elev. Above Sidewalk	By Warrant	G
Ground Floor Ceiling Height	By Warrant	H
Upper Floor(s) Ceiling Height	By Warrant	I

¹See Division 5 Lot & Building Standards for more information

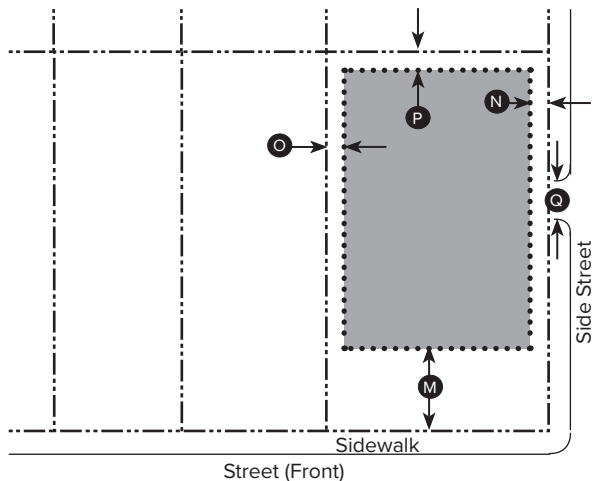
d. Allowed Use Types

Ground Floor	All Permitted Uses Allowed	K
All Floors Otherwise	All Permitted Uses Allowed	L



E. Civic Building Parking

F. Civic Building Encroachments



Key
 - - - - - Frontage/Property Line ······ Setback Line
 █ Parking Area

a. Parking Location¹ (Distance from Property Line)

Front Setback	By Warrant	M
Side Street Setback	By Warrant	N
Side Setback	By Warrant	O
Rear Setback	By Warrant	P

¹Parking Location applies to location of garage or parking lot

b. District Specific Parking Requirements

Parking shall be provided as established in Section 4.1

Parking shall be located behind the Front Façade of buildings and accessed from rear Alleys or Side Streets whenever possible.

Streetscreens, Garden Walls, fences, or hedges are required along all un-built Street Right-of-Ways adjacent to parking.

When a Civic Building is located within a Civic Space completely surrounded by Streets, then parking shall be accommodated on-Street or within mid-block locations within 1/4 mile of the Civic Building's front door.

Parking/Driveway Curb Cut Width	22' max. ¹	Q
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¹ Unless wider access is necessitated by emergency vehicle use. Then width is determined by Warrant.

a. Allowed Encroachments

Encroachments are not permitted

b. Miscellaneous

All buildings must have a Principal Entrance along the Front Façade.



A. Civic Open Space Standards

1. Open Space, including in the form of parks, greens, squares, plazas, Playgrounds, pocket parks, or recreational fields, shall be located in each neighborhood at a minimum area greater than or equal to 5 percent of all land within each Neighborhood.
2. Civic space shall be in the form of parks, greens, squares, plazas, Playground/pocket parks, or recreational fields and shall meet the requirements in Table 3-2 Open Space Type Requirements.
3. Heightened attention shall be paid to the quality of landscape design and function according to the following principles:
 - a. All designated Open Spaces shall be shall be accessible to the public.
 - b. The landscape design shall support and express environmental, cultural, and historical attributes.
 - c. The landscape design shall promote connection with nature, social interaction and mental restoration.
 - d. Views of natural features shall be preserved or maximized.
 - e. The landscape design shall promote connection to surrounding neighborhood resources, amenities and services, and provide for optimum accessibility, safety and way-finding.
 - f. Stormwater management improvements shall be integrated with the final landscape design as aesthetically and visually pleasing design elements.
 - g. Whenever appropriate, landscape design shall promote sustainability awareness and education through interpretive signs, demonstrations and other forms of interpretation.

B. Civic Open Space Examples and Standards



Neighborhood green serving as an active community gathering space.





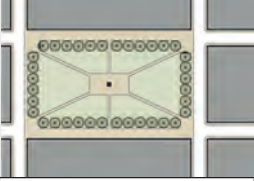
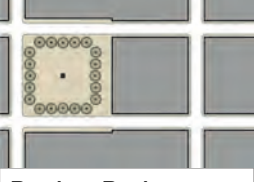
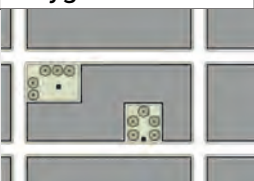

A well-designed public plaza anchoring a town center.



Trail connection within a major regional park.



TABLE 3-2: OPEN SPACE TYPE REQUIREMENTS

	Description	Size Range	Frontage & Location	Typical Features	Planting	Surrounding Building Frontages
Park 	A large Open Space providing a range of active and passive recreational uses, including trails, Playgrounds, pavilions, event spaces, and conservation areas.	1 acre to 50+ acres	At edges of neighborhoods, districts, or towns; often near natural features	Extensive lawns, trails, Playgrounds, pavilions, water features, woodlands, meadows, arroyos, restrooms, recreational fields, performance or event space	Mix of natural and formal plantings	Fronted by Streets, neighborhoods, or civic uses
Green 	An informal Open Space with lawns and shade trees, intended for passive recreation, informal play, and community gatherings.	10,000 sf to 5 acres	Embedded within neighborhoods; fronted by residential or mixed-use buildings	Informal open lawn; paths; shade structures; benches; community gathering areas	Large shade trees, lawns, perimeter planting	50–75% fronted by building façades or Streets
Square 	A formal civic space bounded by Streets and fronted by buildings, with a central lawn or paved area, seating, and monuments or fountains.	5,000 sf to 1 acres	Bounded on at least 2–3 sides by Streets with active Frontages	Formal lawn, trees, symmetrical layout, central monument/ fountain, café seating	Formally arranged trees, planters	Minimum 50% active ground-floor Frontages
Plaza 	A hardscaped civic space, typically fronting civic, commercial, or mixed-use buildings, designed for gatherings, markets, or events, with seating, lighting, and shade structures.	1,000 to 10,000 sf	Fronts civic, commercial, or mixed-use buildings; near intersections	Hardscaped; seating, lighting, kiosks, shade structures; may support events	Sparse; primarily potted or perimeter trees	75% fronted by building façades
Pocket Park or Playground 	A small neighborhood Open Space, often near schools or residences, providing play equipment, seating, and shaded areas for daily use.	1,500 to 10,000 sf	Near schools, parks, or residential areas; may be within larger park	Play equipment, seating for caregivers, fencing, shade structures, water play	Shade trees, turf, perimeter planting	Passive building or park edges
Recreational Fields 	Athletic spaces designed for organized sports, including fields, courts, bleachers, and supporting facilities; may be part of larger parks or school campuses.	0.5 to 20 acres	Adjacent to schools, parks, or neighborhoods; may be within larger park or greenway	Athletic fields, courts, bleachers, lighting, restrooms, concessions	Turf, perimeter trees or screening	Passive building edges, park boundaries

SECTION 3.12 PERMITTED USES

The Permitted Uses Table, Table 3-3, lists the various types of uses and identifies whether or not a use is permitted By-right or By Warrant.

■ = By-right

□ = By Warrant

Blank = Not Permitted

A. Listed Uses

- 1. Permitted Use:** A Land Use that is allowed by-right in a Transect Zone because it is considered to be consistent with the vision and goals established for that Transect Zone.
- 2. By Warrant:** A Land Use that is allowed By Warrant shall seek special approval as described in Division 7.

B. Use Not Listed

If a proposed use is not listed in Table 3-3, the applicant may seek approval by Warrant/Exception, as described in Division 7.

**TABLE 3-3:
PERMITTED USES**

	T1	T2	T3	T4-R	T4-O	T5	SD-W	C
RESIDENTIAL TYPES¹								
Mixed-Use Building/Block					■	■	■	
Apartment Building				■	■	■	■	
Quadplex			■	■	■	■		
Triplex			■	■	■	■		
Live/Work Unit				■	■	■		
Townhouse				■	■	■	■	
Duplex House			■	■	■	■		
House		■	■	■	■			
Accessory Dwelling Unit			■	■	■	■		
Residential Convertible to Retail					■	■	■	
Nursing Home			□	■	■	■	■	
Assisted Living Facility				■	■	■	■	
Group Living (Under 12)			■	■	■	■		
School Dormitory				■	■	■	■	■
LODGING								
Hotel (no room limit)						■	■	
Inn (up to 12 rooms)		□			■	■	■	
Bed & Breakfast		□	□	□	■	■	■	
Hostel					□	□	□	

**TABLE 3-3:
PERMITTED USES**

	T1	T2	T3	T4-R	T4-O	T5	SD-W	C
COMMERCIAL								
Office/ Research						■	■	■
Medical Office					□	■	■	
Home Occupation			■	■	■	■	■	
Animal Services: Sales and Grooming / Veterinary		■			□	■	■	
Open-Market Building (Farm Markets)		■		■	■	■	■	■
Retail					■	■	■	
Food and Beverage Retail Sales					■	■	■	
Personal Improvement Service					■	■	■	
Financial Services					■	■	■	
Repair or Laundry Service, Consumer					■	■	■	
Restaurant					■	■	■	
Kiosk					■	■	□	
Push Cart						■	□	
Food Truck	□	□	□	□	■	■	■	□
Tavern or Nightclub						■	■	
Movie Theater						■	■	
CIVIC								
Bus Shelter	■	■	■	■	■	■	■	■
Convention Center							□	□
Conference Center						■	■	□
Exhibition Center							□	□
Fountain or Public Art		■	■	■	■	■	■	■
Library						■		■
Live Theater						■	■	□
Museum						■	■	■
Amphitheater/Outdoor Auditorium		■						■
Parking Structure						■	■	
Playground		■	■	■	■	■	■	■
Sports Stadium							□	□
Surface Parking Lot	■	■		■	■	■	■	■
Religious Assembly		■	■	■	■	■	■	
Government Building & Use						■	■	■
Trailhead	■	■	■	■				■

**TABLE 3-3:
PERMITTED USES**

	T1	T2	T3	T4-R	T4-O	T5	SD-W	C
Other: CIVIL SUPPORT								
Fire Station								■
Police Station								■
Hospital						■	■	■
Other: EDUCATION								
College							■	■
High School								■
Trade School							■	■
Middle School								■
Elementary School								■
Adult Day Care Center				■	■	■	■	
Child Day Care Center			■	■	■	■	■	
Other: INDUSTRIAL / MANUFACTURING								
Artisan Manufacturing, Production and Industrial						■	■	
Limited Manufacturing, Production and Industrial							■	
Distribution Center							■	
Research Service						■	■	
Water Supply Facility							□	
Sewer and Waste Facility							□	
Electric Substation		□	□	□	□	□	■	
Wireless Transmitter	□	□	□	□	□	■	■	■
Warehouse							■	
Produce Storage		□					■	
Mini-Storage/Self-Storage ²							□	
Cottage Food		■	■	■	■	■	■	
Microbrewery/Microdistillery						■	■	
Utility-Scale Renewable Energy Generation		■					□	
Other: AGRICULTURE								
Agricultural Uses, Animals		□						
Agricultural Uses, Crops		■						
Community Garden		■	■	■	■	■	■	■
Agritourism		■						
Greenhouse		■					■	
Stable		■						
Shelter or Boarding Kennel		■			□	□	■	

**TABLE 3-3:
PERMITTED USES**

	T1	T2	T3	T4-R	T4-O	T5	SD-W	C
Other: Automotive								
Gasoline								□
Automobile Service								□
Truck Maintenance								□
Drive-Through Facility						□	□	
Rest Stop								
Roadside Stand		■						

C. Employment Corridor (EC) Overlay: Use Modifiers

1. Applicability

- a. The provisions of this section apply only to properties located within the Employment Corridor (EC) Overlay as established in Division 2.
- b. These provisions modify the permitted use standards of Table 3-3 only within the EC Overlay.
- c. Where a conflict occurs between Table 3-3 and this section, the provisions of this section shall control.

2. Prohibited Uses within the EC Overlay

Notwithstanding any permissions in Table 3-3, the following uses are prohibited within the EC Overlay:

- a. Mini-storage / self-storage facilities;
- b. Stand-alone distribution centers where the primary function is regional or national goods movement rather than on-site employment-intensive activity;
- c. Truck stops, rest stops, or truck-oriented fueling facilities;
- d. Stand-alone surface parking lots as a principal use;
- e. Automotive service, truck maintenance, or drive-through facilities not clearly accessory to a permitted employment use.

3. Uses Permitted Only by Warrant within the EC Overlay

The following uses may be permitted within the EC Overlay only by Warrant, pursuant to Division 7, regardless of their status in Table 3-3:

- a. Lodging uses exceeding 150 rooms or 120,000 square feet of gross floor area;
- b. Large-scale entertainment or assembly uses exceeding 1,500 seats or 75,000 square feet;
- c. Utility-scale infrastructure or facilities not primarily serving on-site development;
- d. Any use that would result in non-employment uses exceeding the maximum percentages established in Division 2, Section 2.3.

4. Retail, Food, and Service Uses

- a. Retail, food and beverage, and personal service uses are permitted within the EC Overlay only when:
 - i. Such uses are allowed by the underlying Placetype and Transect Zone; and
 - ii. The cumulative gross floor area of such uses does not exceed the maximum allowed by Division 2, Section 2.3.
- b. Retail uses shall not be designed or configured as linear strip commercial development oriented exclusively to a highway or arterial frontage.

5. Residential Uses

- a. Residential uses are permitted within the EC Overlay only where allowed by the underlying Placetype and Transect Zone.
- b. Residential uses shall comply with the maximum gross floor area limitations established in Division 2, Section 2.3.
- c. Residential uses exceeding those limitations may be approved only by Warrant.

6. No Expansion of Permitted Uses

Nothing in this section shall be interpreted to authorize a use not otherwise listed or allowed in Table 3-3 or elsewhere in this Code.

DIVISION 4: GENERAL DEVELOPMENT STANDARDS

Standards that are not specifically addressed elsewhere in the Code are included in the General Development Standards. The purpose and intent of the General Development Standards are to provide provisions and regulations that apply to all Transect Zones, specifying the design of parking, Garden Walls and fences, signage, lighting, utilities, landscaping, stormwater management, and Environmental.



SECTION 4.1 PARKING STANDARDS

The intent of the parking standards is to encourage a balance between pedestrian-oriented development and necessary vehicle storage. The goal is to construct neither more nor less parking than is needed.

A. Parking Requirements

1. Parking shall be provided for each use based upon the minimum and maximum requirements outlined by use in Table 4-1: Automobile Parking Requirement Chart. Required parking quantities for a parcel may be modified by Warrant.
2. Parking may be located on the same Lot as the use it serves. Required parking may also be located on-Street or in a common parking lot, provided the space is within 1/4 mile of the building's Principal Entrance. The required parking may be purchased or leased from a public or private civic parking reserve, the owner of which will manage the purchase or lease. Off-site parking may also be under separate ownership if an agreement is provided guaranteeing the long-term availability of the parking, commensurate with the use served by the parking.
3. Parking shall be located behind the Principal Façade of buildings whenever possible and according to their allowed location based on Transect Zone.
4. Parking garages shall be masked from the Frontage by a Liner Building.
5. Streetscreens, Garden Walls, fences, or hedges are required along all un-built Street Right-of-Ways adjacent to surface parking lots to shield views to parking.
6. Parking for T1, Civic Buildings and Civic Open Spaces shall be determined by Warrant.
7. Shared and Reduced Parking is encouraged in all Transect Zones for more efficient parking solutions. The amount of parking required is calculated by adding the total number of spaces required by each separate function in the Parking Requirement Chart and dividing by the appropriate factor from the Sharing Factor matrix. For example, the residential function requires 10 spaces while the office function requires 12 spaces. Independently they would require 22 spaces, but when divided by the sharing factor of 1.4, they would require only 16 spaces. When multiple functions share parking, the lowest sharing factor shall apply.

TABLE 4-1: AUTOMOBILE PARKING REQUIREMENT CHART

Use	Number of Parking Spaces				
	T5 ¹	T4-O ² T4-R ²	T3 ²	T2 ²	SD-W ²
Residential (Primary Dwelling)	2 / dwelling ³	1 / dwelling ³	2 / dwelling ³	2 / dwelling ³	1 / dwelling ³
Lodging	1 / guest room				
Office	2 / 1,000 sq. ft.				
Retail	3 / 1,000 sq. ft.				
Civic	TBD by Warrant				
Education	1 per 12 students				
Other: General	TBD by Warrant				
Other: Agricultural / Industrial	1 per employee on largest shift				

¹ Maximum number of spaces permitted

² Minimum number of spaces required

³ Accessory Dwelling Units (ADUs) do not require additional off-Street parking

TABLE 4-2: SHARING FACTOR

Function	with		Function
RESIDENTIAL			RESIDENTIAL
LODGING			LODGING
OFFICE		1	OFFICE
RETAIL	1.4	1.1	1.1
	1.2	1.7	1.7
	1.3	1	1.3
	1.2	1.2	1.2
		1	

B. Bicycle Parking

Bicycle parking shall be provided in all Transect Zones per Table 4-3: Bicycle Parking Requirement Chart and subject to the subsections below:

1. Anchors: All spaces provided shall include a metal anchor sufficient to secure the bicycle frame when used in conjunction with a user-supplied lock.
2. Short-term bicycle parking accommodates bikes parked for short periods of time in locations that are easily accessible and convenient for visitors, customers and residents.
3. Short-term bicycle parking shall be located on sidewalk Frontage zones, bicycle corrals located in the Street parking lane, parks and other public facilities, and on private property. Bike parking shall not impede the sidewalk clear zone.
4. Long-term bicycle parking provides a place that is reasonably free from vulnerability to both weather and theft for bikes typically parked for periods of 8 hours or more and on a regular basis. This applies particularly to employees while at work and for residents of multi-family dwellings.
5. Long-term bicycle parking shall be located in a secure area covered from weather such as a building bike room, shared cage in a garage, or in a standalone enclosure such as a locker or structure.
6. Bike Parking in Public Parking Garages: Parking garages should include bike parking and a designated bike lane to enter/exit the garage. Bike parking should be provided on the ground floor.
7. Long-term bike parking facilities can be provided within parking garages, some of which can be made available for lease to non-residential uses to meet their long-term bike parking requirements.
8. Bicycle parking for Civic Buildings and Civic Open Spaces shall be determined by Warrant.

TABLE 4-3: BICYCLE PARKING REQUIREMENT CHART		
Use	Min. Number of Spaces	
	Short-Term	Long-Term
Multi-dwelling Residential	1 space per 5 dwelling units; 2 spaces min.	1.25 spaces per dwelling unit
Multi-dwelling Residential (ages 55+)	1 space per 20 dwelling units; 2 spaces min.	1 per 8 dwelling units; 2 spaces min.
Lodging	1 per 20 keys plus 1 per 4,000 SF of conference and meeting space	3 per 40 keys
Office	1 per 10,000 SF; 2 spaces min.	1 per 2,000 SF; 2 spaces min.
Retail	1 per 2,000 SF; 2 spaces min.	1 per 4,000 SF; 2 spaces min.
Eating and Drinking Establishments	1 per 1,000 SF; 2 spaces min.	1 per 2,500 SF; 2 spaces min.
Libraries, Community Centers, Museums	1 per 1,500 SF; 2 spaces min.	1 per 3,000 SF; 2 spaces min.
Industrial	1 per 4,000 SF; 2 spaces min.	1 per 4,000 SF; 2 spaces min.
Health Care Facilities and Nursing Homes (9+ residents)	1 per 2,000 SF; 2 spaces min.	1 per 4,000 SF; 1 space min.

C. Parking Access

1. Rear Alleys/Lanes, where proposed, shall be the primary source of access to off-Street parking. Parking along Alleys may be perpendicular, diagonal, or parallel.
2. Alleys may be incorporated into parking lots as standard drive aisles. Access between parking lots across Property Lines is encouraged.
3. Corner Lots that have both rear and side access shall access parking through the rear. If no rear access exists, access to on-Lot parking shall be provided from the side Street.
4. If no Alley or side Street exists, then efforts should be made to demonstrate an attempt to gain access across neighboring properties.
5. When access to rear parking must be directly from the Primary Frontage, driveways shall be located along the sides of the Property Lines and designed such that pedestrians crossing on sidewalks always have the right of way.
6. The maximum width of vehicular driveways are provided in the Transect Zone Standards.

D. Off-Street Surface Parking Lots

1. Minimum Setbacks for off-Street surface parking lots from all Property Lines are provided in the Transect Zone Standards.

E. Structured Parking Lot Placement

1. Structured Parking shall be lined with Liner Buildings along all adjacent Thoroughfares, except rear Alleys.
2. Liner Buildings, where utilized, shall be a minimum of two stories and may be attached or detached from parking structures.
3. Liner Buildings, where utilized, shall contain a minimum of 15 feet of Habitable Space behind each building Façade along the Street Frontage.

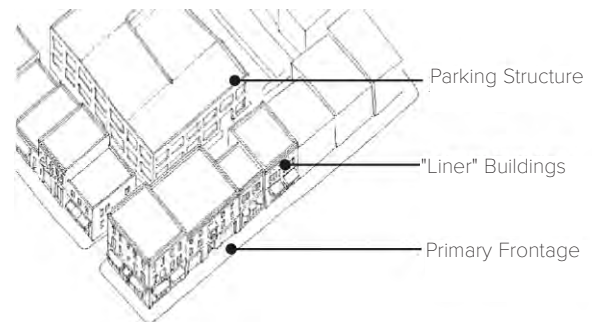


Figure 4-2: Structured Parking Lot Placement

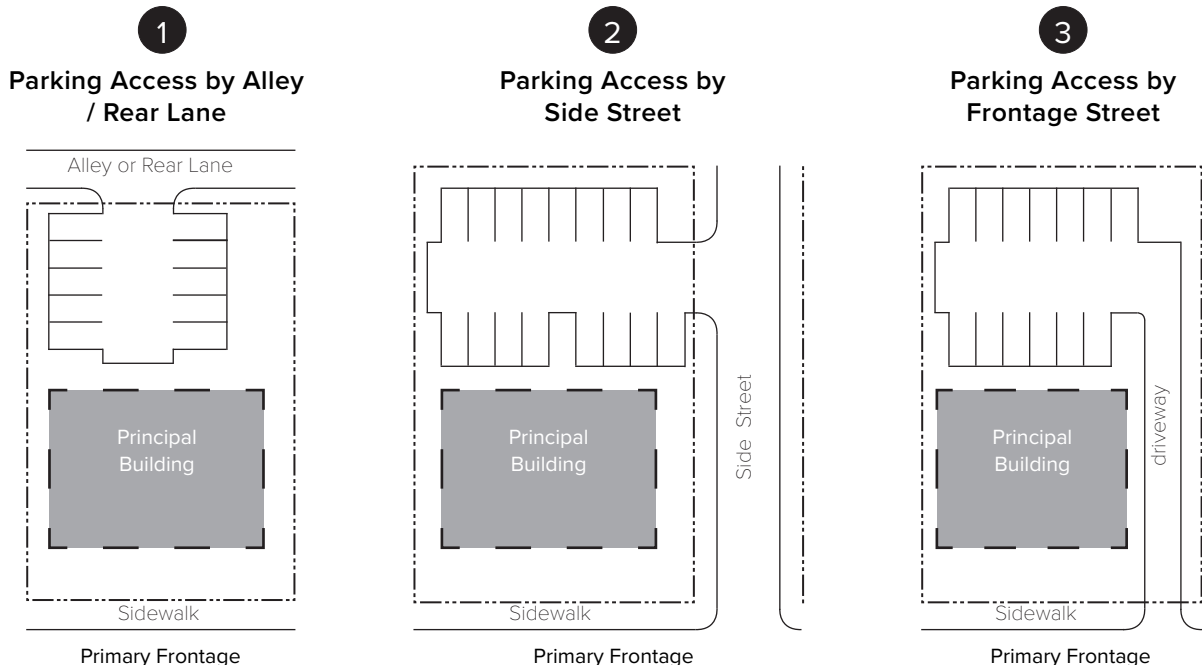


Figure 4-1: Parking Access Diagrams

SECTION 4.2 GARDEN WALLS, FENCES, AND HEDGES

A. Garden Walls, fences, or hedges may be located along Frontage Lines and other Lot Lines, or parallel with the Façades of buildings. When located along Frontage Lines, Garden Walls, fences, and hedges are called Streetscreens. Streetscreens may mask a parking lot from the Thoroughfare, provide privacy to a side yard, and/or strengthen the spatial definition of the Public Realm.

1. Streetscreens shall be a minimum of 2.5 feet tall in all Transect Zones. Maximum heights are established in Table 4-4.
2. All Streetscreens over 4 feet high should be a minimum of 30% permeable or articulated.
3. Streetscreens may be non-permeable by Warrant.
4. Streetscreens along surface parking lots shall have openings no larger than necessary to allow automobile and pedestrian access.
5. Streetscreens shall not be permitted in the Right-of-Way.
6. Can be located along the Property Line (Build to Zone or Setback does not apply).

B. Gates and Doorways

1. Pedestrian and vehicular gates or doorways within walls and fences shall be a maximum of twenty (20) feet in width, unless a wider opening is required for fire/emergency access.
2. Vehicular gates shall be no taller than the adjacent wall or fence height.
3. A framed entrance may be used over a pedestrian gate or doorway where there is a walkway. The framed entrance shall provide minimum of 6'8" height clearance, shall not exceed an overall height of 10', and must be outside of the Right-of-Way.

TABLE 4-4: GARDEN WALLS, FENCES, AND HEDGES

Maximum Height (feet) ¹	T2	T3	T4-R	T5	SD-W
			T4-O		
Primary Frontage and up to the Principal Structure	4	4	4	4	4
Other Frontages	6	6	6	6	6
Interior Side and Rear Lot Lines	8	6	6	6	8

¹ Garden Walls, fences, and hedges screening mechanical equipment and Dumpsters may be up to 8 feet in height.

SECTION 4.3 SIGNAGE STANDARDS

Signs in the Public Realm shall enhance the character of the Public Realm, provide orientation to pedestrians, cyclists, and motorists, and help to give identity to the Street and businesses along it. Signs should be designed and scaled for use by the pedestrian. The City of Greeley Sign Regulations shall apply within Greeley Westside except as provided in this section. To the extent that a conflict occurs between this section and the City of Greeley Regulations, the provisions of this section shall control.

A. Wayfinding Signs (Public ROW)

1. Signage should be coordinated with other Streetscape furniture (e.g., light posts) to reduce visual clutter in the Public Realm.
2. The Approval Authority shall set the pole and frame standard for use throughout Greeley Westside Subarea prior to approval of the first Final Site Plan for aesthetic conformity and maintenance inventory. Any signage, post, or frame to be maintained by the governing body shall be approved by the jurisdiction.
3. Wayfinding signage, which identifies key civic areas or public destinations, shall be consistent in theme and placement as determined by the Approval Authority.
4. Architectural features and gateways announcing arrival to the entire community or individual neighborhoods may have identification signs of no more than 36 square feet.

B. Commercial Signs

1. Free standing signs, ground signs, and monument signs are not permitted. All signs shall be attached to the Façade. Signs may be flat against the Façade, or mounted, projecting or hanging from the Façade.
2. The maximum gross area of signs on a given Façade shall not exceed 10 percent of the Façade area. Signage painted on a building Façade or mounted on the roof may exceed this limit, with approval as a Special Sign.
3. Signs mounted on the Façade shall maintain a minimum clear height above sidewalks and other pedestrian areas of 9 feet. Signs shall not extend within 2 feet of the curb line.
4. Projecting signs shall not extend within 2 feet of the curb line and shall not be placed closer than 16 inches apart.

5. The maximum area of any single sign mounted perpendicular to a given Façade shall not exceed 9 square feet in the T5 and SD-W Zones and shall not exceed 6 square feet in other Transect Zones.
6. A single external Sign Band may be applied to the Façade of each building, provided that such sign not exceed 3 feet in height by any length.
7. Permitted finish materials include: wood (painted or natural); metal (copper, brass, galvanized steel); painted or printed canvas; painted/engraved directly on Façade surface.
8. External neon signs are permitted in the T5 Transect Zone only. Signage within a Storefront may be neon-lit in any Transect Zone where Storefronts are permitted.
9. Special Signs may be permitted when, following review, the Design Review Board or Approval Authority finds such signs to meet the intent of the FBC.

C. Banner Signs (Public ROW)

1. The use of banner signs in the Public ROW shall be limited to the promotion of public events and activities, or to identify a district.
2. Banner signs may be mounted on light poles or other Street furniture designed specifically for such a purpose.
3. Temporary banner signs not exceeding 4 feet in height and forty feet in length may be hung over the public Right-of-Way with a minimum vertical clearance of 14 feet.

D. Temporary Sidewalk Signs

1. Temporary Sidewalk Signs are permitted on public sidewalks immediately adjacent to a building being occupied by the advertiser on the sign.
2. Each business may have up to two Sidewalk Signs.
3. The placement of signs on the sidewalk must maintain a clear sidewalk path of a minimum dimension of 6 feet.
4. The dimensions of the sign shall be no greater than 3 feet wide and 5 feet high.



Wayfinding Sign



Blade/Projecting Sign



Hanging Sign



Awning Sign



Wall Sign



Painted Wall Sign



Cornice Sign



Banner Signs



Sidewalk Sign

Figure 4-3: Examples of Permitted Commercial Signage. These examples are not inclusive of all permitted commercial signage types and designs, but are for illustrative purposes only to demonstrate the intent of the commercial signage standards.

SECTION 4.4 LIGHTING STANDARDS

Adequate and quality lighting of the sidewalk and Street area is essential to creating a safe and inviting Streetscape.

A. General Public Realm Lighting Standards

1. Lighting fixtures shall be appropriately chosen for Greeley Westside Subarea. There shall be consistency in creating a unifying scheme of illumination that is appropriate to the scale of the Street and the level of evening activity.
2. Lamp styles should not be mixed along any one particular Block of a Street.
3. Light fixtures shall be downcast or low cut-off fixtures to prevent glare and light pollution.
4. In order to conserve energy and reduce long-term costs, energy-efficient lamps shall be used for all Public Realm lighting.

B. Light Levels

1. Lighting standards protect against glare, preserve the night sky, and reduce unnecessary energy use from overlighting. Rural zones tend to be darker, while higher levels of outdoor lighting may be more suitable in mixed-use urban zones.
2. It is the intent of this Section to follow Dark Sky provisions as established by the International Dark-Sky Association (IDA). All outdoor lighting within the Public Realm shall be IDA-Approved fixtures.
3. The standards in Table 4-5 maintain the desired general ambient light levels across the Transect. Light levels in the Civic Transect Zone shall be consistent with the intent of this Section and not contribute to excessive light pollution, as determined by the Approval Authority.

C. Street Lighting

1. See Section 6.5: Street Lighting

D. Parking Lot Lighting

1. All fixtures shall be full cut-off, downward-facing.
2. Light fixtures located within the interior area of a parking lot shall not exceed 30 feet in height. Light fixtures located along the perimeter edge of a parking area within 50 feet of a Property Line shall not exceed 16 feet.

E. Pedestrian Walkway Lighting

1. Light fixtures located along pedestrian walkways adjacent to parking lots shall not exceed 16 feet in height.
2. Light fixtures located along internal pedestrian walkways or paths not adjacent to a parking area shall not exceed 10 feet in height.

F. Building and Security Lighting

1. All exterior building or security lighting must be full cut-off, shielded, and/or angled to focus the light only on the intended building element, surface, doorway, or walkway, as necessary.

TABLE 4-5: LIGHT LEVELS

	T2	T3	T4-R T4-O	T5	SD-W
Ambient Light Levels	Very low		Low	Medium	
Standards					
Maximum Lighting Standards	Minimal lighting, all Full Cut-off		Full Cut-off lighting	Full Cut-off lighting, some low wattage, non-Full Cut-off lighting	
No lighting level measured at the building Frontage Line shall exceed:	1.0 fc		1.0 fc	2.0 fc	
Required Shielding	Fully shielded Luminaire with no uplight or better		Shielded Luminaire or better	Partially shielded Luminaire or better	

SECTION 4.5 UTILITIES

A. General Standards

2. Security lighting is encouraged to be provided with regular pedestrian light fixtures where visible from the Street or Public Realm to match others used on site.
 3. Building-mounted architectural “accent lights” are encouraged to emphasize architectural character and signage.
 4. Business owners are encouraged to assist with lighting the sidewalk and accent their business location by leaving display window and interior lighting on at night. Lighting shall be designed in such a way as to prevent the direct view of the light source to neighboring residential areas.
 5. Edges of Civic Open Spaces, especially Plazas and Squares, should be lit along the Right-of-Ways to define and identify the space.
 6. Focal points such as sculptures, fountains, and towers, especially those visible to pedestrians and vehicles, may be illuminated to call attention to the element and provide a form of wayfinding.
1. With the exception of fire hydrants, utilities shall run underground, and above-ground projections of utilities shall be placed in rear service areas wherever practicable.
 2. All utility services should be located in the rear or side of buildings, clear of pedestrian and vehicular interaction, and screened from view from adjacent public Right-of-Ways, properties, and pedestrian walkways (not including rear Alleys).
 3. Tree wells or root barriers should be used where proposed Street trees are near proposed underground infrastructure lines or adjacent to a travel way (vehicular or pedestrian).
 4. Rear Alleys and portions of the Thoroughfare Right-of-Way should serve as designated drainage and utility corridors/easements.
 5. Where special circumstances dictate utility easements are needed between Lots, utility easements shall be a minimum width as required by the utility agency and approved by the Approval Authority.

SECTION 4.6 LANDSCAPE STANDARDS

A. General Landscape Standards

1. Landscape design shall enhance the quality and character of the Public Realm by coordinating public and private spaces, providing spatial definition, screening undesirable views, sounds, and odors, and promoting safety, health, and comfort. Landscaping shall complement the architectural design of mixed-use development.
2. Design of landscape shall maximize the use of green infrastructure and stormwater Best Management Practices (BMPs), such as pervious paving, bioretention systems, rain gardens, bioswales, and stormwater planters, to slow and treat runoff while providing ecological and community benefits.
3. Plant selection and placement shall prioritize drought-tolerant, low-water-use species adapted to Colorado's climate and soils. Plant varieties shall be chosen for resistance to drought, moisture extremes, salts, urban conditions, pests, and other local stressors. Landscaping shall be designed to minimize the need for irrigation, fertilizers, and pesticides while still achieving intended aesthetic, functional, and ecological goals. Native species are strongly encouraged, and irrigation shall be used only as needed to establish new plantings.
4. Existing healthy trees and shrubs shall be preserved to the greatest extent feasible during development.

B. Native Vegetation

The purpose of these regulations is to establish minimum standards for appropriate native vegetation to preserve indigenous plant communities and support ecosystem functions.

1. A minimum of 25% of all landscaped areas shall be planted with appropriate native vegetation. Maximizing native plant coverage up to 100% is encouraged.
2. Exemptions to minimum native coverage include: active agricultural areas, community gardens, play areas, civic open spaces, and non-invasive food plants.
3. The planting of invasive exotic species is strictly prohibited.
4. No City, County, or private action shall prevent the planting, maintenance, or protection of native vegetation except when required for public safety.

C. Parking Lot Landscape Standards

The following standards reduce the visual impact of paved areas, provide shade, reduce heat, improve stormwater management, and enhance aesthetics.

1. Developments with 6 or more parking spaces shall provide a minimum of 10% landscaped open space within the parking area, including landscaped borders. Landscaped areas may include vegetated stormwater BMPs.
2. Surface parking lot entrances shall be landscaped with trees, shrubs, walls, or other features. Landscaping shall not obstruct driver sightlines.
3. Ends of parking aisles longer than 15 spaces shall include landscape islands with at least one tree. For aisles exceeding 25 spaces, additional islands shall be installed at intervals no greater than every 13 spaces. Islands shall be at least 6 feet wide perpendicular to parking spaces.
4. Trees shall also be planted along lot lines and buffers at a minimum spacing of one tree every 40 linear feet. Trees must be selected and placed to achieve an expected 30% canopy coverage over parking areas. Canopy radius for each species shall be documented in site plans.

D. Street Trees

1. Refer to Section 6.4 Street Trees. Species selection shall prioritize low-water-use and drought-tolerant trees suitable for Colorado conditions.

E. Irrigation Systems

1. Irrigation shall be used only as necessary to establish new plantings. When provided, systems shall utilize water-efficient methods such as drip irrigation, low-volume heads, moisture sensors, or other sustainable technologies. Overwatering or inefficient irrigation practices are prohibited.

SECTION 4.7 STORMWATER MANAGEMENT STANDARDS

A. General Standards

1. The objectives of the stormwater management standards are to:
 - a. Reduce water quality impacts at receiving waters,
 - b. Enhance community character in support of compact development, and
 - c. Promote public health, safety, and welfare.
2. Stormwater management should prioritize water conservation, compliance with Colorado water rights, and use of native or drought-tolerant landscaping. The goals include:
 - a. Manage rainfall as close to where it falls as possible, approximating natural pre-development hydrology, while respecting soil infiltration capacity and site constraints.
 - b. Recognize stormwater as an integral part of the built environment.
 - c. Establish watershed-sensitive planning and design criteria at the neighborhood scale to support shared flood control solutions.
 - d. Encourage incorporation of Light Imprint BMPs at the Block, Street, and Site scales, appropriate to land use context, site conditions, and climate considerations.
 - e. Develop a Stormwater Management System for each neighborhood as a whole, implemented within the Final Site Plan.
 - f. Stormwater management standards for individual Lots may assist in meeting neighborhood-wide objectives, provided legal constraints on stormwater capture and use are followed.

B. Light Imprint Storm Drainage Methods

1. Table 4-6 outlines recommended stormwater management methods from the Light Imprint Handbook. At least one method should be applied at the neighborhood, corridor, and Lot levels to implement the neighborhood stormwater strategy.
2. Methods appropriate for the Civic Transect Zone shall be determined based on surrounding Transect Zones and site-specific conditions, including soil infiltration capacity, cold-climate performance, and water availability.

C. Design Criteria

1. Pervious Paving
 - a. Pervious paving is permitted and encouraged to reduce runoff volume.
 - b. Site-specific soil testing is required to ensure feasibility. Pervious paving may be infeasible in clay soils, contaminated sites, or areas subject to freeze-thaw cycles.
2. Green Roofs
 - a. Permitted for all building types.
 - b. Plant selection should prioritize drought-tolerant, native species. Structural design must account for snow loads and roof drainage.
3. Roof Drainage
 - a. Roof drains shall not discharge onto impervious pedestrian areas. They should be directed to underground storm drainage systems or vegetated stormwater management systems designed for Colorado soils and climate.
4. Irrigation and Water Use
 - a. Irrigation systems may utilize retained or detained stormwater only where legally permitted.
 - b. Landscapes should prioritize xeriscaping, native/adapted plants, and water-efficient irrigation systems. Stormwater capture for irrigation must comply with Colorado water rights and municipal requirements.
5. Vegetated Stormwater Management Systems
 - a. Bioretention systems, rain gardens, bioswales, tree filters, and other vegetated BMPs are encouraged.
 - b. Use underdrains or impermeable liners where infiltration is limited due to soil conditions or contamination.
 - c. Vegetation should be drought-tolerant and suitable for Colorado's climate.

TABLE 4-6: LIGHT IMPRINT STORM DRAINAGE

	T1	T2	T3	T4	T5	SD-W
Paving						
Compacted Earth	■	■	■			
Wood Planks	■	■	■			
Plastic Mesh/Geomat		■	■	■		
Crushed Stone/Shell		■	■	■		
Cast/Pressed Concrete Paver Block			■	■	■	■
Grassed Cellular Plastic			■	■	■	■
Grassed Cellular Concrete			■	■	■	■
Previous Asphalt			■	■	■	■
Asphalt			■	■	■	■
Concrete			■	■	■	■
Pervious Concrete			■	■	■	■
Stamped Asphalt			■	■	■	■
Stamped Concrete			■	■	■	■
Pea Gravel				■	■	■
Stone/Masonry Paving Blocks				■	■	■
Wood Paving Blocks on Concrete					■	■
Asphalt Paving Blocks					■	■
Channeling						
Natural Creek	■	■				
Terracing	■	■	■			
Vegetative Swale	■	■	■	■		
Drainage Ditch	■	■	■			
Stone/Rip Rap Channels		■	■	■		
Vegetative/Stone Swale		■	■	■	■	■
Grassed Cellular Plastic			■	■	■	■
Grassed Cellular Concrete			■	■	■	■
Soak-away Trench			■	■	■	■
Slope Avenue			■	■	■	■
French Drain			■	■	■	■
Shallow Channel Footpath/ Rainwater Conveyor			■	■	■	■
Concrete Pipe			■	■	■	■
Gutter			■	■	■	■
Planting Strip Trench			■	■	■	■
Masonry Trough				■	■	■
Canal		■	■	■	■	■
Sculpted Watercourse, i.e. cascades			■	■	■	■
Concrete Trough					■	■
Archimedean Screw					■	■

	T1	T2	T3	T4	T5	SD-W
Storage						
Irrigation Pond		■	■			
Retention Basin with Sloping Bank		■	■			
Retention Basin with Fence		■	■	■		
Retention Hollow			■	■		
Detention Pond			■	■		
Vegetative Purification Bed			■	■	■	■
Flowing Park			■	■	■	■
Retention Pond			■	■	■	■
Landscaped Tree Well				■	■	■
Pool/Fountain				■	■	■
Underground Vault/Pipe/Cistern- Corrugated Metal				■	■	■
Underground Vault/Pipe/Cistern- Precast Concrete				■	■	■
Underground Vault/Pipe/Cistern- Cast in place Concrete				■	■	■
Underground Vault/Pipe/Cistern- Plastic					■	■
Grated Tree Well					■	■
Paved Basin					■	■
Filtration						
Wetland/Swamp	■	■				
Filtration Ponds	■	■				
Shallow Marsh	■	■	■			
Surface Landscape	■	■	■			
Natural Vegetation	■	■	■	■	■	■
Constructed Wetland		■	■			
Bio-retention Swale		■	■	■	■	■
Purification Biotype		■	■	■	■	■
Green Finger		■	■	■	■	■
Roof Garden		■	■	■	■	■
Rain Garden		■	■	■	■	■
Detention Pond			■	■		
Grassed Cellular Plastic			■	■		
Grassed Cellular Concrete			■	■		
Waterscapes			■	■	■	■

6. Trees and Planting

- a. Trees planted in tree wells, plazas, or sidewalks shall have adequate soil volume and structural cells to ensure healthy growth and support stormwater management.
- b. Use native or drought-tolerant tree species. Supplemental irrigation should be minimized.

7. Special Detention Areas

- a. Parking lots, rooftops (“blue roofs”), parks, plazas, and fields may serve as temporary detention areas if:
 - i. Ponding does not disrupt pedestrian, bicycle, or vehicle traffic.
 - ii. Areas drain completely after storms.
 - iii. Emergency overflows prevent excessive water depths. Ponding generally shall not exceed 12 inches.
- b. Rooftop detention must account for structural support, HVAC, waterproofing, and emergency overflow requirements.
- c. High-intensity public areas (parks, athletic fields) must be located on well-draining soils to avoid prolonged wet conditions.

SECTION 4.8 ENVIRONMENTAL

A. General Provisions

This section applies to the T1 Natural Zone within the arroyos and their protected buffers.

- 1. Native vegetation shall be protected and restored in preserve areas, except for limited clearing required for trails, shared-use paths, bridges, and supporting infrastructure. These areas shall be maintained free of invasive exotic plant species.

B. Zero-Waste Provisions

- 1. Opportunities for recycling and composting shall be provided wherever trash receptacles are located.
- 2. Public areas such as sidewalks, squares, and parks shall have three-bin collection systems, with instructional signage, to collect landfill, compostable, and recyclable materials.

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DIVISION 5: LOT & BUILDINGS STANDARDS

This Division establishes standards for Lots and individual buildings within the Greeley Westside Subarea. Topics include Building Types, Accessory Units, Building Standards, Façades, Frontage Types, Green Building, and Site Standards.



SECTION 5.1 LOT STANDARDS

A. Front and Backs

Buildings and Lots have fronts, sides, and backs, and how these relate to one another forms neighborhood character.

1. Front Façades, the main presentation faces of buildings and Lots containing the Principal Entrance, should face the Public Realm.
2. The backs of buildings and Lots, which are the private or service sides, should face mid-block and be screened from view. Backs of buildings or Lots shall not abut the Frontage Line.
3. The sides of buildings and Lots may face either the Frontage Line or be concealed mid-block.
4. Thoroughfares, with the exception of Alleys, should be faced with the fronts or sides of buildings and Lots.
5. Alleys and mid-block parking areas should be faced with the backs or sides of buildings and Lots.
6. The backs of buildings and Lots should not be across from, or adjacent to, a Civic Open Space.
7. The backs of buildings and Lots shall not be across from, or adjacent to, a Plaza, Square, or Green.
8. The backs of buildings and Lots shall not face Civic Building Frontages.

B. The table below outlines the range of relationships between the fronts, sides, and backs of buildings/Lots.

Fronts facing Fronts	Ideal
Fronts facing Sides	Acceptable
Fronts facing Backs	Prohibited
Sides facing Backs	Acceptable
Backs facing Backs	Ideal
Sides facing Sides	Ideal

SECTION 5.2 RESIDENTIAL BUILDING TYPES

A mix of residential building types creates neighborhoods which allow a diversity of ages and incomes, and permit residents to move up or downsize their homes without having to move away. Multi-generational neighborhoods create strong social networks, avoid concentrations of poverty or wealth, and lead to safer communities. The residential building types permitted within the Greeley Westside Subareas are described textually below and then through illustrations.

A. Mixed-use Building/Block

This type features Storefronts on the ground floor with space designed to accommodate residential or office on the floors above. Mixed-use buildings often have a parapet with a pronounced Cornice.

B. Apartment Building

This type contains multiple units accessed via a main entrance on the Primary Frontage. Units may be for rent or for sale as a condominium.

C. Quadplex

A multi-unit residential building with four dwelling units, either in a stacked or side-by-side configuration, this type is designed to maintain a house-like scale and Street presence appropriate for walkable neighborhoods.

D. Triplex

A small-scale multi-unit residential building designed to resemble a single-family home in form and scale, this type contains three distinct dwelling units, either stacked vertically or arranged side-by-side, and is typically located on a single Lot.

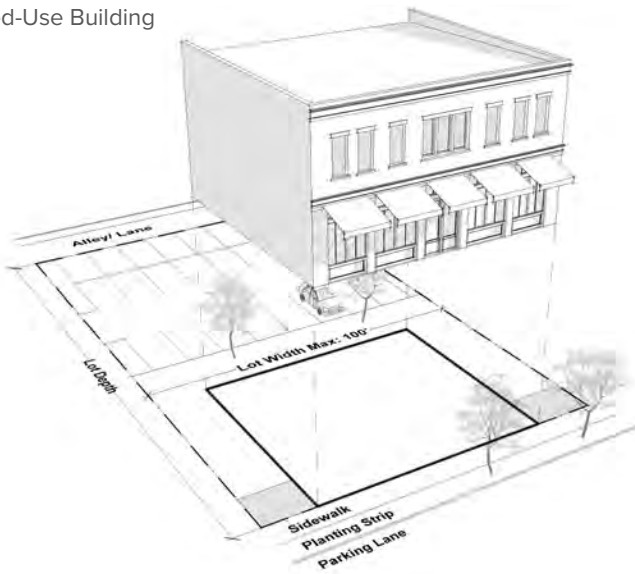
E. Live/Work Unit

This type contains buildings or structures used jointly for commercial and residential purposes where the residential use of the space is secondary or accessory to the primary place of work. The commercial function may be anywhere in the unit. It is intended to be occupied by a business operator who lives in the same structure that contains the commercial activity or industry.

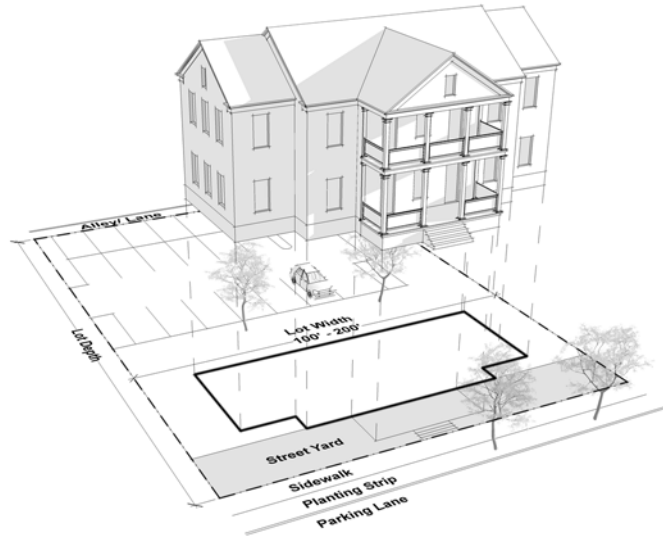
F. Townhouse

This is a single-family attached building type. Adjacent dwellings may share a party wall with another unit of the same type and occupies the full Frontage Line. Townhouses typically feature a private yard or patio between the main structure and the rear out-building.

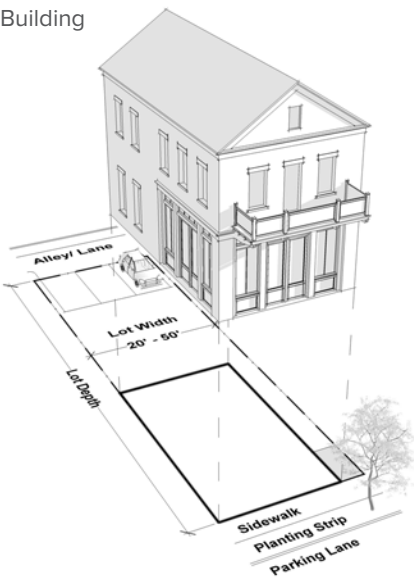
Mixed-Use Building



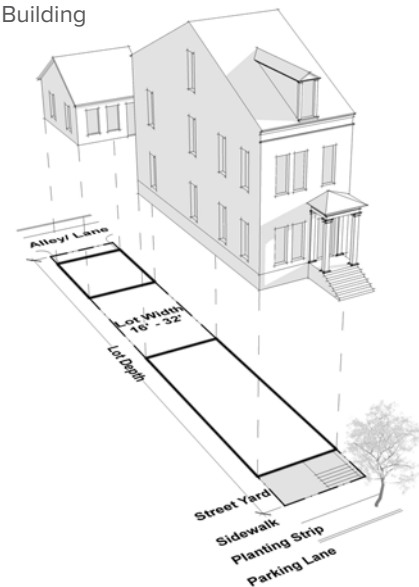
Quadplex Building



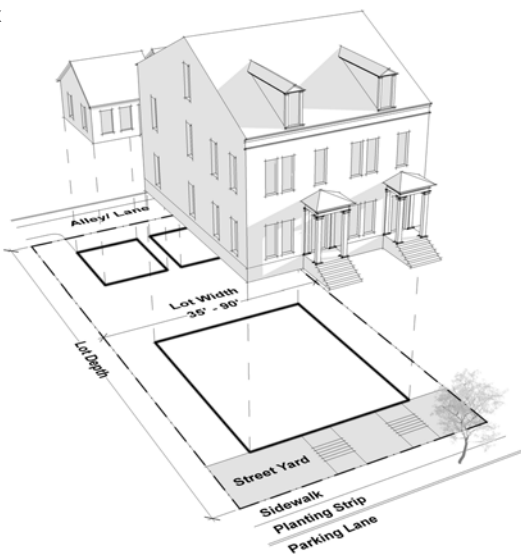
Live/Work Building



Townhouse Building



Duplex



House & Accessory Dwelling Unit

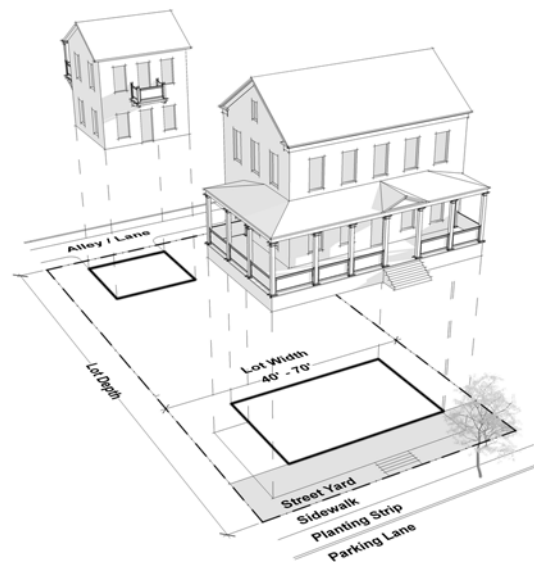


Figure 5-1: Illustrative examples of select residential building types

G. Duplex

This type is a residential building designed to resemble a single-family home in form and scale, but contains two distinct dwelling units. These units may be arranged side-by-side (horizontal duplex) or stacked vertically (upper/lower duplex), and the building is typically located on a single Lot.

H. House Types

1. Courtyard House

A dwelling that occupies the boundaries of its Lot while internally defining one or more private patios.

2. Sideyard House

A dwelling that is zero Lot Line on one side and occupies one side of the Lot with a Setback on the other side. This type can be a single or duplex, depending on whether it abuts the neighboring house.

3. Cottage

The smallest of the single-family house types and generally has a small front Porch.

4. House

Generally two-stories in height and often contains habitable attic space. Often shared with an Accessory Building in the back yard.

5. Villa

Typically two-stories in height and often contains habitable attic space. The type is typically located on the edge of a neighborhood and is sited on a very large Lot of rural character, often shared by one or more Accessory Buildings.

I. Accessory Dwelling Unit

Also referred to as accessory apartments, second units, granny flats, or ADUs are additional living quarters on single-family Lots that are independent of the primary dwelling unit. The separate living spaces are equipped with kitchen and bathroom facilities and can be either attached or detached from the Principal Building.

J. Residential Convertible to Retail

A flexible mixed-use or live/work building type where the ground floor is initially occupied with residential units that can later be converted to commercial use as the market matures.

SECTION 5.3 ACCESSORY DWELLING UNITS

- A. One Principal Building and one Accessory Dwelling Unit may be built by-right on each single-family or Duplex Lot, as permitted by Table 3-3.
- B. Accessory Dwelling Units shall not exceed 900 square feet in Habitable Space, excluding parking.
- C. Each single-family (attached or detached) Lot may accommodate one Accessory Dwelling Unit in the T2, T3, T4-R, T4-O, and T5 Transect Zones.
- D. Accessory Dwelling Units shall be limited to 2 Stories, including ground floor parking. These units are not computed towards overall density or unit calculations.
- E. Accessory Dwelling Units do not require additional off-Street parking.

SECTION 5.4 BUILDING HEIGHT

General Building Height information is provided below. Refer to the Transect Zone Standards for Setback and height information specific to each Transect Zone.

- A.** A Story is that part of a building contained between any finished floor and the floor or roof next above. Habitable attics (space within the roof structure), basements and underground parking are permitted and are not considered Stories for the purpose of determining Building Height.
- B.** Stories may not exceed 14 feet in height from finished floor to finished ceiling, except for a first floor commercial function, which shall be a minimum of 12 feet and may be a maximum of 25 feet. A single floor level exceeding 14 feet, or 25 feet for ground floor commercial, shall be counted as two-stories. Mezzanines extending beyond 33% of the floor area shall be counted as an additional Story.
- C.** Building Height shall be measured in Stories.
- D.** These regulations shall not limit or restrict the height of chimneys, clock towers, cooling towers, elevators, bulkheads, grain elevators, stacks, water towers, ornamental towers or similar elements that do not add habitable floor area to a building, wireless towers for amateur radios, skylights, flag poles, mechanical equipment such as ventilation equipment required to ventilate the building, stairs, open guard rails, roof-mounted solar panels, air pollution abatement equipment, or any similar appurtenances to buildings, but these structures shall be subject to such restrictions and regulations as may be imposed by the provisions of other City resolutions and regulations.

SECTION 5.5 GENERAL BUILDING REQUIREMENTS

A. Principal Entrances

The Principal Entrance of every Principal Building must be located along the Front Façade and directly face a Street or Civic Open Space. Additional building entrances are permitted.

B. Entry/Exit Doors

Public entry and exit doors that swing outward shall be recessed into the Façade a minimum of 3 feet where the sidewalk abuts the building.

C. Window and Door Openings

1. Window and door openings in masonry Façades should express a structural lintel above to express the conveyance of building weight. A similar method using wood trim can be used on wood-clad Façades.
2. Windows and doors shall be vertically proportioned or subdivided to appear vertical.

D. Columns/Posts

1. The proportion of structural elements such as columns or posts should be appropriate to the weight they appear to be carrying.
2. Columns and posts shall not be spaced further apart than they are tall.

SECTION 5.6 FAÇADES

A. Material Changes

1. When materials are combined on a building Façade horizontally, heavier materials should occur below lighter materials.
2. For buildings of three stories or more, the ground floor should be differentiated from those floors above in order to reinforce the pedestrian space.
3. Changes from one material or color to another along the horizontal direction should occur at “inside corner” transitions, as illustrated in Figure 5-2.
4. Changes in material or color along the vertical direction should occur at a hard-edge “bump-out” transition, which gives materials a surface to terminate into.
5. Façades with an overabundance of different materials or colors are generally discouraged.

B. Wide Façades

Building Façades longer than 60 feet shall be varied with at least one change of architectural expression. These changes in expression may be a vertical element running from the ground plane to the roof, a change in fenestration, color, or texture, or a break in building Façade plane or roof line. These changes may be subtle or significant but should soften the visual effect of very wide buildings, especially those directly across the Street from narrower buildings. Changes should appear authentic when subdividing a large Façade into multiple smaller Façades resembling distinct buildings.



Figure 5-2: Material or color changes on a building Façade should occur at an “inside corner,” as shown here.

C. Façade Transparency

All building Façades that face onto a Street or Civic Open Space in the T5, T4-O, T4-R, or T3, or SD-W Transect Zones shall meet the minimum transparency requirements outlined herein. The percentage of transparency per Story shall be calculated within the area between finished floor and finished ceiling and shall be a total percentage of doors and windows along that portion of the Façade.

1. Buildings with Storefront

- a. Minimum building Façade Transparency for ground Story: 50 percent and should allow a view of at least 5 feet of interior space.
- b. Minimum building Façade Transparency for upper Stories: 40 percent.

2. Buildings without Storefront

- a. Minimum building Façade Transparency for ground Story: 30 percent.
- b. Minimum building Façade Transparency for upper Stories: 20 percent.



Figure 5-3: Façade Transparency

SECTION 5.7 STOREFRONTS

- A.** A Storefront Frontage is required for all ground floor retail uses.
- B.** The entrances to all Storefronts shall be covered, either by an Awning, Canopy, Marquee, second floor Balcony, arcade/colonnade, or by being inset into the main body of the building.
- C.** Open-ended and operable Awnings are encouraged. Rounded and hooped Awnings are discouraged.
- D.** Folding or Garage doors that open and close a restaurant or retail space to the Street are permitted in place of Storefront windows. Such doors shall extend from the adjacent sidewalk to at least 8 feet above the adjacent sidewalk.
- E.** Storefront windows may not be made opaque by window treatments (excepting operable sunscreen devices within the conditioned space).
- F.** Doors or entrances for public access shall be provided at intervals no greater than 50 feet, unless otherwise approved. The intent is to maximize Street activity, provide pedestrians with frequent opportunities to enter buildings, and minimize any expanses of inactive wall.
- G.** Storefronts shall have a Cornice or Expression Line between the first and second Story.
- H.** Storefronts shall meet the dimensional requirements of Table 5-1.

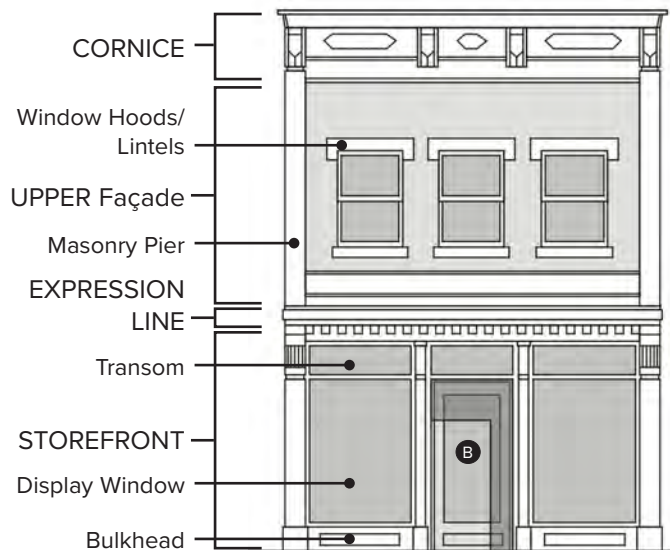


Figure 5-4: Anatomy of a storefront

TABLE 5-1: STOREFRONT DIMENSIONAL REQUIREMENTS

Distance between Glazing	2' max.	(A)
Door Recess ¹	5' max.	(B)
Habitable Space Depth	15' min.	
Window Sill Height	3' above sidewalk max.	(C)
Storefront Window	8' min. above sill	(D)

¹ A recessed entry may be designed in a variety of configurations (recessed door, sawtooth pattern, etc.) and may be located on the Front Façade or the corner of a building. An exception to meet ADA ramp compliance between sidewalk height and inside finished floor is permitted.

SECTION 5.8 BUILDING ELEMENTS

A. Small Footprint Towers/Cupolas

These features are designed to extend above the roof line and are generally intended to be visual landmarks. They are commonly placed to terminate vistas.

1. In those locations where a Terminated Vista is indicated on an approved Regulating Plan, Towers/Cupolas shall be provided and:
 - a. Towers/Cupolas with a footprint smaller than 30 feet by 30 feet may extend up to 30 feet above the designated height limit.
 - b. Towers/Cupolas with a footprint smaller than 20 feet by 20 feet may extend up to 40 feet above the designated height limit.
2. Towers/Cupolas are additionally permitted in all other locations. However, in locations not indicated as a Terminated Vista on the Regulating Plan, Towers/Cupolas shall not exceed 30 feet by 30 feet in footprint and shall not exceed 20 feet above the height limit.

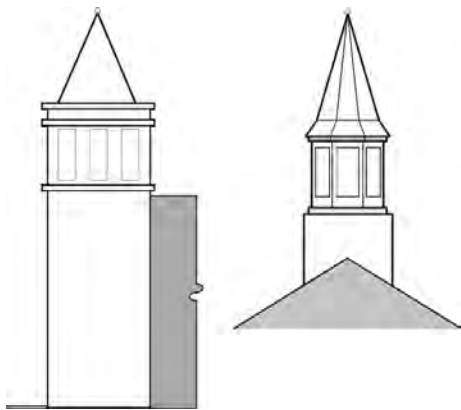


Figure 5-5: Small Footprint Tower and Cupola



a

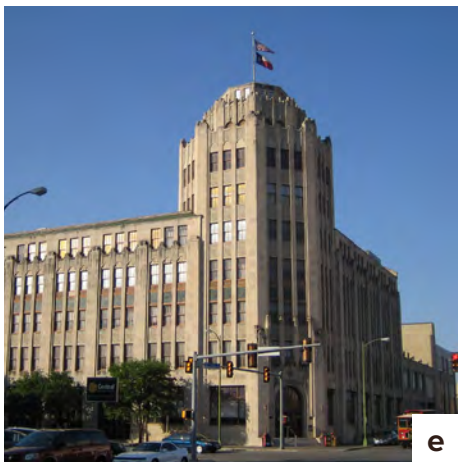


d



h

Figure 5-6: Examples of Small Footprint Towers/Cupolas



- a. Square Corner Tower with pyramidal roof
- b. Pedimented cupola
- c. Tall domed lantern cupola
- d. Curved corner with domed cupola
- e. Octagonal Corner Tower with stepped-back top
- f. Cylindrical Corner Tower with domed roof

- g. Corner steeple with square base and octagonal roof
- h. Chamfered corner with octagonal mansard roof form
- i. Stepped square-top lantern
- j. Square Tower with corbelled top
- k. Pedimented Tower with balustraded mansard roof

B. Appurtenances & Encroachments

Appurtenances are structural or architectural elements, such as Balconies, bay windows, Awnings, and other Frontage Elements that extend from the primary mass of the building. Requirements and standards for Encroachments are provided in the Transect Zone Standards for each Transect Zone and the Frontage Type Standards.

1. Balconies

- a. The minimum Balcony depth is 3 feet measured perpendicular to the wall face. Balconies may be inset either partially or wholly within the main body of the building.
- b. The minimum underside clearance of a first floor Balcony is 8 feet.
- c. Balconies may occur forward of the build-to line or zone, and may encroach within the right-of-way with special easement permission, but shall not extend closer than 2 feet from the curb line.
- d. Balconies shall be permitted to have roofs but are required to be open, unconditioned parts of buildings.
- e. Balconies must be visually supported from below by brackets or another structurally implicit mechanism from above by suspension cables or chains or adjacent side walls (if the Balcony is set completely within the main body of the building).
- f. On corners, balconies shall be permitted to wrap around the side of the building facing the adjacent Street.

2. Awnings/Canopies

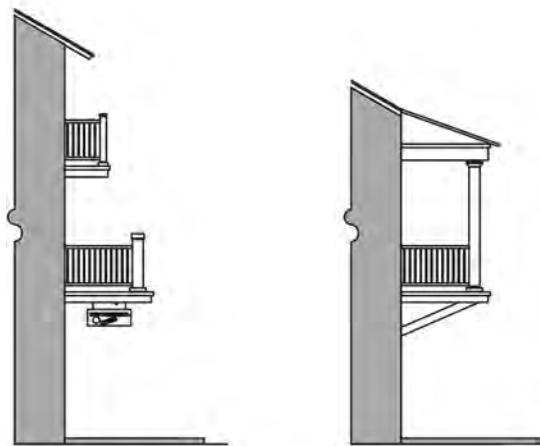


Figure 5-7: Balcony

- a. The minimum Awning depth for a first floor Awning is 4 feet measured perpendicular to the wall face.
- b. The minimum underside clearance of a first floor Awning is 8 feet.
- c. If a building Façade has Awnings, they shall occur for at least 50% of that Façade.
- d. Awnings may occur forward of the build-to line or zone, and may encroach within the right-of-way, but shall not extend closer than 2 feet to the curb line.
- e. Awnings shall be made of durable fabric and may be either fixed or retractable. High-gloss or plasticized fabrics are prohibited. Backlit Awnings are also prohibited.

3. Marquees

- a. The minimum marquee depth is 5 feet measured perpendicular to the wall face.
- b. The minimum underside clearance is 9 feet.
- c. The above requirements apply to first floor marquees. Marquees above the first floor are not permitted.
- d. Marquees may occur forward of the build-to line or zone, and may encroach within the right-of-way, but shall not extend closer than 2 feet from the curb line.

SECTION 5.9 FRONTAGE TYPES

Building Frontages shall conform with the basic Frontage Types described in this section. The illustrations and photographs provided are for illustrative purposes.

A. Common Yard

A planted Frontage wherein the Façade is set back substantially from the Frontage Line. The front yard created remains unfenced and is visually continuous with adjacent yards, supporting a common landscape. The deep Setback provides a buffer from the higher speed Thoroughfares. This Frontage can be combined with a Porch or Stoop.

B. Porch

A planted Frontage wherein the Façade is set back from the Frontage Line with an attached Porch permitted to Encroach. A fence at the Frontage Line maintains Street spatial definition. Porches shall be no less than 8 feet deep to function as an outdoor living space and are elevated above the finish level of the sidewalk to ensure privacy within the building.

C. Terrace or Lightwell

A Frontage wherein the Façade is set back from the Frontage Line by an elevated terrace or a sunken Lightwell. This type buffers Residential use from urban sidewalks and removes the private yard from public Encroachment. Terraces are suitable for conversion to outdoor cafes.

D. Forecourt

A Frontage wherein a portion of the Façade is close to the Frontage Line and the central portion is set back. The Forecourt created is suitable for garden space, apartment or office entry, outdoor dining, or vehicular drop-off areas. This type should be allocated in conjunction with other Frontage types. Large trees within the Forecourts may overhang the sidewalks.

E. Stoop

A Frontage wherein the Façade is aligned close to the Frontage Line with the first Story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. Stairs from the stoop may descend forward or to the side. Entry doors are encouraged to be covered or recessed to provide shelter from the elements. This type is recommended for ground-floor Residential use.

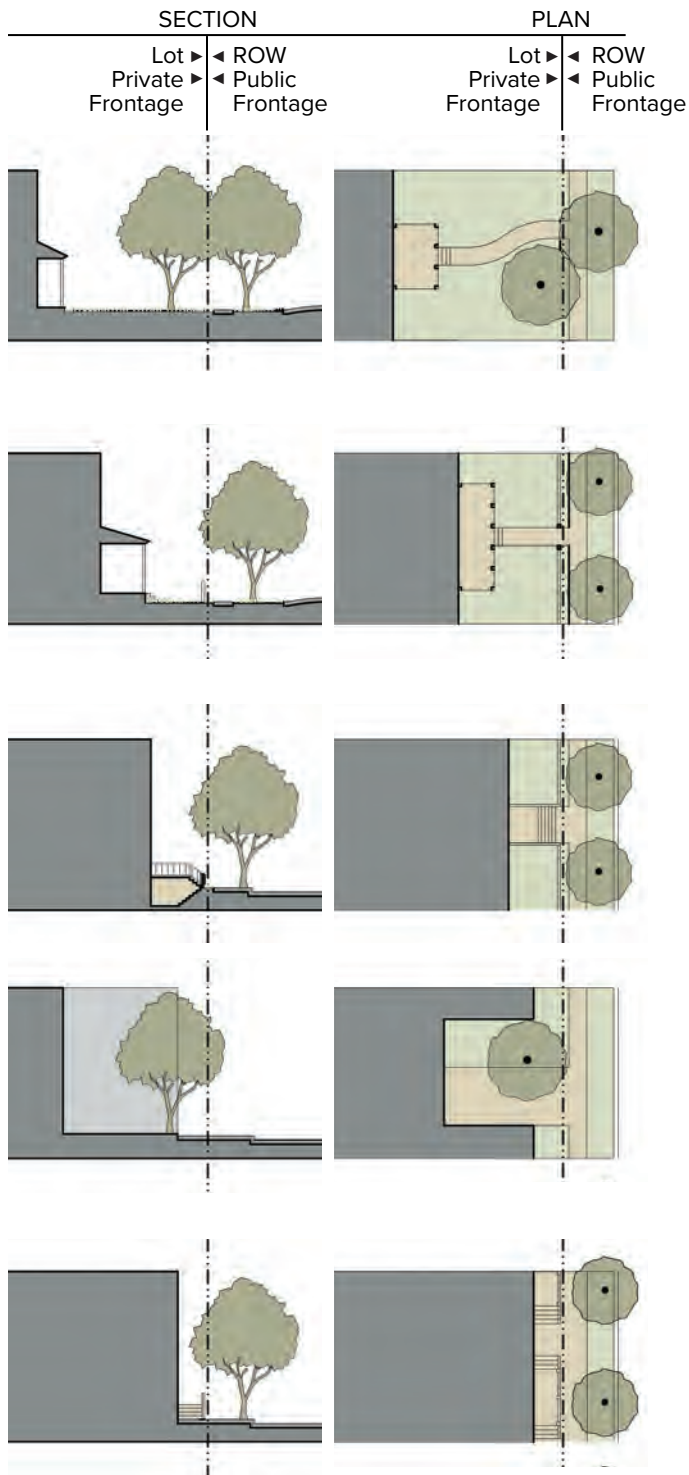


Figure 5-8: Graphic illustrating plan and section view of Frontage types Common Yard, Porch & Fence, Terrace or Lightwell, Forecourt, and Stoop.

F. Storefront

A Frontage wherein the Façade is aligned close to the Frontage Line with the building entrance at sidewalk grade. This Frontage type is conventional for retail use. It has substantial glazing on the sidewalk level and an Awning. See Section 5.7 Storefronts and Section 5.8.B.2 Awnings/Canopies for additional requirements.

G. Gallery/Colonnade

A Frontage wherein the Façade is aligned close to the Property Line with an attached cantilevered shed or a lightweight colonnade overlapping the sidewalk. This type is conventional for retail use and may be combined with a storefront. The Gallery/Colonnade shall be no less than 9 feet wide and should overlap the sidewalk to within 2 feet of the Curb.

H. Arcade

A colonnade supporting Habitable Space that overlaps the sidewalk, while the Façade at sidewalk level remains at or within the Build-to Zone. This type is conventional for retail use and may be combined with a storefront. The Arcade shall be no less than 12 feet wide and should overlap the sidewalk to within 2 feet of the Curb.

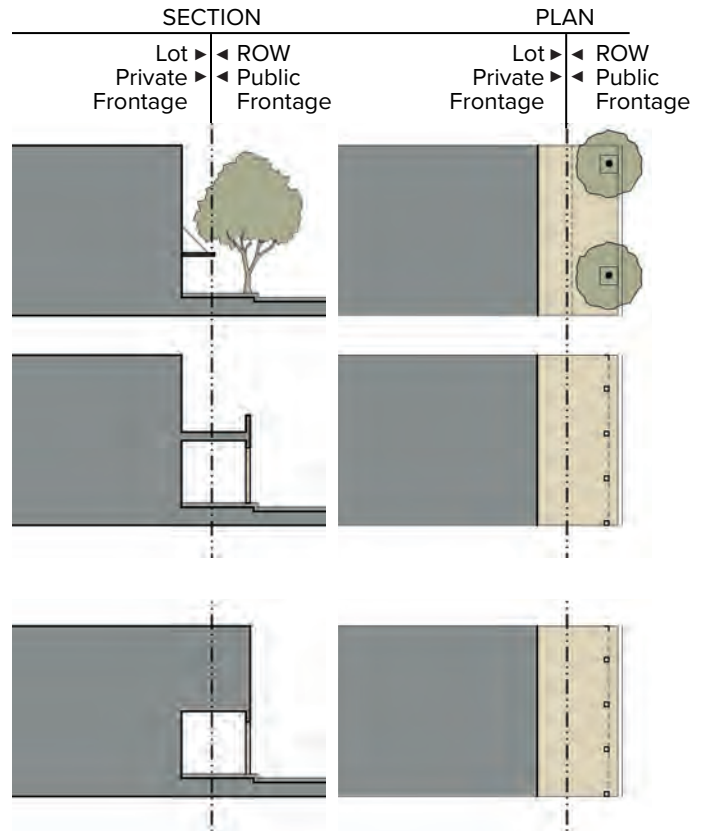


Figure 5-9: Graphic illustrating plan and section view of Frontage types Storefront, Gallery/Colonnade, and Arcade.

SECTION 5.10 SPECIAL BUILDING TYPES

Some automotive-oriented uses and large footprint buildings may be provided to serve the daily needs of residents. The following criteria shall be used to ensure these uses and buildings do not detract from the overall walkability of the neighborhoods.

A. Gas Stations

A ground-floor Storefront shall face the Primary Street and define the Frontage of the Lot. All pumps, parking, and drive-through areas must be located behind the Storefront building toward a mid-block location.

B. Drive-through

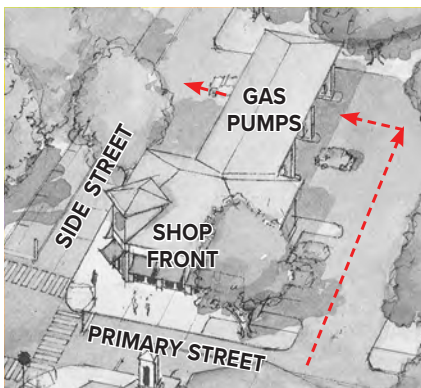


Figure 5-10: Gas Station special building type site design

A ground-floor Storefront shall face the Primary Street. All parking shall be located in the rear and accessed from an Alley when present. Drive-through windows shall be located to the side or rear of the building and accessed from mid-block or Alleys, where existing. Where no mid-block or Alley access exists, access from a Secondary Street Frontage is permitted but shall not substantially disrupt pedestrian activity or surrounding uses.

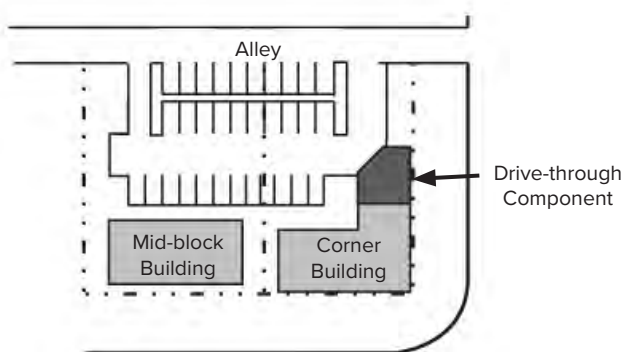


Figure 5-11: Drive-through special building type site design

C. Large Footprint Buildings

Large Footprint Buildings are those with footprints greater than 30,000 sq. ft. Examples may include a grocery store, large-format retailer, or theater. Lots may exceed the dimensions set in the Transect Zone Standards, provided such buildings comply with the following provisions:

1. Habitable Frontages (as part of the building or a separate Liner Building) that provide doors and windows facing the Public Realm shall be provided along Façades that face Civic Uses or Streets.
2. To encourage use by pedestrians and decrease the need for solely auto-oriented patronage, Large Footprint Buildings must reinforce the urban character of the neighborhood and shall therefore continue a connected system of walkable Blocks/ Street Frontages (including sidewalks and Street trees) through the site as part of the design of parking and drive aisles.
3. Building footprints may not be larger than a single Block.
4. Loading docks, service areas, and trash disposal facilities shall not face Streets or Civic Uses.

D. Liner Buildings

The character of some uses of land, such as warehouses and parking structures, may preclude their buildings from complying with the Façade Transparency requirements. Such buildings may be constructed in a manner that they will be separated from adjacent Streets (but not Alleys) by Liner Buildings:

1. Liner Buildings must be at least one-story in height (2 stories min. when lining structured parking) with no less than fifteen (15) feet in depth.
2. Liner Buildings may be detached from or attached to the Principal Building.
3. Liner Buildings may be used for any purpose allowed on the Lot on which they are located except for parking.
4. Liner Buildings must meet the Front Façade Transparency requirements in Section 5.6.C.

SECTION 5.11 GREEN BUILDING

Many of the standards and guidelines throughout this document already promote inherently sustainable buildings and neighborhood design. The following regulations are intended to enhance the sustainability and resilience of buildings within the Greeley Westside Subarea.

A. General Construction Techniques

1. All private and shared parking spaces for residential units should be designed and built with rough-in and conduit for electric vehicle charging ports. At least 10% of commonly shared parking spaces in surface lots and garages should also be built with rough-in and conduit. Conduit should be sized appropriately to accommodate wiring for a Level 1 Charge Port (120-volt, 20 amp.) with the ability to be upgraded to at least a Level 2 Charge Port (240 Volt, 40 Amp.) Level 3 Charge Ports are recommended for public or shared EV charging stations.
2. Wherever possible, green building materials shall be used in the construction of building walls, including siding composed of reclaimed or recycled material, salvaged masonry brick or block, and locally produced stone or brick.

B. Sustainable Building Incentives

To encourage the use of sustainable buildings, the Approval Authority shall grant the following incentives, to the extent authorized by state law. Warrants may be approved by the Approval Authority Administrator for the inclusion of the following types of building or site features:

1. Energy Efficient Development: New development or redevelopment of a primary building that is registered, designed, and documented for a LEED Platinum or LEED Gold certification or equivalent shall receive the following benefits, regardless of whether the final structure receives a LEED Platinum or LEED Gold certification or equivalent. The Approval Authority Administrator shall determine whether a proposed alternative energy efficiency system or facility is equivalent.
 - a. The project may increase the maximum impervious Lot Coverage by 20 percent.
2. Buildings that include photovoltaic panels sufficient to power the entire building may select from one of the following options:
 - a. Buildings within the project may increase the maximum Lot Coverage by 20 percent; or,
 - b. Buildings within the project may increase the maximum height by one-story.

3. Single-Family homes that include Level 2 EVSE charging stations for electric vehicles.
 - a. Homes within project may reduce Setbacks by 20 percent.
4. Multifamily buildings that include Level 2 EVSE charging stations for electric vehicles for at least half the units.
 - a. Buildings within project may reduce Setbacks by 20 percent.
5. Low-Impact Development: New development or redevelopment of a site that incorporates a blue (water retaining) roof, or a green (vegetated) roof, or other building or site features that are designed so that off-site flow of the first one inch of rainfall during the first 24 hours after rainfall ends is reduced by at least 50 percent shall receive the following benefits:
 - a. The project may reduce any required building Setback by 20 percent (provided the required reduction in off-site water flow is still achieved).

C. Active Solar Energy Systems

A system composed of panels, arrays, or devices that convert the sun's radiant energy into thermal, chemical, mechanical, or electric energy, which may include an energy storage facility, and components for the transmission and distribution of transformed energy. Accessory Solar Energy Systems are those designed primarily for servicing the on-site needs of a Principal Use and/or ground-mounted systems occupying up to 1,750 square feet of surface area.

1. Building-Mounted System

An Active Solar Energy System mounted on or integrated into the construction of a structure, such as, but not limited to, a roof-mounted solar energy system.

- a. Permitted by-right in all Transect Zones.
- b. Rooftop systems shall be mounted as flush as possible to the roof. In order to achieve proper solar orientation, panels may exceed the roof line by up to 6 feet. Roof-mounted solar hot water heaters may exceed the roof line by up to 10 feet.
- c. Buildings shall be physically and structurally designed to support rooftop solar energy systems.

- d. Buildings are encouraged to be electrically wired and plumbed to support the later installation of Solar Energy System(s).

2. Ground-Mounted System

An Active Solar Energy System mounted on a rack or poles that rests on or is attached to the ground, not including a solar energy system mounted on parking canopies.

- a. Ground-mounted accessory solar energy systems are permitted by-right in all Transect Zones.
- b. Solar Energy Systems that occupy more than 1,750 square feet of surface area are permitted by site plan review.
- c. Systems should be located in side and rear yards. Placement of solar energy systems in the front yard should be avoided where possible.
- d. Systems shall be screened from view from any Street.
- e. Ground-mounted systems are considered Structures and must meet applicable Setbacks for the Transect Zone with the following exceptions:
 - i. If necessary for the system's effectiveness, ground-mounted solar energy systems may encroach within the minimum Lot Line Setbacks for the subject property's Transect Zone, provided that the solar energy system is located no less than 15 feet from the Frontage Line and no less than 5 feet from other Lot Lines.
- f. Ground-mounted systems shall not exceed 15 feet in height.
- g. If mounted over a pervious surface, ground-mounted solar systems do not count towards Lot Coverage.

3. Parking Canopy System

An Active Solar Energy System mounted on or integrated into the construction of a vehicle parking shade structure that covers vehicle or other multimodal parking areas.

- a. Must not exceed 30 feet in height.
- b. Permitted by site plan review in all Transect Zones.
- c. Must not obstruct or encroach into a fire lane.

- d. Unobstructed separation of no less than 16 feet, between Canopy structures, must be maintained over dedicated parking aisles.
- e. The vehicle shade structure must meet building code requirements.

4. Solar Farm

An Active Solar Energy System that is the primary use on the Lot, designed for providing energy to off-site uses or export to the wholesale market.

- a. Conditional Use in Transect Zones T2, T3, T4, T5, and SD-W.

D. Geothermal

1. Geothermal shall be permitted on all sites.

SECTION 5.12 SITE STANDARDS

A. Service Areas & Loading Docks

1. Trash and recycling dumpsters or similar collection areas shall be located in the rear or to the side of buildings and screened from view from adjacent public Right-of-Ways, properties, and pedestrian walkways (not including Alleys).
2. Enclosure and/or screened areas must have adequate storage space for landfill, compostable, and recyclable materials collection dumpsters and/or compactors.

B. Mechanical Equipment

For the purposes of these standards, mechanical equipment includes any heating, ventilation, and air conditioning (HVAC) or electrical machinery, as well as air compressors, hoods, mechanical pumps, exterior water heaters, water softeners, utility and telephone company transformers, meters or boxes, garbage cans, storage tanks, generators, electric vehicle (EV), geothermal wells, and similar elements. This section does not regulate Active Solar Energy Systems, which shall follow the regulations in Section 5.11.C.

1. If mechanical equipment is located at-grade and is visible from an adjacent Street or sidewalk, it shall be screened by a fence or Streetscreen.
2. All mechanical equipment or penthouse screening placed on a roof shall be set back from the roof line by a distance at least equivalent to the height of the screening in order to minimize visibility from surrounding Streets.

SECTION 5.13 ACCESSIBILITY

- A.** If a Building is to be constructed with an elevated first finished floor for a portion or all of the ground Story, the main entrance may be flush (0-5") with the adjacent sidewalk/fronting walkway elevation. The transition to a required finished floor elevation shall then be made within the building interior, using either a ramp (as shown in Figure 5-16), an elevator (as shown in Figure 5-17), or by a similar design, as determined by the Approval Agency.
- B.** Methods by which accessibility to residential units can be achieved while maintaining the elevated finished floor prescribed in this code (which is vital to privacy in urbanized areas) include the following:
1. On Alley-served Lots: The ground may be sloped or raised to provide a zero-step entrance at the rear, convenient to parking. This may be accomplished by grading the Alley higher than the Street, by sloping individual Lots toward the Alley. (See Figure 5-12)
 2. Well-integrated ramps, set completely behind the building Façade or front stoop, may lead to a side or rear Porch. Side or rear ramps are encouraged to be shared between multiple units. The entrance to the ramp at the sidewalk could be through a doorway or integrated into the building design as an archway. (See Figure 5-14)
 3. In cases where accessibility cannot be met by rear grading or access ramps, one alternative is to provide an at-grade entrance at the front of the building with an exterior stair leading to the entrance of the unit above. (See Figure 5-13)
 4. Where ramping begins at the front sidewalk and leads up to the front door, the ramp shall only run perpendicular to the building Façade and may continue within a recessed entryway. (See Figure 5-15)
 5. An additional alternative is to provide an at-grade entrance at the front of the building with an interior ramp (see Figure 5-16) and/or elevator (see Figure 5-17) up to the first finished floor level.

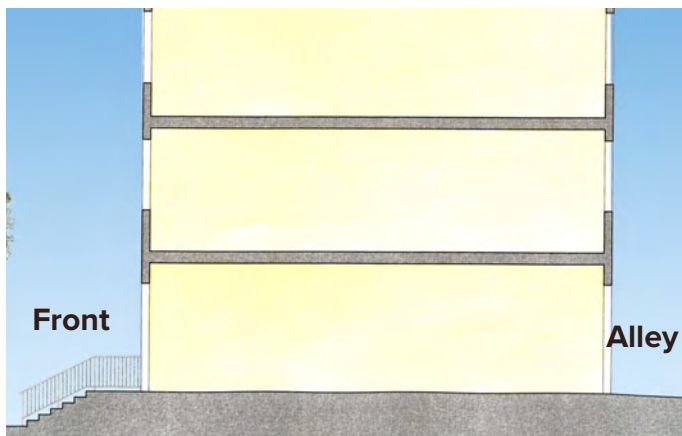


Figure 5-12: On Alley-served Lots, ground may be sloped or raised to provide a zero-step entrance at the rear.



Figure 5-13: An at-grade entrance and an exterior stair leading to the entrance of the unit above.

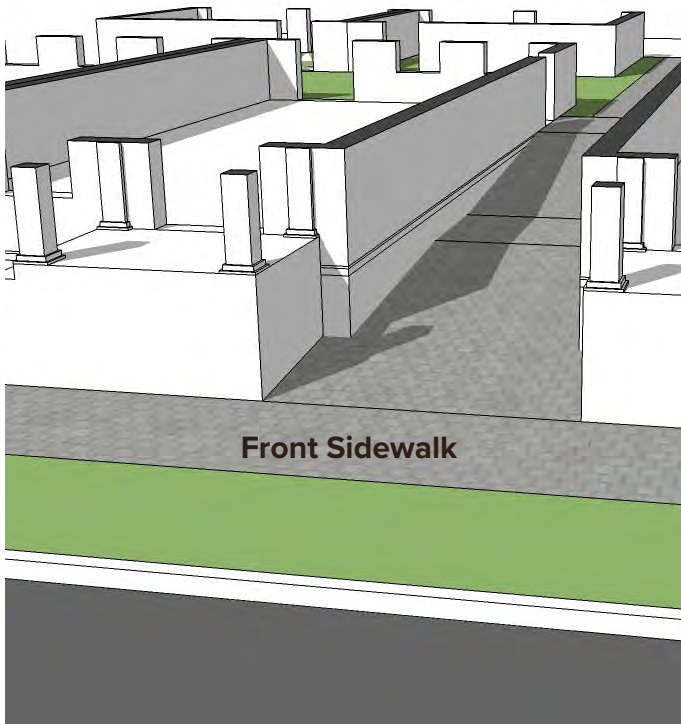


Figure 5-14: A subtle and well-integrated ramp set behind the building's Front Façade plane.

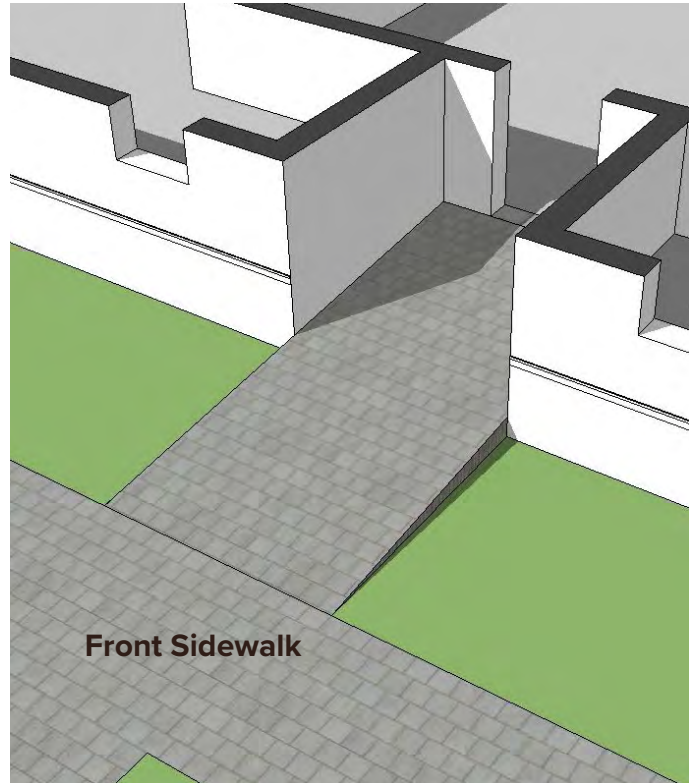


Figure 5-15: Ramp from the front sidewalk to the front door.

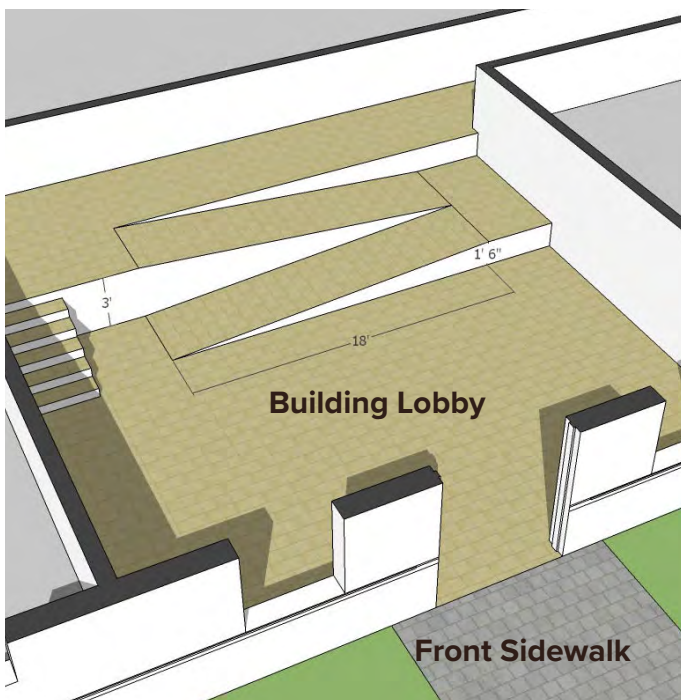


Figure 5-16: Elevated first finished floor accessed by interior ramp.

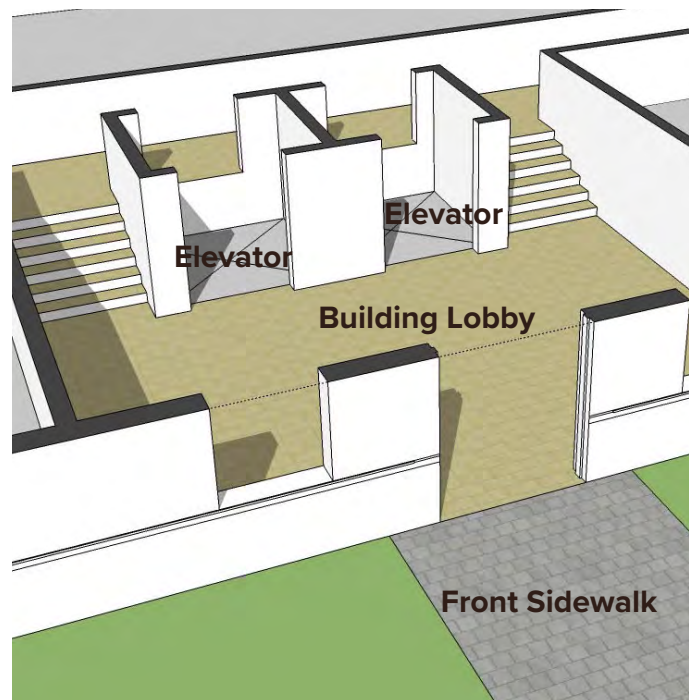


Figure 5-17: Elevated first finished floor accessed by elevator.

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DIVISION 6: THOROUGHFARE STANDARDS

The Thoroughfare Standards provide guidance for creating an accessible, interconnected network of Streets that accommodates all ages, abilities, and modes of transportation, including walking, cycling, driving, and public transit.



DIVISION 6

THOROUGHFARE STANDARDS

SECTION 6.1 PURPOSE

The purpose of the Thoroughfare Standards is to share guidance to create an interconnected network of Streets that can accommodate all modes of travel, including vehicular, pedestrian, and bicycle. The function of Streets within the Grow West Subarea is to handle multimodal forms of traffic by offering a memorable experience for persons using the Street network. The Streets will work together with the trail network to provide multiple options for moving throughout the neighborhoods that make up the Grow West Subarea. The function of each Street will guide the Street design in concert with its context. The context is determined by the character of the adjacent Transect Zone(s).

The location, Street type, and hierarchy of proposed new or improved corridors are identified in the Street Atlas, Section 6.6. Typical Street sections illustrating the key characteristics of each proposed new or improved Street are provided in Section 6.7. The conditions illustrated in the typical sections may be interrupted for intersections, bump-outs, central greens, or other traffic calming devices and special design features, depending on the context details of the final neighborhood designs.

SECTION 6.2 STREET HIERARCHY

On each parcel that has multiple Street Frontages (e.g., corner Lots), the Street hierarchy will determine the highest priority (Primary) Street Frontage, where the Front Build-to-Zone or Setback shall apply. Along the lower priority Frontages, the Side (Secondary) Build-to-Zones or Setbacks shall apply. If both Street Frontages have the same priority, then either Street may be designated as to which Frontage is Primary and which is Secondary on the Master Site Plan. The designated Street hierarchy is as follows:

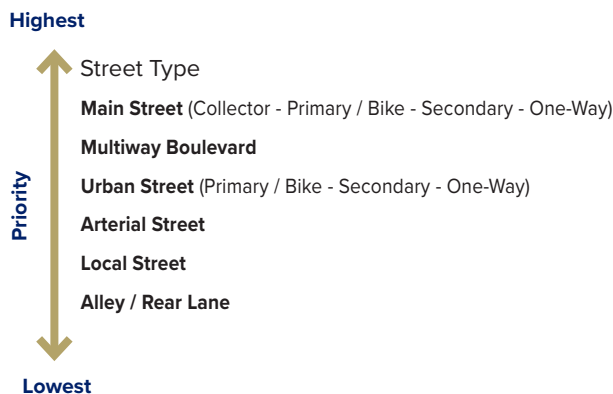


Figure 6-1: Street hierarchy

SECTION 6.3 GENERAL STANDARDS

The precise location and alignment of new Streets may be adjusted to allow flexibility in the design of the site plan; however, the intended purpose and network connectivity shall not be compromised.

A. Street Design

The design of new Streets and modifications to existing Streets shall adhere to the following requirements:

1. Some dimensional flexibility is permitted for Street types to account for varying Right-of-Way widths or create unique public spaces, however they shall be designed to have all the basic functional characteristics, including roadway width, on-Street parking/curbside flex zones, sidewalks, trails, Street trees, and landscaped areas shown for their type, and shall be appropriately sized for the Transect Zone(s) in which they are located.
2. Streets designated for transit may have 11-foot travel lanes.
3. Two-way, two-lane streets should not have a striped centerline, except for 25 feet from intersections.
4. Pre-approved new or improved Street Sections are provided in Section 6.7. Additional Street types and sections may be created and approved by the Approval Authority.
5. Each Neighborhood shall demonstrate appropriate provisions for Street connectivity and integration with adjoining neighborhoods. Connections to future development is required. When a site plan consisting of one or more neighborhoods is submitted for approval, the Street network contained in those neighborhoods should connect to stub-outs of adjacent neighborhoods or other rights-of-way that form the edge of the neighborhood(s).
6. All Streets shall connect to other Streets, except that dead-end Streets with cul-de-sacs or T-turnarounds may be permitted only when necessitated by environmental constraints, provided the Street terminus provides adequate maneuverability for public services.

B. Curbside Use, Parking, and Flex Zones

Curbside space may function as traditional on-street parking or as a Curbside Flex Zone. Flex Zones are permitted in T4, T5, C, and SD-W Transect Zones and may vary along the block or by time of day or year. Permitted uses include parking, transit stops, rideshare pick-up/drop-off, deliveries, vendors, parklets, outdoor dining, and shared-mobility stations.

1. Curbside lanes (parking or flex) shall not be located within 25 feet of intersections, measured from the curb line.
2. Where on-street parking is provided, a parking-protected sidewalk is preferred to enhance pedestrian comfort and safety.
3. Standard parking lane width: 8 feet with striped outer edge.
4. Standard stall length: 22 feet (or 20 feet at bulbouts).

C. Bulbouts

Bulbouts shall be installed at intersections and significant driveway entrances to shorten crossing distances and slow turning vehicles.

1. Extend 6–7 feet from the curb line.
2. Use a 45-degree taper.
3. Taper begins 10 feet from the crosswalk at intersections and 5 feet from the drive at major driveways.

D. Curb Cuts (Driveways)

1. To maintain safe, continuous sidewalks, curb cuts shall be minimized.
 - a. Limit curb cuts to demonstrated access needs.
 - b. No private property may have more than one curb cut.
 - c. Properties with alley access shall not have curb cuts.
 - d. Driveway widths shall be minimized to prioritize pedestrian continuity.

2. Curb Cut Dimensions

- a. Major curb cuts (serving >20 parking spaces): 20 feet wide plus curb radii.
- b. Minor curb cuts: 12 feet wide with 3-foot radii; bulbouts are not installed at minor curb cuts.
- c. A 3-foot parking setback from minor curb cuts creates an 18-foot no-parking zone.

E. Curb Radii

1. Curb radii shall support pedestrian safety, reduce crossing distances, and slow turning vehicles while accommodating emergency access.
 - a. Standard corner radius: 15 feet.
 - b. Major driveway radius: 5 feet.
 - c. Larger radii may be permitted only when required for emergency vehicle turning movements.
2. Where on-street parking is present, the effective turning radius is larger than the curb radius; increased curb radii should not be used to compensate for this natural effect.

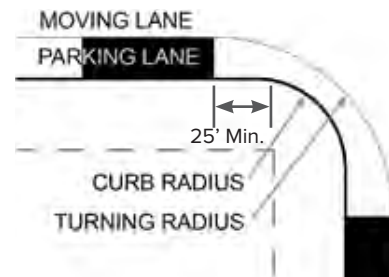


Figure 6-2: Curb radius and effective turn radius where on-Street parking is present

F. Intersection Design

Intersection design shall prioritize pedestrian and bicyclist safety while accommodating larger vehicles when necessary.

1. Intersecting streets should meet at approximately 90 degrees; angles less than 60 degrees should be avoided.
2. Auxiliary turn lanes may be considered only when benefits outweigh impacts to pedestrian and cyclist safety; decisions shall not be based on traffic analysis alone.
3. On Main Streets, collector streets and arterial streets, on-street parking may be removed near intersections to accommodate left-turn lanes when needed.

4. Crossing & Signal Requirements

- a. Provide pedestrian and bicycle crossing infrastructure on all approaches: high-visibility crosswalks, ADA-compliant ramps, and detectable warnings.
- b. Traffic signals shall prioritize pedestrian and bicyclist convenience and safety, using tools such as Leading Pedestrian Intervals (LPIs) and automatic pedestrian detection.
- c. Walk phases should be automatic where feasible.
- d. Minimize pedestrian exposure through refuge islands, bump outs, or pedestrian-only signal phases.

5. Bicycle/Micro-Mobility Requirements

- a. Protected intersections are required where both approaches include protected bicycle or micro-mobility lanes, unless insufficient space prevents setting back the bikeway; in such cases, a dedicated intersection treatment is required.
- b. Along streets with protected bicycle/micro-mobility lanes, minor street crossings shall include dedicated intersection treatments or raised crossings.

G. Rear Alleys and Lanes

A network of rear Alleys and/or Lanes is desirable to serve as the primary means of vehicular ingress to individual Lots. Such networks are mandatory in the T5, T4-O and T4-R Transect Zones, and encouraged in the T3 Transect Zone.

1. Rear Alley and Lane entrances should align so as to provide ease of ingress for service vehicles.
2. Rear Alley and Lane entrances shall not face Civic Open Spaces or Civic Buildings.
3. Internal deflections or variations in the rear Alley/Lane network are encouraged to prevent excessive or monotonous views of the rear of structures resulting from long stretches of rear Alleys/Lanes.
4. Rear Alleys/Lanes should meet Streets with a mountable gutter pan, allowing the sidewalk to continue uninterrupted across the rear Alley/Lane pavement. The use of curb cuts, ramps, and marked crosswalks should be avoided for rear Alleys/Lanes.

H. Lane Markings

1. Two-way, two-lane streets should not have a striped centerline, except for 25 feet from intersections.

I. Relationship to Other Standards

1. Where a conflict occurs between these Thoroughfare standards and Greeley Street Engineering Standards or Fire Code Standards, these Thoroughfare Standards shall control unless the conflict is a matter of life safety.

SECTION 6.4 EMPLOYMENT CORRIDOR (EC) OVERLAY: ACCESS AND FRONTAGE MODIFIERS

A. Applicability

1. The provisions of this section apply only to properties located within the Employment Corridor (EC) Overlay as established in Division 2.
2. These standards apply in addition to the Thoroughfare Standards of Division 6.
3. Where a conflict occurs between the provisions of this section and other standards of Division 6, the provisions of this section shall control.

B. Corridor Street Priority

1. Streets identified as Arterial Streets, Main Streets, Urban Primary Streets, or other designated corridor streets within the EC Overlay shall be treated as Primary Street Frontages for purposes of building orientation and access.
2. Development within the EC Overlay shall prioritize building frontage, pedestrian access, and streetscape quality along corridor streets over vehicular access.

C. Access Management

1. Minimization of Curb Cuts

- a. Curb cuts along corridor streets within the EC Overlay shall be limited to the minimum necessary to provide safe and functional access.
- b. No development parcel shall be permitted more than one (1) curb cut per block face along a corridor street, unless otherwise approved by the Approval Authority upon demonstration of necessity.

2. Shared Access

- a. Shared driveways, cross-access easements, or internal circulation systems shall be required where feasible between adjacent parcels.
- b. Developments shall demonstrate that reasonable efforts have been made to coordinate access with adjoining properties.

3. Alley and Lane Access

- a. Where rear alleys or lanes are provided or required by the applicable Transect Zone, they shall serve as the primary means of vehicular access for parking, loading, and service functions.
- b. Properties with functional alley or rear-lane access shall not be permitted curb cuts along corridor streets.

D. Parking and Service Area Orientation

1. Surface parking lots, loading docks, drive aisles, overhead doors, and service entries shall not be located between a principal building and a corridor street.

2. Vehicular access to parking and service areas shall be provided from:
 - a. Rear alleys or lanes;
 - b. Side streets; or
 - c. Internal private streets, where feasible.
3. Where parking or service areas are visible from a corridor street due to site constraints, streetscreens, landscape buffers, or building elements shall be provided consistent with Division 3 and Division 5 standards.

E. Internal Circulation and Connectivity

1. Developments within the EC Overlay shall provide internal vehicular and pedestrian circulation systems that:
 - a. Connect buildings, parking areas, and adjacent parcels; and
 - b. Reduce the need for trips re-entering corridor streets for internal site movements.
2. Internal private streets or drives serving multiple buildings shall be designed to function as complete streets, with sidewalks or shared-use paths provided where appropriate.

F. Intersection and Driveway Design

1. Driveways intersecting corridor streets shall align with existing or planned intersections where feasible.
2. New intersections or major driveways shall minimize pedestrian crossing distances and shall incorporate bulbouts, marked crosswalks, or other pedestrian safety measures consistent with Division 6 standards.
3. Driveway widths shall comply with the minimum necessary dimensions established in Section 6.3 and shall not be expanded for convenience or stacking capacity alone.

G. Street Atlas and Regulating Plan Coordination

1. Street Atlas submissions within the EC Overlay shall clearly identify:
 - a. Corridor streets subject to this section;
 - b. Proposed curb cuts and access points; and
 - c. Internal circulation networks.
2. Regulating Plans within the EC Overlay shall demonstrate consistency with this section as part of the required Employment Mix Summary and overall site organization.

SECTION 6.5 SIDEWALKS

A. General

1. Sidewalks are essential to walkability, providing safe, comfortable, and accessible routes for pedestrians in all seasons. Sidewalk design in Greeley shall reflect the community's commitment to water-wise landscaping, multimodal access, and year-round safety.
2. Sidewalk standards vary based on whether adjacent land uses are primarily Commercial or Residential. "Commercial" includes non-residential uses such as retail, office, civic, mixed-use, and industrial frontages with higher pedestrian activity.
3. Standards below are divided into:
 - a. Universal Requirements – apply to all sidewalks.
 - b. Use-Based Requirements – additional standards for Commercial vs. Residential sidewalks.

B. Universal Requirements

1. Design

Sidewalks shall include two functional zones:

- a. Walking Zone: A clear, unobstructed pedestrian path 4–12 feet wide, constructed of concrete with control joints suitable for freeze–thaw conditions.
- b. Tree/Furnishing Zone: A 3–12-foot-wide zone along the curb edge for trees, utilities, snow storage, and pedestrian amenities. Ground treatments shall follow Greeley's water-wise landscaping requirements.

2. Bollards

Where sidewalk width cannot accommodate trees, continuous bollards may be used for pedestrian protection. Bollards shall be metal, slim profile, and spaced approximately 5 feet on center.

3. Crosswalks

Crosswalks shall:

- a. Be provided on all legs of all intersections.
- b. Use high-visibility (zebra) reflective white markings.
- c. Be 10 feet wide.
- d. Align with pedestrian desire lines.

4. Wheelchair Ramps

Ramps shall:

- a. Comply with ADA/PROWAG.
- b. Be at least 5 feet wide, with side flares.
- c. Direct users into the crosswalk.

5. Sidewalk Objects

- a. Hydrants, utilities, mailboxes, trash cans, micromobility racks, and other furnishings shall be located within the Tree/Furnishing Zone.
- b. On Commercial sidewalks, outdoor seating, sandwich boards, and temporary merchandise displays are permitted in the Furnishing Zone so long as the Walking Zone remains clear.

6. Benches

Where the Tree/Furnishing Zone is 6 feet or wider, benches shall be provided near active commercial frontages at a rate of two per block face, arranged facing each other across a 6-foot clear zone.

7. Furnishing Zone (Commercial Areas)

Where the Walking Zone exceeds 6 feet, the zone adjacent to the building may be used for:

- a. Outdoor dining
- b. A-frame signs
- c. Temporary displays (removed overnight)

8. Bike Racks

Provide one bike rack per 100 feet of sidewalk frontage.

9. Trash Cans

Commercial block faces shall include one trash can within 30 feet of each intersection corner, or as required by city waste guidelines.

C. Use-Based Requirements

1. Commercial Sidewalks

Commercial sidewalks shall:

- a. Use permeable pavers or other water-permeable ground surfaces in the Tree/Furnishing Zone.
- b. Include tree wells integrated into structural soil or suspended pavement systems.
- c. Use removable pavers around tree trunks for expansion.

2. Residential Sidewalks

Residential sidewalks shall:

- a. Use water-wise groundcovers from Greeley’s approved xeric landscape lists.
- b. Plant trees in structural soil or suspended pavement systems, consistent with the universal requirement.
- c. Use permeable or low-water surface materials in Tree Zones.

D. Sidewalk Design Matrix

1. A sidewalk design matrix may be used to illustrate the arrangement of the Walking Zone and Tree/Furnishing Zone for both Commercial and Residential sidewalks, consistent with these standards.

TABLE 6-1: SIDEWALK DESIGN MATRIX		
Available Width	Commercial	Residential
< 7'	Concrete Walk: Full Tree Zone: N/A Structural Soil: N/A Bollards at 5' off center behind curb	=
7' - 9'	Concrete Walk: 4' - 6' Tree Zone: 3' (with pavers) Structural Soil: Full	=
9' - 12'	Concrete Walk: 6' Tree Zone: 3' - 6' (with pavers) Structural Soil: 6' minimum	Concrete Walk: 6' Tree Zone: 3' - 6' (with ground cover) Structural Soil: 6' minimum
12' - 18'	Concrete Walk: 6' minimum Tree Zone: 6' (with pavers) Structural Soil: 6' minimum	Concrete Walk: 6' Tree Zone: 6' minimum (with ground cover) Structural Soil: None
Ideal Width	Commercial	Residential
18'	Concrete Walk: 12' Tree Zone: 6' (with pavers) Structural Soil: 12'	Concrete Walk: 6' Tree Zone: 12' Structural Soil: None

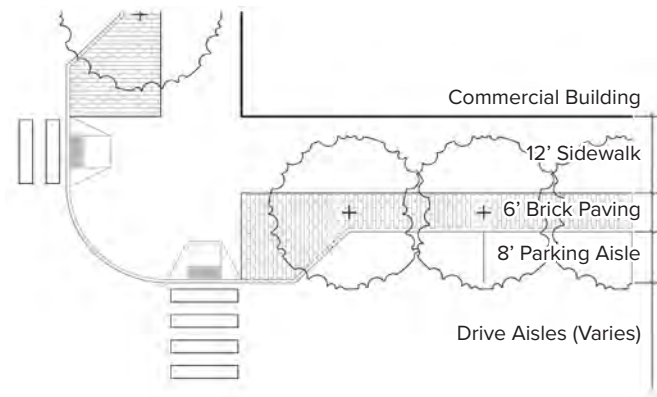


Figure 6-3: Ideal Commercial Sidewalk

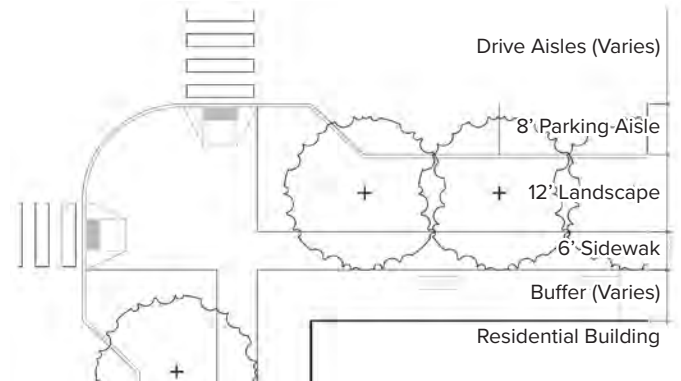


Figure 6-4: Ideal Residential Sidewalk

SECTION 6.6 STREET TREES

Street trees and landscaping in the public right-of-way shall support pedestrian comfort, long-term tree health, and Greeley's water-conservation goals. All street trees shall comply with applicable landscape requirements and the City of Greeley Forestry Program.

A. Placement and Spacing

1. Street trees shall be located within planting strips or tree wells (with or without grates), as required by the applicable Street Type.
2. Trees shall be spaced no farther apart than the mature crown width or 30 feet, whichever is less.
3. Tree placement shall begin approximately 15 feet from crosswalks and continue at regular intervals.

B. Species and Quality

1. All street trees shall be drought-tolerant, cold-hardy species approved by the City of Greeley Forestry Program.
2. Required shade trees shall have a minimum 3-inch caliper (or 4-inch if preferred for higher establishment success) and a minimum height of 10 feet at planting.
3. Large canopy species are required where overhead utilities do not restrict height.
4. Accent trees and plantings may be used in addition to required street trees.

C. Consistency and Diversity

1. To create an orderly streetscape, each block face shall use a single tree species, unless utilities or site conditions require variation.
2. Species diversity shall be achieved across blocks and neighborhoods, not within individual blocks, to avoid monocultures.

D. Soils, Root Infrastructure, and Planting Standards

1. Street trees shall be planted in structural soil or suspended pavement systems (e.g., Silva Cells) to ensure adequate rooting volume, support pavement loads, and promote long-term health.
2. Planting areas less than 7 feet wide and 15 feet long shall use suspended pavement systems.
3. Minimum soil depth shall be 3 feet, with 4 feet preferred.
4. Root barriers or other root-management systems are required.
5. Tree planting must protect surrounding infrastructure, utilities, and pavement.

E. Water-Wise Requirements

1. All non-tree plantings shall use xeric or low-water species from Greeley's approved lists.
2. Turf grass is prohibited within the Tree/Furnishing Zone.
3. Surface treatments may include permeable pavers, gravel mulch, or other approved dry-climate materials.
4. Properly designed tree box filters that receive stormwater runoff are encouraged and may count toward tree-planting requirements when adequate maintenance access is provided and the planted tree meets all standards.

SECTION 6.7 STREET LIGHTING

Street lighting shall enhance pedestrian safety, support multimodal travel, and maintain Greeley’s commitment to dark-sky sensitivity. All fixtures and installations shall comply with applicable City of Greeley lighting standards and specifications.

A. Location and Height

1. Streetlights shall be placed approximately 18 inches to 4 feet from the back-of-curb, coordinated with street trees, furnishings, utilities, crosswalks, and driveways.
2. Pedestrian-scaled fixtures are required on all streets, with a typical fixture height of 10–16 feet.
3. Intersection-scaled lighting may be added where needed for safety but shall complement pedestrian-scaled fixtures.

B. Illumination and Fixture Standards

1. Lighting shall use warm-tone, dark-sky-friendly illumination with full cut-off fixtures to minimize glare and light spill.
2. Cobra-head fixtures are prohibited.
3. Light poles may incorporate hardware for banners, hanging baskets, or similar streetscape amenities.

C. Spacing and Distribution

1. Fixture spacing shall provide consistent illumination while supporting walkability and place-making. Typical spacing:
 - a. Commercial retail frontages: 20–30 feet on center
 - b. Office or mixed-use areas: 40–50 feet on center
 - c. Residential areas: 60–70 feet on center
2. In areas of higher pedestrian activity or denser street types, spacing may be reduced to ensure adequate lighting levels, generally not exceeding 60 feet on center.

D. Coordination and Uniformity

1. Streetlight placement shall be coordinated with street tree locations to avoid conflicts with canopies and root zones.
2. Fixture types and configurations shall maintain logical and visual uniformity along each block or street segment.
3. Any fixture or pole intended for City maintenance must be approved by the City of Greeley.

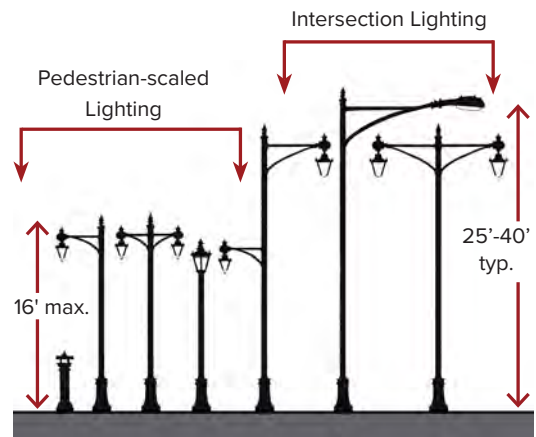


Figure 6-5: Pedestrian and Intersection Lighting

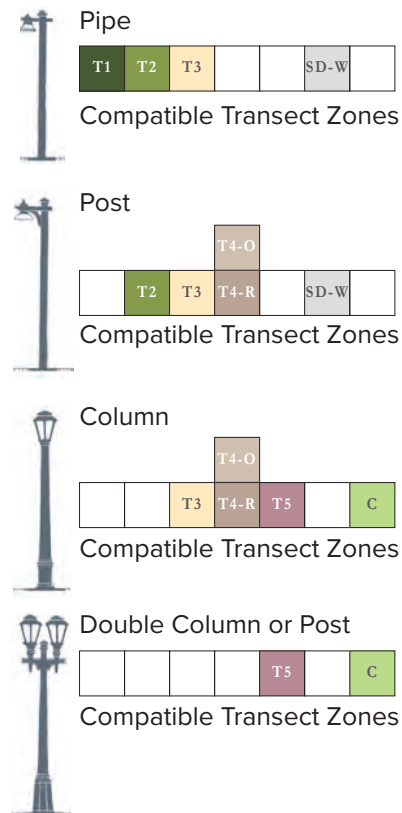


Figure 6-6: Diagrammatic Fixture Configurations

DIVISION 6

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SECTION 6.8 STREET ATLAS

- A.** The Street Atlas provides guidance to create an interconnected network of Streets, shared-use paths, and trails that can accommodate all modes of travel, including cars, transit, freight, deliveries, pedestrians, and bicyclists. The function of each Street will guide the design in concert with its context, which is determined by the character of the adjacent Transect Zones.
- B.** A Street Atlas must be submitted for Site Plan Approval.

SECTION 6.9 STREET SECTIONS

- A. New Street Sections**
The pre-approved street sections in Section 6.8 of this Code are intended to guide design. Alternative street sections may be created if they achieve equal or better outcomes in function, safety, and streetscape quality. Such alternatives shall be reviewed and approved by the Approval Authority.

STREET TYPES:

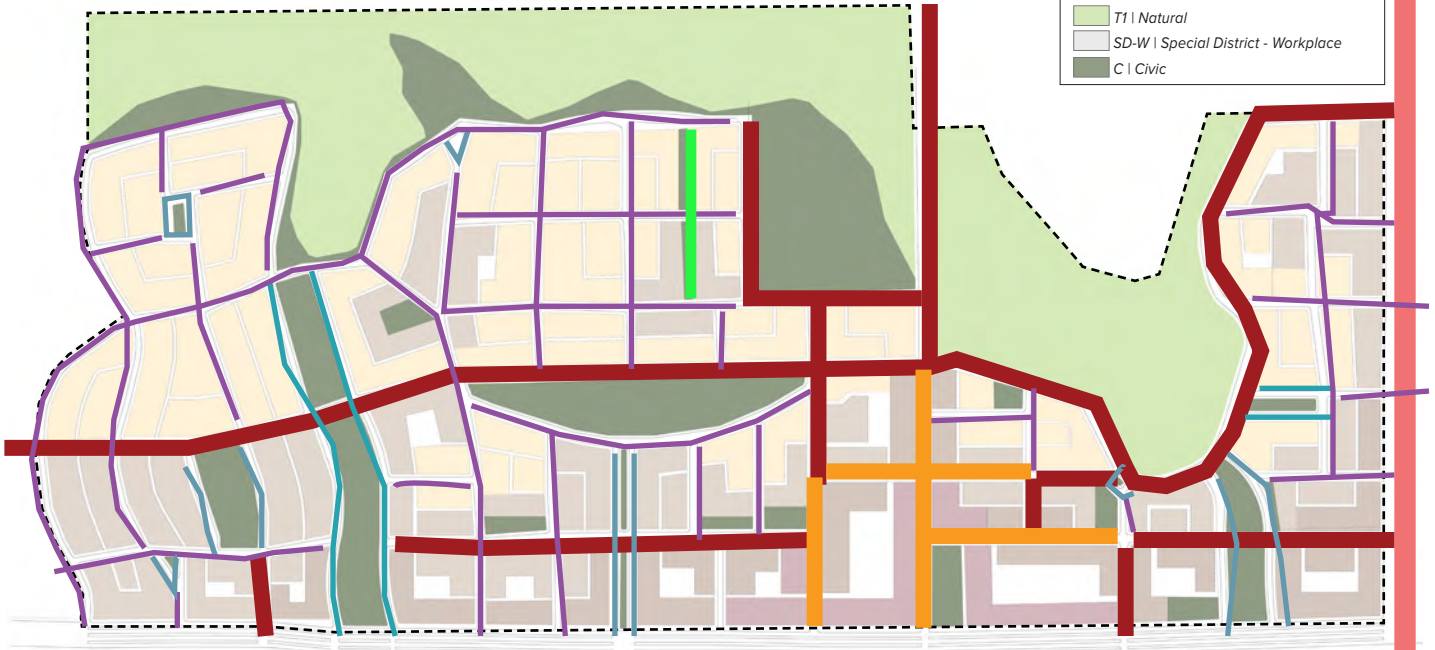
- Principal Arterial
- Main Street 1 / 2
- Urban Streets
- Neighborhood Streets
- Neighborhood One Way
- Pedestrian Street
- Alley / Lane

Legend

Property Line / Placetype Boundary

Transect Zones

- T5 | Mixed-Use Center
- T4-O | Neighborhood General Open
- T4-R | Neighborhood General Restricted
- T3 | Neighborhood Edge
- T2 | Rural
- T1 | Natural
- SD-W | Special District - Workplace
- C | Civic



Not to Scale

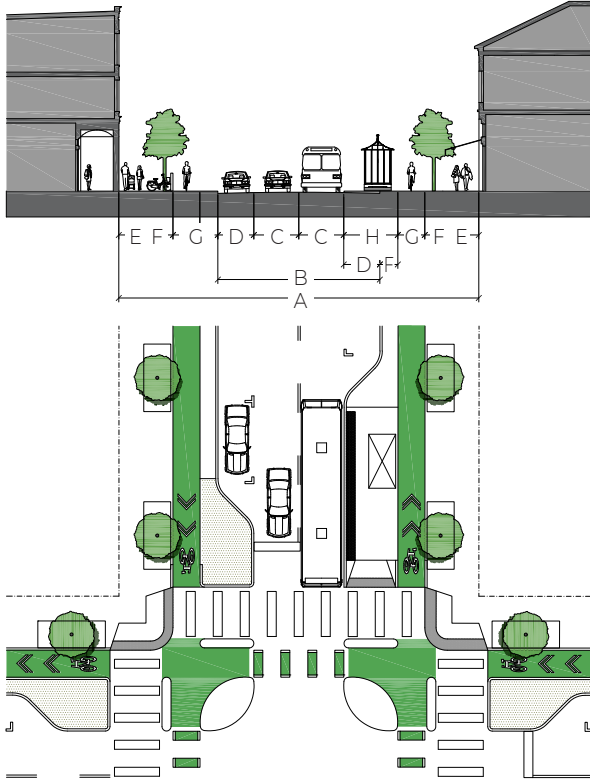


Figure 6-7: Sample Street Atlas

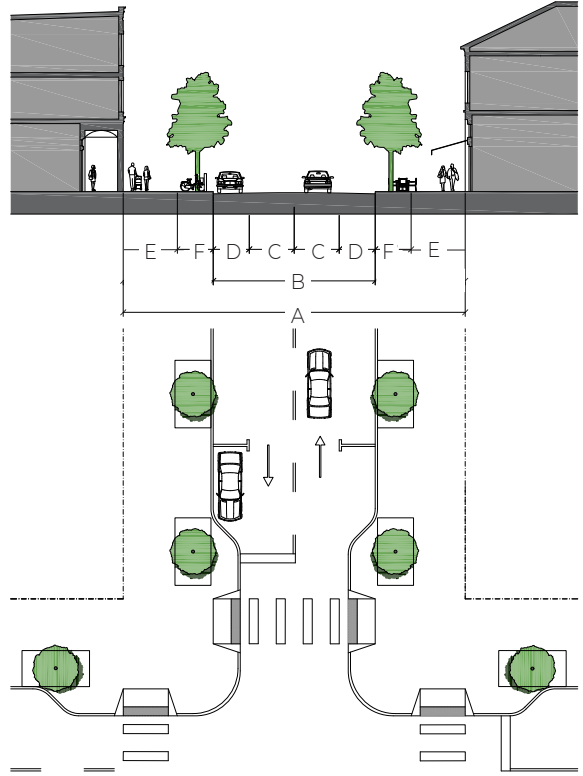
SECTION 6.10 STREET TYPES - TYPICAL SECTIONS

The Typical Intersections shown in Sec. 6.7 represent possible intersection concepts only and are not fully engineered designs nor do they represent the full range of intersection treatments that may be appropriate.

A. Main Street Collector



B. Main Street - Primary



Thoroughfare Type	Main Street Collector	
Transect Zone Assignment	T5, T4-O, SD-W	
Right-of-Way Width	90 feet	A
Pavement Width	36-38 feet	B
Traffic Lanes	Two lanes - 10-11 feet wide ¹	C
Transit	Bus	H
Bicycle / Micro-Mobility Facility	Two - 5-6' Protected Lanes 3 foot buffer	G
Parking Lanes/Curbside Flex Zone	Both sides @ 8 feet marked	D
Sidewalk Walking Zone	12 feet min.	E
Landscape Zone - Sidewalk	10' wide x 15' Tree Wells ²	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb	
Green Infrastructure	Bioswale, Tree Box Filter	F

¹ 11-12 feet wide lanes are only acceptable for bus lanes

² Tree wells smaller than 7' wide by 15' are permitted if suspended pavement system is utilized.

Thoroughfare Type	Main Street - Primary	
Transect Zone Assignment	T5, T4-O, SD-W	
Right-of-Way Width	76-80 feet ¹	A
Pavement Width	36-40 feet ¹	B
Traffic Lanes	Two lanes - 10 feet wide (Up to 12' in SD-W)	C
Transit	Bus	
Bicycle / Micro-Mobility Facility	Shared Travel Lanes	C
Parking Lanes/Curbside Flex Zone	Both sides @ 8 feet marked ¹	D
Sidewalk Walking Zone	12 feet min.	E
Landscape Zone - Sidewalk	8' wide x 15' Tree Wells ²	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb or Curbless	
Green Infrastructure	Bioswale, Tree Box Filter	F

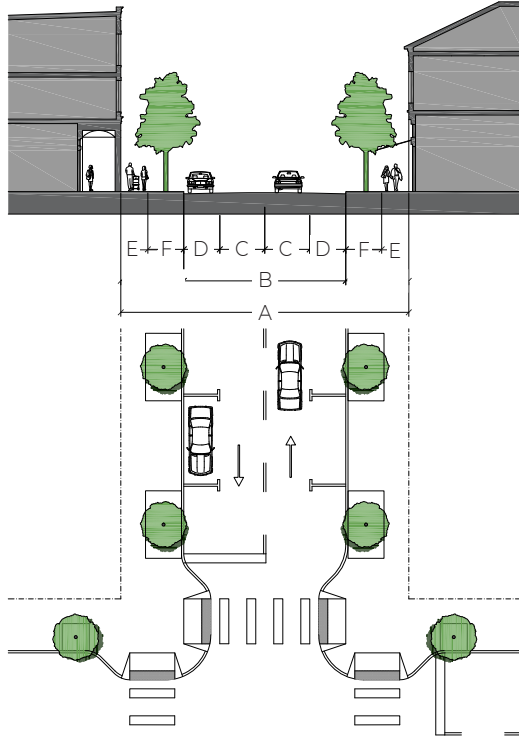
¹ When adjacent to Civic Space, the adjacent Parking Lane/Curbside Flex Zone may be eliminated

² Smaller tree wells are permitted if suspended pavement system is utilized.

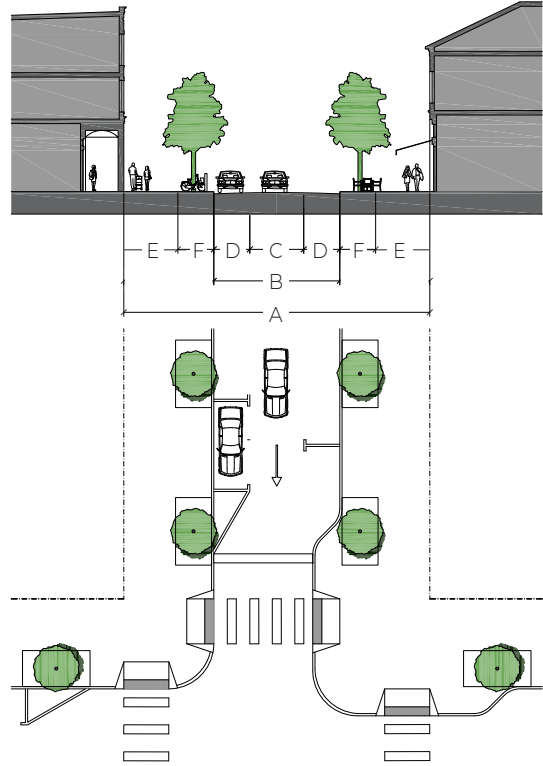
DIVISION 6

THOROUGHFARE STANDARDS

B. Main Street - Secondary



C. Main Street One-way



Thoroughfare Type	Main Street Secondary	
Transect Zone Assignment	T5, T4-O	
Right-of-Way Width	64 feet min.	A
Pavement Width	36 feet	B
Traffic Lanes	Two lanes - 10 feet wide	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	Shared Travel Lanes	C
Parking Lanes/Curbside Flex Zone	Both sides @ 8 feet marked	D
Sidewalk Walking Zone	6-12 feet min.	E
Landscape Zone - Sidewalk	8' wide x 15' Tree Wells ¹	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb or Curbless	
Green Infrastructure	Bioswale, Tree Box Filter	F

¹ Smaller tree wells are permitted if suspended pavement system is utilized.

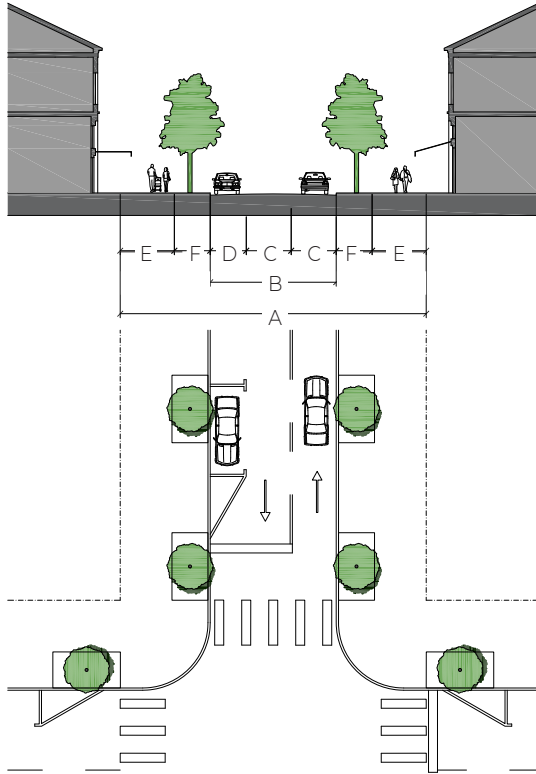
Thoroughfare Type	Main Street One-way	
Transect Zone Assignment	T5, T4-O	
Right-of-Way Width	48-68 feet ¹	A
Pavement Width	20-28 feet ¹	B
Traffic Lanes	One lane - 12 feet wide	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	Shared Travel Lane	C
Parking Lanes/Curbside Flex Zone	Both sides @ 8 feet marked ¹	D
Sidewalk Walking Zone	6-12 feet	E
Landscape Zone - Sidewalk	8' wide x 15' Tree Wells ²	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb or Curbless	
Green Infrastructure	Bioswale, Tree Box Filter	F

¹ When adjacent to Civic Space, the adjacent Parking Lane/Curbside Flex Zone may be eliminated

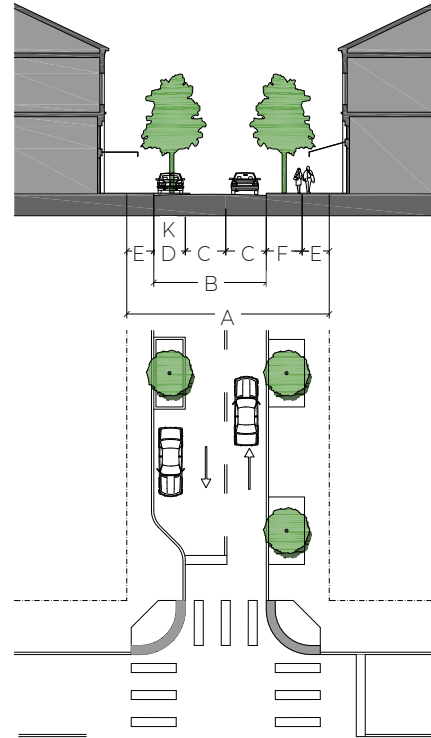
² Smaller tree wells are permitted if suspended pavement system is utilized.

The Typical Intersections shown in Sec. 6.7 represent possible intersection concepts only and are not fully engineered designs nor do they represent the full range of intersection treatments that may be appropriate.

D. Urban Primary Street



E. Urban Secondary Street



Thoroughfare Type	Urban Primary Street	
Transect Zone Assignment	T5, T4-O, T4-R, SD-W	
Right-of-Way Width	56-80 feet	A
Pavement Width	28-40 feet	B
Traffic Lanes	Two lanes - 10 feet wide (Up to 12' in SD-W)	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	Shared Travel Lanes	C
Parking Lanes/Curbside Flex Zone	One or Two sides @ 8 feet marked	D
Sidewalk Walking Zone	6-12 feet	E
Landscape Zone - Sidewalk	8' wide x 15' Tree Wells ¹	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb	
Green Infrastructure	Bioswale, Tree Box Filter	F

¹ Smaller tree wells are permitted if suspended pavement system is utilized.

Thoroughfare Type	Urban Secondary Street	
Transect Zone Assignment	T5, T4-O, T4-R	
Right-of-Way Width	45 feet	A
Pavement Width	28 feet	B
Traffic Lanes	Two lanes - 10 feet wide	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	Shared Travel Lanes	C
Parking Lanes/Curbside Flex Zone	One side @ 8 feet marked	D
Sidewalk Walking Zone	6-12 feet	E
Landscape Zone - Sidewalk	8' wide x 15' Tree Wells ¹	F
	7' wide x 15' parking planter	K
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb	
Green Infrastructure	Bioswale, Tree Box Filter, Pervious Pavers	F/K

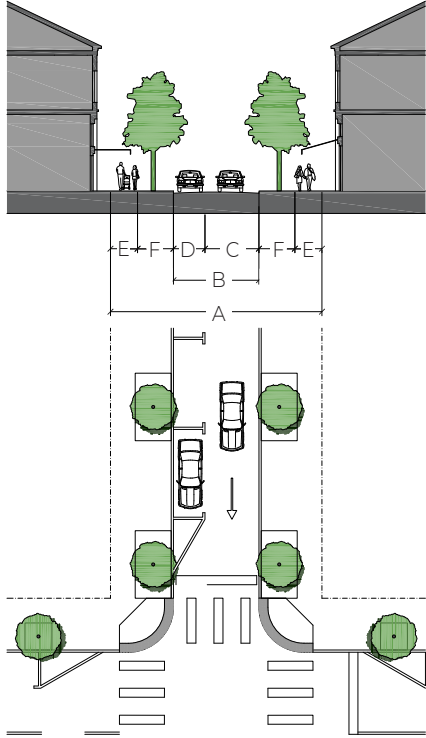
¹ Smaller tree wells are permitted if suspended pavement system is utilized.

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DIVISION 6

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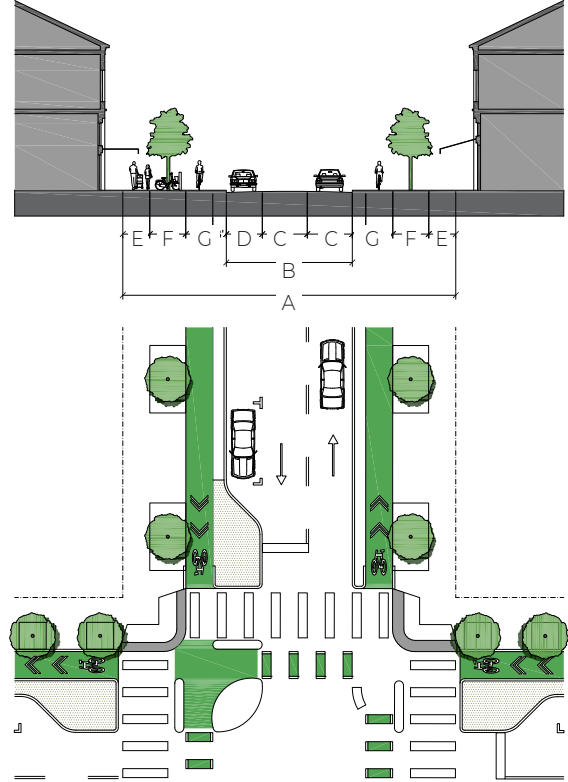
F. Urban One-way Street



Thoroughfare Type	Urban One-way	
Transect Zone Assignment	T5, T4-O, T4-R	
Right-of-Way Width	48 feet	A
Pavement Width	20 feet	B
Traffic Lanes	One lane - 12 feet wide	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	Shared Travel Lane	C
Parking Lanes/Curbside Flex Zone	One side @ 8 feet marked	D
Sidewalk Walking Zone	6-12 feet	E
Landscape Zone - Sidewalk	8' wide x 15' Tree Wells ¹	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb	
Green Infrastructure	Bioswale, Tree Box Filter	F

¹ Smaller tree wells are permitted if suspended pavement system is utilized.

G. Urban Bike Street

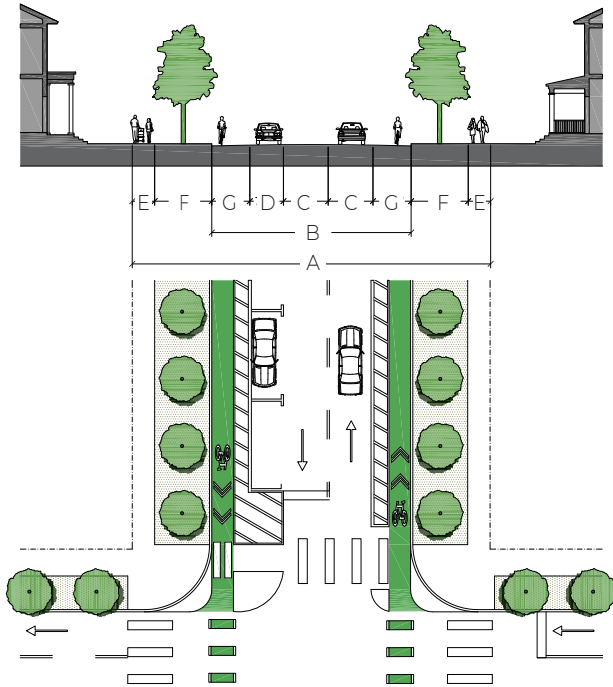


Thoroughfare Type	Urban Bike	
Transect Zone Assignment	T5, T4-O, T4-R, SD-W	
Right-of-Way Width	74-82 feet min.	A
Pavement Width	28-36 feet	B
Traffic Lanes	Two lanes - 10 feet wide	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	Two - 6' Protected Lanes 3 foot buffer	G
Parking Lanes/Curbside Flex Zone	One or Two sides @ 8 feet marked	D
Sidewalk Walking Zone	6-12 feet	E
Landscape Zone - Sidewalk	8' wide x 15' Tree Wells ¹	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb	
Green Infrastructure	Bioswale, Tree Box Filter	F

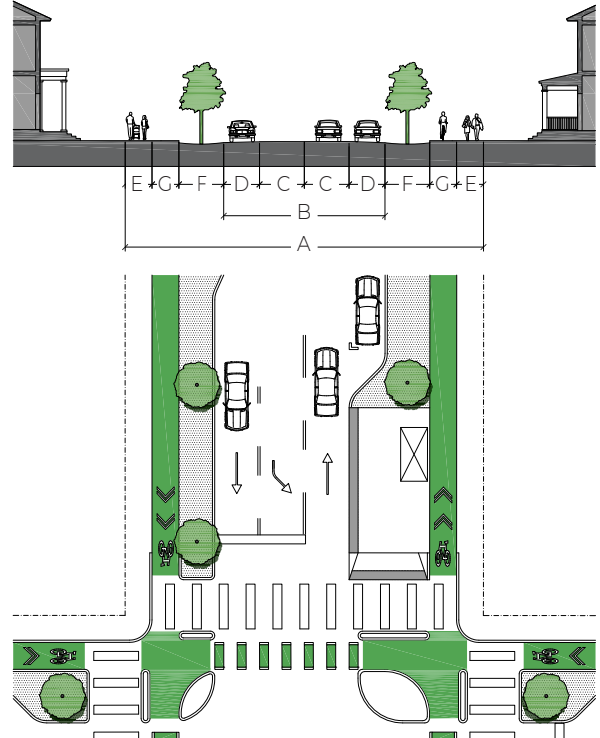
¹ Smaller tree wells are permitted if suspended pavement system is utilized.

The Typical Intersections shown in Sec. 6.7 represent possible intersection concepts only and are not fully engineered designs nor do they represent the full range of intersection treatments that may be appropriate.

I. Neighborhood Collector 1



H. Neighborhood Collector 2



Thoroughfare Type	Neighborhood Collector Existing Street Segments	
Transect Zone Assignment	N/A	
Right-of-Way Width	80-87.5 feet	A
Pavement Width	44.5-52 feet	B
Traffic Lanes	Two - 10.5 foot drive lanes	C
Transit	Bus	
Bicycle / Micro-Mobility Facility	Two - 5' Protected Lanes 3 foot striped buffer	G
Parking Lanes/Curbside Flex Zone	One or Two sides @ 7.5 feet marked	D
Sidewalk Walking Zone	6 feet min.	E
Landscape Zone	12.75 foot continuous planter	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb	
Green Infrastructure	Bioswale	F

Thoroughfare Type	Neighborhood Collector	
Transect Zone Assignment	T4-R, T3	
Right-of-Way Width	90 feet	A
Pavement Width	44-46 feet	B
Traffic Lanes	Two - 10-11 foot drive lanes ¹	C
Transit	Bus	
Bicycle / Micro-Mobility Facility	Two - 6' Protected Lanes	G
Parking Lanes/Curbside Flex Zone	Both sides @ 7-8 feet marked	D
Sidewalk Walking Zone	6 feet min.	E
Landscape Zone	9 to 15 foot continuous planter	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb	
Green Infrastructure	Bioswale	F

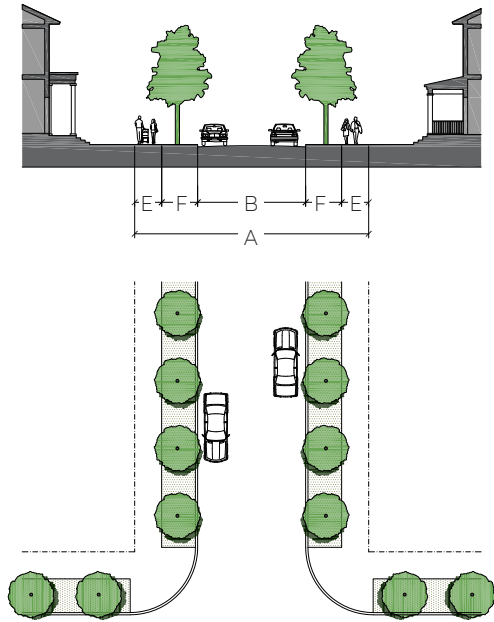
¹ 11-12 feet wide lanes are only acceptable for bus lanes

The Typical Intersections shown in Sec. 6.7 represent possible intersection concepts only and are not fully engineered designs nor do they represent the full range of intersection treatments that may be appropriate.

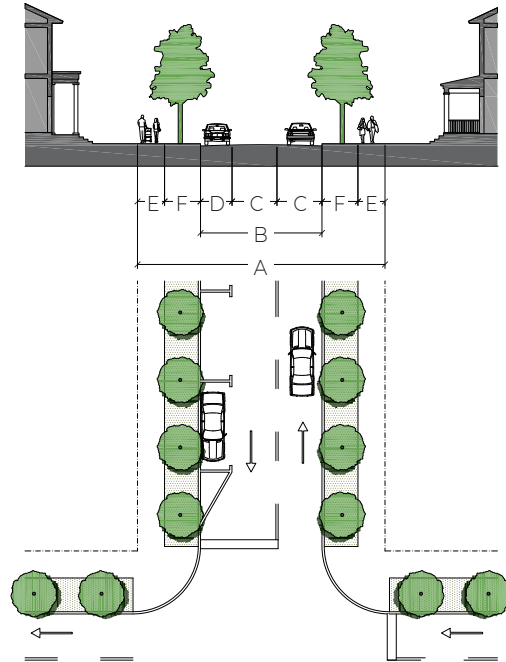
DIVISION 6

THOROUGHFARE STANDARDS

J. Neighborhood Yield Street



K. Neighborhood Street



Thoroughfare Type	Neighborhood Yield	
Transect Zone Assignment	T4-R, T3	
Right-of-Way Width	54 - 62 feet	A
Pavement Width	26 feet	B
Traffic Lanes	n/a (Yield Movement)	B
Transit	n/a	
Bicycle / Micro-Mobility Facility	Shared Travel Lanes	B
Parking Lanes/Curbside Flex Zone	Unmarked On-Street Parking	B
Sidewalk Walking Zone	6 feet	E
Landscape Zone	8 - 12' wide planting strip	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb or Curbless	
Green Infrastructure	Bioswale	F

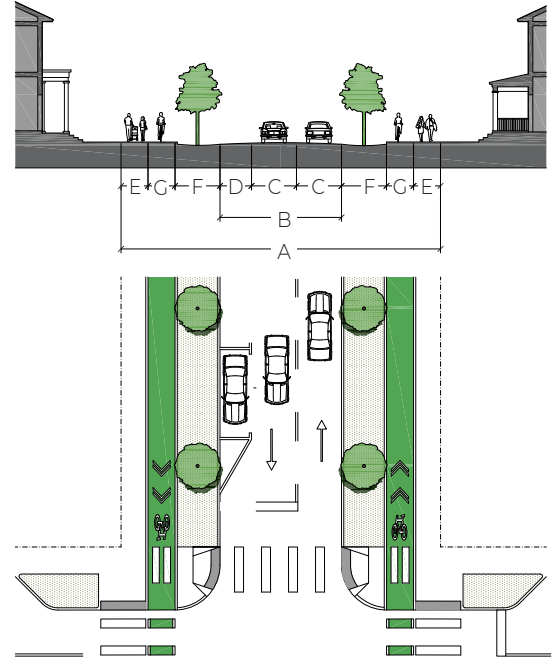
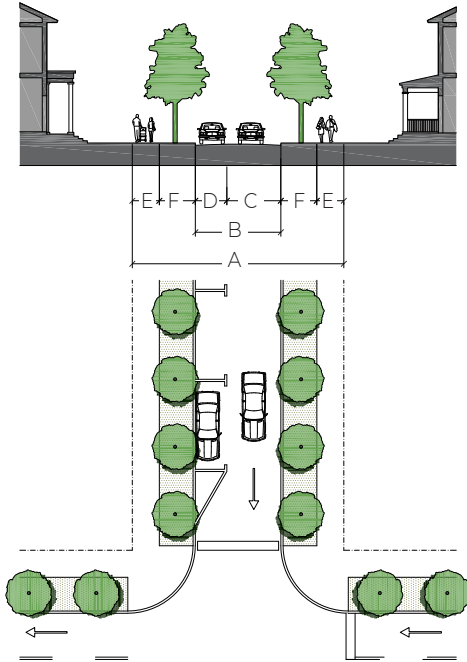
Thoroughfare Type	Neighborhood Street	
Transect Zone Assignment	T4-R, T3	
Right-of-Way Width	55-63 feet	A
Pavement Width	27 feet	B
Traffic Lanes	Two - 10 foot drive lanes	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	Shared Travel Lanes	C
Parking Lanes/Curbside Flex Zone	One side @ 7 feet	D
Sidewalk Walking Zone	6 feet	E
Landscape Zone	8 - 12' wide planting strip	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb or Curbless	
Green Infrastructure	Bioswale	F

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L. Neighborhood One-way Street



M. Neighborhood Bike Street



Thoroughfare Type	Neighborhood One-way	
Transect Zone Assignment	T4-R, T3	
Right-of-Way Width	48-56 feet	A
Pavement Width	20 feet	B
Traffic Lanes	One - 12 foot drive lane	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	Shared Travel Lane	C
Parking Lanes/Curbside Flex Zone	One side @ 8 feet	D
Sidewalk Walking Zone	6 feet	E
Landscape Zone	8 - 12' wide planting strip	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb or Curbless	
Green Infrastructure	Bioswale	F

Thoroughfare Type	Neighborhood Bike	
Transect Zone Assignment	T4-R, T3	
Right-of-Way Width	71 feet	A
Pavement Width	27 feet	B
Traffic Lanes	Two - 10 foot drive lanes	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	Two - 6' Protected Lanes	G
Parking Lanes/Curbside Flex Zone	None	
Sidewalk Walking Zone	6 feet	E
Landscape Zone	6' wide planting strips	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb or Curbless	
Green Infrastructure	Bioswale	F

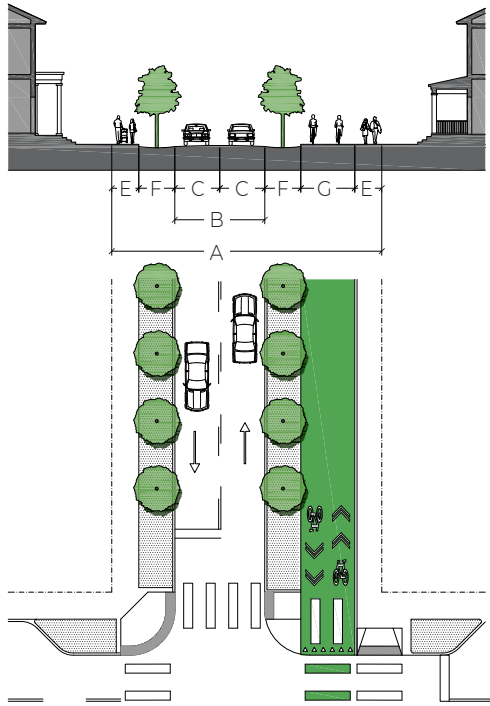
The Typical Intersections shown in Sec. 6.7 represent possible intersection concepts only and are not fully engineered designs nor do they represent the full range of intersection treatments that may be appropriate.

DIVISION 6

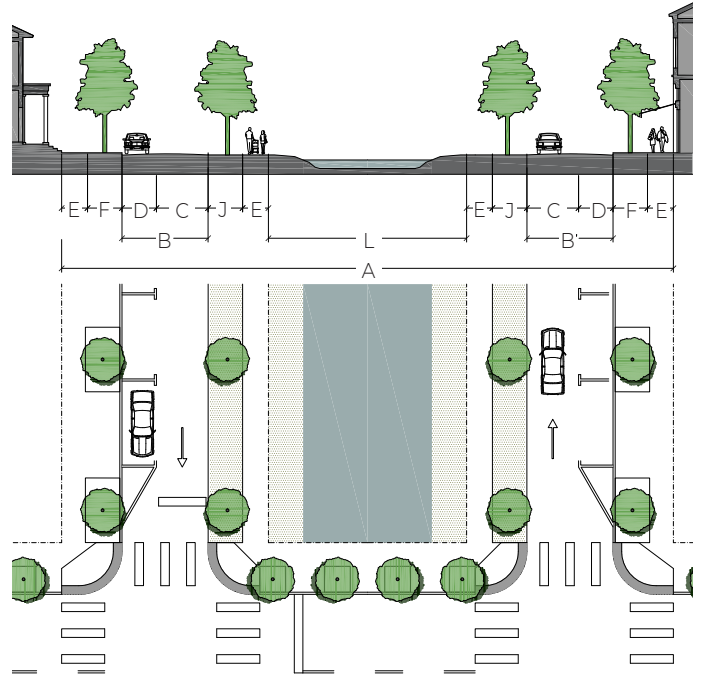
THOROUGHFARE STANDARDS

The Typical Intersections shown in Sec. 6.7 represent possible intersection concepts only and are not fully engineered designs nor do they represent the full range of intersection treatments that may be appropriate.

N. Neighborhood Bike Street (Two-Way Track)



O. Canal Street



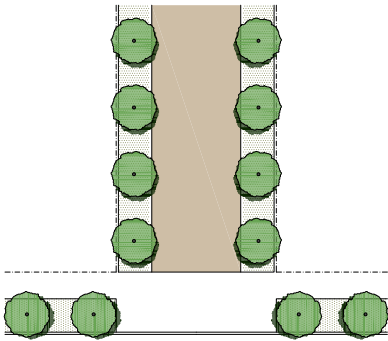
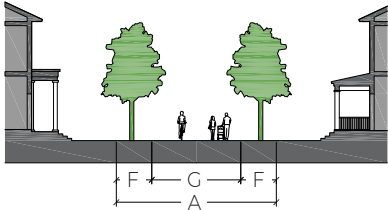
Thoroughfare Type	Neighborhood Bike (Two-way Track)	
Transect Zone Assignment	T4-R, T3	
Right-of-Way Width	60 feet min.	A
Pavement Width	20 feet	B
Traffic Lanes	Two - 10 foot drive lanes	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	12 foot min. two-way Cycle Track ¹	G
Parking Lanes/Curbside Flex Zone	None	
Sidewalk Walking Zone	6 feet	E
Landscape Zone	8' min. wide planting strips	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb or Swale	
Green Infrastructure	Bioswale	F

¹ Cycle Track may be located on either side of Street

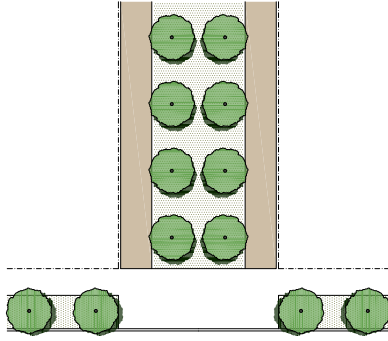
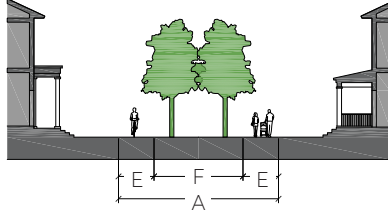
Thoroughfare Type	Canal	
Transect Zone Assignment	ALL	
Right-of-Way Width	140 feet minimum	A
Pavement Width	Two sides - 19 feet	B
Traffic Lanes	Two lanes - 12 foot lanes	C
Transit	n/a	
Bicycle / Micro-Mobility Facility	Shared Travel Lanes	C
Parking Lanes/Curbside Flex Zone	One side @ 8 feet marked	D
Sidewalk Walking Zone	6 feet ¹	E
Landscape Zone - Sidewalk	8' wide x 15' Tree Wells or 8' wide planting strips	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	Curb or swale	F/J
Green Infrastructure	Bioswale, Tree Box Filter	F
	Bioswale	J
	Canal, Rain Garden	L
Green Median / Canal	Minimum 44 feet	L

¹ Sidewalks along canal are optional

P. Trail Street — —



Q. Pedestrian Street = =



Thoroughfare Type	Trail Street	
Transect Zone Assignment	ALL	
Right-of-Way Width	36 feet min.	A
Pavement Width	n/a	
Traffic Lanes	n/a	
Transit	n/a	
Bicycle / Micro-Mobility Facility	20 feet min. Shared-Use Path	G
Parking Lanes/Curbside Flex Zone	n/a	
Sidewalk Walking Zone	20 feet min. Shared-Use Path	G
Landscape Zone	8' wide planting strips	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	n/a	
Green Infrastructure	Bioswale	F

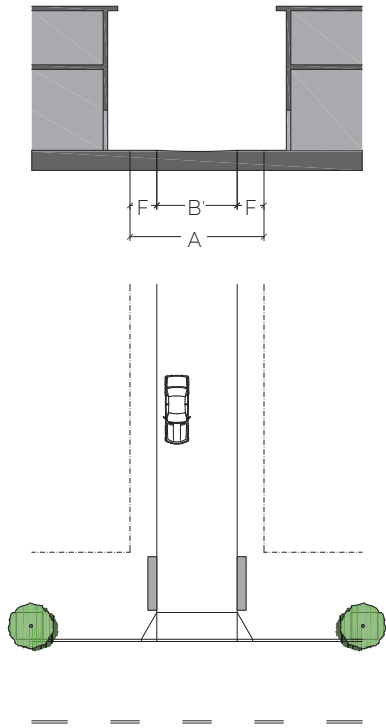
Thoroughfare Type	Green Street	
Transect Zone Assignment	ALL	
Right-of-Way Width	36 feet min.	A
Pavement Width	n/a	
Traffic Lanes	n/a	
Transit	n/a	
Bicycle / Micro-Mobility Facility	n/a	
Parking Lanes/Curbside Flex Zone	n/a	
Sidewalk Walking Zone	6 feet min.	E
Landscape Zone	24' min.	F
Landscape Type	Trees @ 35' o.c. average	F
Road Edge Treatment	n/a	
Green Infrastructure	Rain Garden	F

The Typical Intersections shown in Sec. 6.7 represent possible intersection concepts only and are not fully engineered designs nor do they represent the full range of intersection treatments that may be appropriate.

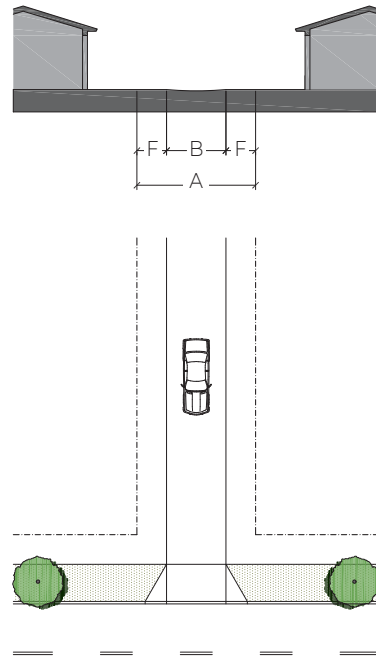
DIVISION 6

THOROUGHFARE STANDARDS

S. Alley



R. Rear Lane



Thoroughfare Type	Alley	
Transect Zone Assignment	T4-R, T4-O, T5	
Right-of-Way Width	24 feet	A
Pavement Width	18-20 feet	B
Traffic Lanes	n/a (Slow Movement)	B
Transit	n/a	
Bicycle / Micro-Mobility Facility	n/a (Slow Movement)	B
Parking Lanes/Curbside Flex Zone	n/a	
Sidewalk Walking Zone	n/a	
Landscape Zone	2-3 feet for utilities and services	F
Landscape Type	None	F
Road Edge Treatment	Inverted Crown	
Green Infrastructure	n/a	

Thoroughfare Type	Rear Lane	
Transect Zone Assignment	T4-R, T3	
Right-of-Way Width	20 feet	A
Pavement Width	12 feet	B
Traffic Lanes	n/a (Yield Movement)	B
Transit	n/a	
Bicycle / Micro-Mobility Facility	n/a (Slow Movement)	B
Parking Lanes/Curbside Flex Zone	n/a	
Sidewalk Walking Zone	n/a	
Landscape Zone	4 feet for utilities and services	F
Landscape Type	None	F
Road Edge Treatment	n/a	
Green Infrastructure	Pervious Pavement	

The Typical Intersections shown in Sec. 6.7 represent possible intersection concepts only and are not fully engineered designs nor do they represent the full range of intersection treatments that may be appropriate.

(Draft)

DIVISION 7: DEVELOPMENT REVIEW PROCEDURES



SECTION 7.1 PURPOSE

- A.** The purpose of this Division is to provide optional development review procedures that implement the Form-Based Code (FBC).
- B.** The development review process established in this Division may be applied, at the applicant's election, to proposed development activity within the Greeley Westside Area as an alternative to the underlying zoning regulations.
- C.** Development, including clearing, excavation of soil, or alteration of vegetation, may proceed under the standards of the FBC when an applicant chooses to do so. In such cases, it is the applicant's responsibility to demonstrate consistency with the goals, objectives, policies, and provisions of the FBC.

SECTION 7.2 APPLICABILITY

A. Geographic Area

The provisions of this apply to all properties located within the Greeley Westside Area, as identified on the Figure 2-1: Placetypes Plan - Regulatory map for the FBC.

B. Optional Application

This Form-Based Code (FBC) is established as an optional alternative regulatory path to the underlying zoning and development standards set forth in Title 24 of the Greeley Municipal Code (the "Development Code"). A property owner or applicant may elect to proceed under the Greeley Westside FBC in lieu of the applicable underlying zoning district and associated standards.

C. Election Procedure

- 1.** Prior to formal application, the applicant shall submit a written notice on a form provided by the Community Development Department indicating the election to proceed under the Greeley Westside FBC.

- 2.** Upon acceptance of a complete application under the Greeley Westside FBC, the development shall be processed, reviewed and approved in accordance solely with the standards, procedures and review bodies identified in this FBC. The underlying zoning district standards shall no longer apply.

D. Underlying Zoning Continues

If the applicant elects not to proceed under the Greeley Westside FBC, the property shall remain subject to all applicable provisions of Title 24 (Development Code), including the underlying zoning district, applicable review procedures, development and design standards, and other requirements of the City of Greeley.

E. Relationship to Other Regulations

- 1.** For projects processed under the Greeley Westside FBC, where a conflict exists between a standard in this FBC and a standard in another portion of Title 24 (or other applicable Chapter of the Greeley Municipal Code), the standard of this FBC shall govern only to the extent expressly stated herein.
- 2.** Projects proceeding under the underlying zoning shall continue to follow the hierarchy of standards in Title 24 and other applicable City codes.

F. Time of Election

An election to use the Greeley Westside FBC must be made prior to submission of the initial development review application. Once an election is made and accepted by the City, the election is irrevocable for that development application and any associated amendments or phases, unless otherwise approved by the City Council.

SECTION 7.3 PLACETYPE PLAN

- A.** The Placetypes Plan in Figure 2-1 is regulatory and sets development standards for each neighborhood.

SECTION 7.4 PRE-APPLICATION CONFERENCE

Applicants are strongly encouraged to schedule a pre-application meeting with Planning Department staff to review standards, goals, and procedures prior to formal submission.

SECTION 7.5 MASTER AND FINAL SITE PLANS REQUIRED

- A.** All development within the Greeley Westside Area requires Master Site Plan and Final Site Plan approval.
- B.** For development requiring subdivision approval, preliminary plat review shall occur congruent with the Master Site Plan review, and final plat review shall occur congruent with Final Site Plan review.
- C.** Issuance of a development order for a Master Site Plan shall authorize the applicant to submit the Final Site Plan(s) in accordance with the terms and conditions of the Master Site Plan, including the timetable of development. Issuance of a development order for a Master Site Plan shall not constitute approval to build structures and is not the final approval necessary for construction of the development.
- D.** Issuance of a development order for a Final Site Plan shall authorize the applicant to receive final plats or other instruments of conveyance and Lot Site Plans or building permit applications, in accordance with the terms and conditions of the approval. Permission to initiate construction of building permits shall not be issued until all required documents are executed and all applicable conditions of approval are satisfied.
- E.** All Master, Final, and Lot Site Plans submitted to the Approval Authority shall be in compliance with FBC requirements.

SECTION 7.6 MASTER SITE PLANS

- A.** The land area encompassed by a Master Site Plan shall:
 - 1.** Encompass a portion of a Placetype that the property owner controls, be in compliance with all other placetype development standards pursuant to Table 2-1 Placetype Standards, and meet the intent of the Regulating Plan and Block structure, as illustrated within Figure 2-2 Example Regulating Plan.
- B.** The following documents must be submitted by the applicant to the Approval Authority for review and approval:
 - 1.** Illustrative plan that is “to scale” and clearly illustrates a placetype structure, a network of Streets and Blocks, the location of Lots, and the reservation of Lots for Civic Buildings and Open Space;
 - 2.** Regulating Plan showing the Lot, Block, and Street network and allocation of Transect Zones;
 - 3.** Street atlas showing the Street Thoroughfare locations and Street Types;
 - 4.** Conceptual stormwater management plan or stormwater information required for the division of land in accordance with the Approval Authority’s subdivision regulations;
 - 5.** Overall program and number of units;
 - 6.** Illustrations that depict the proposed scale and character of the development of the area; and,
 - 7.** If a division of land is to occur, the submittal must include an application for the division of land and all supporting materials, according to the Approval Authority’s subdivision regulations.
- C.** These documents must clearly:
 - 1.** Identify any natural resources with T1 or other Transect Zones that are to be protected through easement or other means;
 - 2.** Establish a maximum allowable development program; and,
 - 3.** Meet all other Placetype requirements as established in Divisions 2, 3, and 5 of this FBC.
- D.** Applications for approval of Master Site Plans shall be subject to the following:

1. The application shall be filed with the Approval Authority Administrator by the owner or other person having power of attorney to act as agent for the owner.
 2. Applications shall be received by the Approval Authority for processing on any working day.
 3. Proof of ownership and financial disclosure is required.
 4. The application shall be submitted in a form approved by the Approval Authority Administrator and made available to the public. At a minimum, it shall include sufficiently detailed and documented information for staff to make the required findings of compliance.
 5. Each application shall be accompanied by the application fee as established by resolution of the Planning Commission or City Council.
 6. If the division of land is required, the application must also include all materials and fees established, according to the Approval Authority's fee schedule and subdivision regulations.
- E. If a Master Site Plan application approval requires changes to the Placetypes Plan, the applicant shall address such changes within the application materials submitted with the Master Site Plan application. The change to the Placetype Plan shall require approval of an amendment by the Planning Commission or City Council, unless the change qualifies as a Warrant.
 - F. Applications will be reviewed for completeness within 5 working days. Applications deemed incomplete will be returned to the applicant with a letter outlining the deficiencies that need to be addressed for review.
 - G. The Approval Authority Administrator shall prepare a staff report within 30 working days of transmittal that addresses all of the requirements of the FBC.
 - H. Upon a finding of noncompliance, a resubmittal of requested materials shall be made within 30 working days of the issuance of the staff report. If the applicant fails to meet the resubmittal deadline, the application shall be terminated, unless the applicant gives notice that an elective resubmittal will be made. The elective resubmittal shall be made within 90 days from the date the prior resubmittal was due and shall include the resubmittal fee established by resolution of the Planning Commission or City Council. All documents that have expired must be updated by the applicant. Upon receipt of the resubmittal, staff will have 20 working days to review and provide comments. Minor adjustments to plans and application materials that do not require full staff review may be amended prior to the Planning Commission or City Council hearing at the discretion of the Approval Authority's Administrator.
 - I. Upon findings of compliance with the FBC, the development application shall be scheduled for a public hearing with the Greeley Planning Board and subsequently for consideration by the Planning Commission or City Council at a public hearing.
 - J. Once a Master Site Plan has been approved by the governing body, the applicant may submit an application for Final Site Plan. No Final Site Plan may be approved without approval of the corresponding Master Site Plan.
 - K. An approved Master Site Plan is valid for ten (10) years or according to the subdivision's approval, if applicable.

SECTION 7.7 FINAL SITE PLANS

- A.** A Final Site Plan application shall contain a minimum area that is two Block faces across a Right-of-Way or public space, or similar acreage that results in a complete place.
- B.** If the development involves a subdivision, the Final Site Plan must correspond to the preliminary plat and approved phasing plan.
- C.** The following documents must be submitted by the applicant to the Approval Authority for review and approval:
1. Illustrative plan that is “to scale” and clearly illustrates a network of Streets and Blocks, the location of Lots, and the reservation of Lots for Civic Buildings and Open Space;
 2. Regulating Plan showing the Transect Zones, Lot, Block and Street network;
 3. Street types;
 4. Detailed stormwater management plan;
 5. Development program and percentages of total neighborhood program; and,
 6. If a division of land is to occur, the submittal must include an application for a final plat for the applicable phase and all supporting materials, according to the Approval Authority’s subdivision regulations.
- D.** Applications for approval of Final Site Plans shall be subject to the following:
1. The application shall be filed with the Approval Authority Administrator by the owner or other person having power of attorney to act as agent for the owner.
 2. Applications shall be received by the Approval Authority for processing on any working day.
 3. Proof of ownership and financial disclosure is required.
 4. The application shall be submitted in a form approved by the Approval Authority Administrator and made available to the public. At a minimum, it shall include sufficiently detailed and documented information for staff to make the required findings of compliance. All applications shall include a checklist specific to Final Site Plan applications, submitted in accordance with this FBC.
 5. Each application shall be accompanied by the application fee as established by resolution of the Planning Commission or City Council.
 6. If the development includes a final plat, the application must also include all materials and fees established according to the Approval Authority’s fee schedule and subdivision regulations.
- E.** Applications will be reviewed for completeness within 5 working days. Applications deemed incomplete will be returned to the applicant with a letter outlining the deficiencies that need to be addressed for review. Applications found complete will be transmitted to development review staff.
- F.** The Approval Authority Administrator shall prepare a staff report within 20 working days of transmittal that addresses all of the requirements of the FBC.
- G.** Upon a finding of non-compliance, the applicant will have 30 working days to re-submit appropriate application materials addressing staff comments. If the applicant fails to meet the resubmittal deadline, the application shall be terminated, unless the applicant gives notice that an elective resubmittal will be made. The elective resubmittal shall be made within 90 days from the date the prior resubmittal was due and shall include the resubmittal fee established by resolution of the Planning Commission or City Council. All documents that have expired must be updated by the applicant.
- H.** Upon findings of compliance with the FBC, the development application shall be scheduled for a public hearing with the Planning Commission or City Council.
- I.** Upon a finding of compliance with the FBC, the Approval Authority Administrator shall issue a development order approving the Final Site Plan and listing all conditions of approval for site development. A post-submittal requirement letter, along with the development order, shall be issued to the applicant within 5 working days.
- J.** The plat approval process shall be in accordance with the City of Greeley’s LDRs.
- K.** A Final Site Plan approval is valid for five (5) years and all infrastructure and horizontal development authorized by a Final Site Plan development order shall be completed within five (5) years. Parks, Streets and other Public Facilities shall be dedicated to the City or County within five (5) years. Timetable amendments shall be obtained according to Approval Authority regulations. Where the development order includes a subdivision of Lots for individual resale, this mandatory timetable shall not apply to the development of approved uses on individual Lots.

SECTION 7.8 LOT SITE PLAN APPROVAL

- A.** Lot Site Plan approval is reserved for Lots that are within an approved Final Site Plan. The development of individual Lots must be consistent with the approved uses on the approved Final Site Plan and the Regulating Plan and shall conform to Divisions 2 through 6 of this FBC.
- B.** No Lot Site Plan shall be approved by the development review staff until a plat consistent with the approved Final Site Plan has been recorded.
- C.** No Lot Site Plan shall be processed without a letter of approval from the Approval Authority.
- D.** The design of Civic Buildings shall be approved by the Approval Authority and development review staff.

SECTION 7.9 POST-APPROVAL

- A.** After a development order has been issued, the applicant will have 30 working days to submit all items listed in the Post-Approval letter. Upon the determination that all requirements are met, including the issuance of any required state and federal agency permits, the Approval Authority will schedule a pre-construction meeting, if applicable. After a pre-construction meeting has taken place, construction is permitted consistent with the approved development order.

SECTION 7.10 WARRANTS, EXCEPTIONS, AND AMENDMENTS

- A.** This section provides a mechanism by which a proposed development may vary from the strict requirements of Divisions 2 through 6 of this FBC. This section also provides for amendment of approved Master Site Plans and Final Site Plans. Lastly, this section addresses amendments to this FBC. The intent of this section is to provide flexibility for unusual situations and offer alternative ways to meet the purposes of this FBC, while ensuring that the FBC realizes the vision sought by the community for the Greeley Westside Area.
- B.** Any proposed variation from the requirements of this FBC shall be reviewed by the Approval Authority Administrator. If the proposed development is consistent with the intended purpose of this FBC, the Administrator may:
 - 1.** Issue a Warrant allowing a variation from the requirements of Divisions 2 through 6 of this FBC; or
 - 2.** Recommend that the Planning Commission or City Council approve an Exception allowing a variation from the requirements of Divisions 2 through 6 of this FBC; or
 - 3.** Recommend that the FBC be amended, pursuant to Approval Authority regulations.
- C.** Warrants may be approved by the Approval Authority Administrator for the following:
 - 1.** The allowance of a use not listed in Table 3-3 Permitted Uses, upon a finding that the use is functionally similar to the permitted uses and that the use is not likely to generate harmful impacts or create incompatibilities with other surrounding uses.
 - 2.** Modifications of a requirement of Divisions 2 through 6 of this FBC to accommodate circumstances such as natural features, access requirements related to fire and life safety, and site designs that demonstrate excellent urban design or architectural merit.
- D.** Exceptions for proposed Master Site Plans may be approved by the Planning Commission or City Council for the following:
 - 1.** Variation of up to 10% change in criteria found in Table 2-1 for a Placetype Size and allocation of Transect Zones, except for the required Open Space.

SECTION 7.11 INSPECTION

2. Realignment and/or reconfiguration of the Street network that does not change the proposed number of intersections or the Placetype Plan.
- E. Amendments to FBC**
1. Unless a proposed change qualifies as a Warrant or an Exception, changes to the Permitted Use Table, other provisions of the FBC, and the Placetype Plan shall require consideration by the Planning Commission or City Council.
- F. Amendments to Approved Site Plans**
1. The Approval Authority Administrator may approve minor or technical changes to approved Master Site Plans and approved Final Site Plans in accordance with the underlying LDRs.
 2. A proposed amendment of a Master Site Plan that does not qualify as a minor or technical change shall require the approval of the Planning Commission or City Council.
 3. The Approval Authority Administrator may approve minor or technical changes to Final Site Plans including changes that do not affect compliance with this FBC or require changes to permits from outside agencies. All other amendments to Final Site Plans shall require review pursuant to Section 7.7.
- G. An application for a Warrant, Exception, or amendment shall be submitted on a form approved by the Approval Authority Administrator and made available to the public. Each application shall be accompanied by the application fee established by resolution of the Planning Commission or City Council.**
- H. The Approval Authority shall keep a record of all Warrants and Exceptions granted.**
- I. Warrants and Exceptions shall not be issued for the following:**
1. Street or Alley dimensions and required infrastructure;
 2. Parking locations;
 3. Building height;
 4. Protection of natural areas, native habitat, and listed species;
 5. Block Size;
 6. Open Space requirements.
- A. Any member of the Planning Commission or City Council and any duly authorized representative of the Planning Commission or City Council, such as, but not limited to, staff of the Building Department or Public Works Department, may enter and inspect any parcel of land for which a development approval or permit has been issued, or where there is a reasonable cause to believe that a development activity is being carried out, for the purpose of ascertaining the state of compliance with the FBC. The interiors of buildings shall not be subject to such inspections unless related to the enforcement of the building code. No person shall refuse immediate entry or access to any authorized representative of the Planning Commission or City Council or one of the specified agencies who requests entry for the purpose of inspection and who presents appropriate credentials. No person shall obstruct, hamper or interfere with any such inspection. If requested, the owner or operator of the premises shall receive a report setting forth the facts and results of the compliance determination.**

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DIVISION 8: GLOSSARY OF TERMS



SECTION 8.1 DEFINITIONS

A

Accessory Dwelling Unit: Also referred to as accessory apartments, second units, or granny flats. Additional living quarters on single-family lots that are independent of the primary dwelling unit. The separate living spaces are equipped with kitchen and bathroom facilities and can be either attached or detached from the Principal Building.

Agricultural Uses: Commercial, semi-commercial, private, or community farming. This includes peri-urban agriculture, incubator farms, community supported agriculture, and community gardens.

Alley: A service roadway that provides access to properties abutting another Street and that is not intended for general traffic circulation.

Approval Authority: The decision making body.

Awning: A light, protective architectural element entirely supported by, but not permanently attached to a building.

B

Balcony: An open habitable portion of an upper floor extending beyond a building's exterior wall that is not supported from below by vertical columns or piers but is instead supported by either a cantilever or brackets. An accessory area to a Dwelling, with one or more sides permanently open to the exterior, except for a railing or parapet not exceeding four feet in height.

Bioswale: A linear landscape feature used to slow, collect, infiltrate, and filter stormwater that is vegetated with plants that can withstand moisture regimes ranging from flooded to dry that are designed to manage a specified amount of runoff from a large impervious area, such as a parking lot or roadway. A bioswale can accommodate larger quantities of stormwater, is deeper than a rain garden, and is often greater in length than width.

Block: The aggregate of private Lots, Passages, and Alleys, circumscribed by Streets.

Building Height: The vertical distance between (1) the lowest permissible elevation above the existing grade that complies with finished floor elevation requirements as established by flood maps, the Health Department, or building code, along the front of a building and (2) either the highest point of the coping of a flat roof, the deck line of a mansard roof, or the mean height level between eaves and ridge for gable, hip, and gambrel roofs.

Build-to-Zone (BTZ): The range of allowable distances from the front property line along which the principal vertical plane of the building's primary Façade shall be built in order to create a moderately uniform line of buildings along the Street.

C

Canopy: A roof or overhead unenclosed structure that provides shade or shelter from the elements.

Civic Building: A building designed specifically for a Civic Use.

Civic Use: A use that is open to the public at least some of the time and provides a focal point for community interaction. Fosters citizen participation in civic activities, including churches, temples, synagogues, mosques, and other religious facilities; lodges; college or university facilities; exhibition halls and art galleries; grade schools; library; meeting halls; museum or similar facilities; performance theaters; post office; fire house; public administration offices; trade or specialty school facilities; or similar uses.

Cornice: Projecting horizontal decorative molding along the top of a wall or building.

E

Encroachment: A structural or architectural element that breaks the plane of a vertical or horizontal regulatory limit extending into a Setback, the Public Frontage, or the Right-of-Way.

Expression Line: A horizontal line, expressed by a material change or a continuous projection not less than two inches nor more than one foot deep.

F

Façade: The exterior wall of a building.

Façade Transparency: The amount of transparent window glass or other openings in the Façade of a building, relative to the overall surface area of the Façade.

Final Site Plan: A development plan authorizing construction and development within an approved Master Site Plan in the Greeley Westside FBC.

Forecourt: a Private Frontage wherein a portion of the Façade is close to the Frontage Line and the central portion is set back.

Front Façade: The Façade of a building that faces the Street. In the case of a corner lot, it is the Façade along the higher priority Street on the Street hierarchy.

Frontage: The area between a building Façade and the vehicular lanes or pedestrian-only Street, inclusive of its built and planted components.

Frontage Buildout: The minimum percentage of the lot width that must be occupied by a building Façade within the Build-To-Zone. For example, a property that is 100 feet wide with a the Frontage Buildout of 60% would require that at least 60 feet of Façade length be maintained in the Build-to-Zone. Any additional length of front Façade would be allowed to step back further from the Build-to-Zone, if desired. The intent of this requirement is to encourage development to maximize their front Façade exposure along the Street or Civic Open Space.

Frontage Element: The structural and architectural element that extend outward from the Façade of a building along Frontages, including awnings, canopies, galleries, porches and stoops, and that do not count as an extension of the Façade itself for the purposes of measuring Setbacks and Build-to-Zone.

Frontage Line: A Lot Line abutting a Street Right-of-Way.

G

Gallery: A covered passage that is open at one side, such as a portico or a colonnade. More specifically, it is a narrow balcony or platform running the length of a wall.

Garden Wall: A wall no greater than 48” in height that defines the Frontage Line and/or the perimeter of a property, dividing private areas from Streets, rear lanes, or adjacent lots.

Gas Station: A commercial enterprise established for the purpose of retail sale or supply to motor vehicles of fuel, lubrication, minor repairs to tires and minor accessories. Includes the customary space and facilities for the installation of such commodities on or in vehicles, but does not include space or facilities for storage, painting, repair, refinishing, body work, extensive mechanical work on or other servicing of motor vehicles.

H

Habitable Space: Space in a structure for living, sleeping, eating or cooking. Habitable space excludes parking garages, self-service storage facilities, warehouses, display windows separated from retail activity, bathrooms, toilet rooms, closets, halls, storage or utility spaces, and similar areas.

Home Occupation: Any for-profit activity carried out within, or on the same lot as, a residential dwelling unit, by a resident of such a dwelling unit.

Hostel: An overnight lodging facility for transient guests that provides communal or dormitory-style accommodations where transient residents can rent a bed, usually a bunk bed (as opposed to renting an entire unit, as in a hotel), and share a bathroom, lounge, and sometimes a kitchen. Rooms can mixed or single-sex, although private rooms may also be available.

L

Liner Building: A building specifically designed to mask a parking lot or a parking garage from a Frontage.

Live-Work Unit: Buildings or structures used jointly for commercial and residential purposes where the residential use of the space is secondary or accessory to the primary place of work. The commercial function may be anywhere in the unit. It is intended to be occupied by a business operator who lives in the same structure that contains the commercial activity or industry.

Lot: A parcel of land with specific boundaries, recorded as such in a deed or subdivision plat.

Lot Coverage: The portion of a Lot, expressed as a percentage, that may be occupied by a Principal Building and Accessory Structures, as well as sidewalks, patios, parking and loading areas, driveways, and other impermeable or man-made surfaces.

Lot Line: The lines abounding a Lot.

Lot Site Plan: A plan developed for the construction on an individual parcel within a platted subdivision within the Greeley Westside FBC.

Lot Width: The length of the Primary Frontage Line of a Lot.

M

Master Site Plan: A governing body-approved plan depicting the proposed development within the Greeley Westside FBC Area.

O

Open Space: Any land that is provided or preserved and open to the public for: (a) park or recreational purposes; (b) conservation of land or other natural resources; or (c) historic or scenic purposes.

P

Playground: A Civic Open Space designed and equipped for children's recreation.

Porch: An open air element of a building with a raised floor and a roof covering the floor that is supported by columns, posts, or piers. A porch may be located on more than one story.

Primary Civic Open Space: The main outdoor gathering place for a community. It is often, but not always, associated with an important Civic Building.

Primary Frontage: The Frontage along the Primary Frontage Line.

Principal Building: The main building on a Lot, usually located toward the Frontage, that contains the principal use or uses.

Principal Entrance: The main point of access for pedestrians into a building.

Property Line: (Syn: Lot Line)

Public Frontage: The area between the Vehicular Lanes and the Frontage Line.

Public Realm: The physical and social domain of the public that is held in common either by their physical presence or by visual association. This includes, but is not limited to, Plazas, Squares, Parks, Thoroughfares, Public Frontages, Private Frontages, Civic Buildings and Civic Open Spaces.

R

Rain Garden: A small or residential landscape feature with a slight depression used to slow, collect, infiltrate, and filter stormwater that is vegetated with plants. Can withstand moisture regimes ranging from flooded to dry.

Regulating Plan: A map that shows the physical locations and boundaries of Neighborhoods, primary Streets, and Open Spaces subject to regulation by this FBC.

Right-of-Way: A strip of land dedicated, deeded, used, or intended to be used, for a Street, alley, walkway, boulevard, railroad, drainage facility, access for ingress or egress, electric transmission line, oil and gas pipeline, sanitary and stormwater sewer line, or other purpose by the public, certain designated persons, or governing bodies. It is an appropriation of the land to some public use made by the owner and accepted for such use by the public.

S

Sign Band: An area on a building above the entrance(s) to tenant spaces that accommodates signage for each tenant.

Small Footprint Tower: A stand-alone structure that is significantly taller than it is wide, or a portion of a building that is significantly taller than it is wide and typically has more detail than the surrounding building(s). When a tower is a portion of a building, the tower eave or cornice is taller than the remainder of the building eave or cornice height, and one or more of the tower Façades is located forward of the remaining building Façade.

Special District: A zoning/transect category sometimes included in development codes that allows for more flexibility than the usual range of zoning/transect categories. A Special District category may be drafted by an applicant. The uses and regulations would need to adhere to the plan vision for the Greeley Westside Neighborhoods. Special Districts recognize that more uses are possible than the plan or code might envision and provide options.

Story: That part of a building contained between any floor and the floor or roof next above.

Street: A public or private Thoroughfare that affords the principal means of access to abutting property for use by motor vehicles, bicycles, and pedestrians. A Street may be for use by pedestrians only or prohibit motor vehicles.

Setbacks: The minimum distance a building Façade or parking area must be located from a frontage line or public right-of-way line.

Streetscreen: Sometimes called streetwall. A freestanding wall built along the frontage line, or coplanar with the Façade, often for the purpose of masking a parking lot from the Thoroughfare.

T

Terminated Vista: A building, structure, or portion of a building or structure specifically designed to visually attract a viewer’s attention at the end of a visual axis, i.e., to terminate a view. A Terminated Vista may include towers, corner towers, symmetrical Façades centered on a visual axis, an architecturally embellished entry, or similar distinctive architectural devices.

Thoroughfare: A way for use by vehicular and pedestrian traffic, or pedestrian traffic only, and for providing access to Lots and Open Spaces, consisting of Public Frontage and often Vehicular Lanes.

Trailhead: The point at which a trail begins. Trail heads often contain rest rooms, sign posts and distribution centers for informational brochures about the trail and its features, and parking areas for vehicles and trailers.

Transect (Transect Zone): A planning and zoning tool that organizes zones in a continuum from rural to urban, referred to as T1, T2, T3, T4, T5, and T6, where T1 is the most rural and T6 is the most urban. Each Transect zone has common characteristics that facilitate form-based regulation.

Tree Canopy Coverage: The percent of land area that is covered by the layer of leaves, branches, and stems of trees that cover the ground when viewed from above.

W

Warrant: A type of amendment that permits a practice that is not consistent with a specific provision of the Greeley Westside FBC but is justified by the practice’s intent. Warrants may be granted administratively by the City Administrator or Designee.

SECTION 8.2 ACRONYMS

For the purposes of this FBC, the following acronyms shall have the meanings set forth below:

ADA: Americans with Disabilities Act

BMP: Best Management Practices

BTZ: Build-to-Zone

CSA: Community Supported Agriculture

EV: Electric Vehicle

FBC: Form-Based Code

HVAC: Heating, Ventilation, and Air Conditioning

IDA: International Dark-Sky Association

LDR: Land Development Regulations

NACTO: National Association of Transportation Officials

PROWAG: Public Rights-of-Way Accessibility Guidelines.

TND: Traditional Neighborhood Design



Westside Area Plan & Code

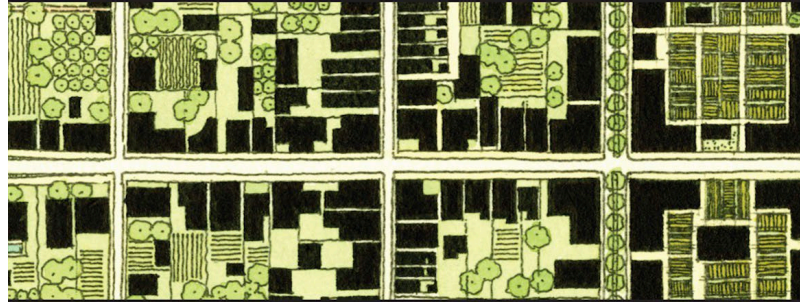
**SPECK
DEMPSEY**

able.city

HOW DO YOU MAKE A REGIONAL PLAN?



The Smart Growth Manual



From the authors of *Suburban Nation*

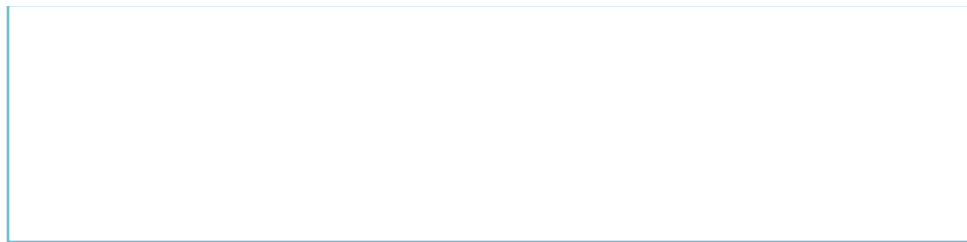
Andres Duany
and Jeff Speck
with Mike Lydon

1.2 A Regional Plan

Think globally, act locally, but plan regionally.



The Louisiana Speaks Regional Plan designates transportation corridors, intended growth sectors, and protected open space.



2.2 Map the Rural Preserve

Identify the land truly protected from development.









The regional plan next maps the natural areas that are legally protected.

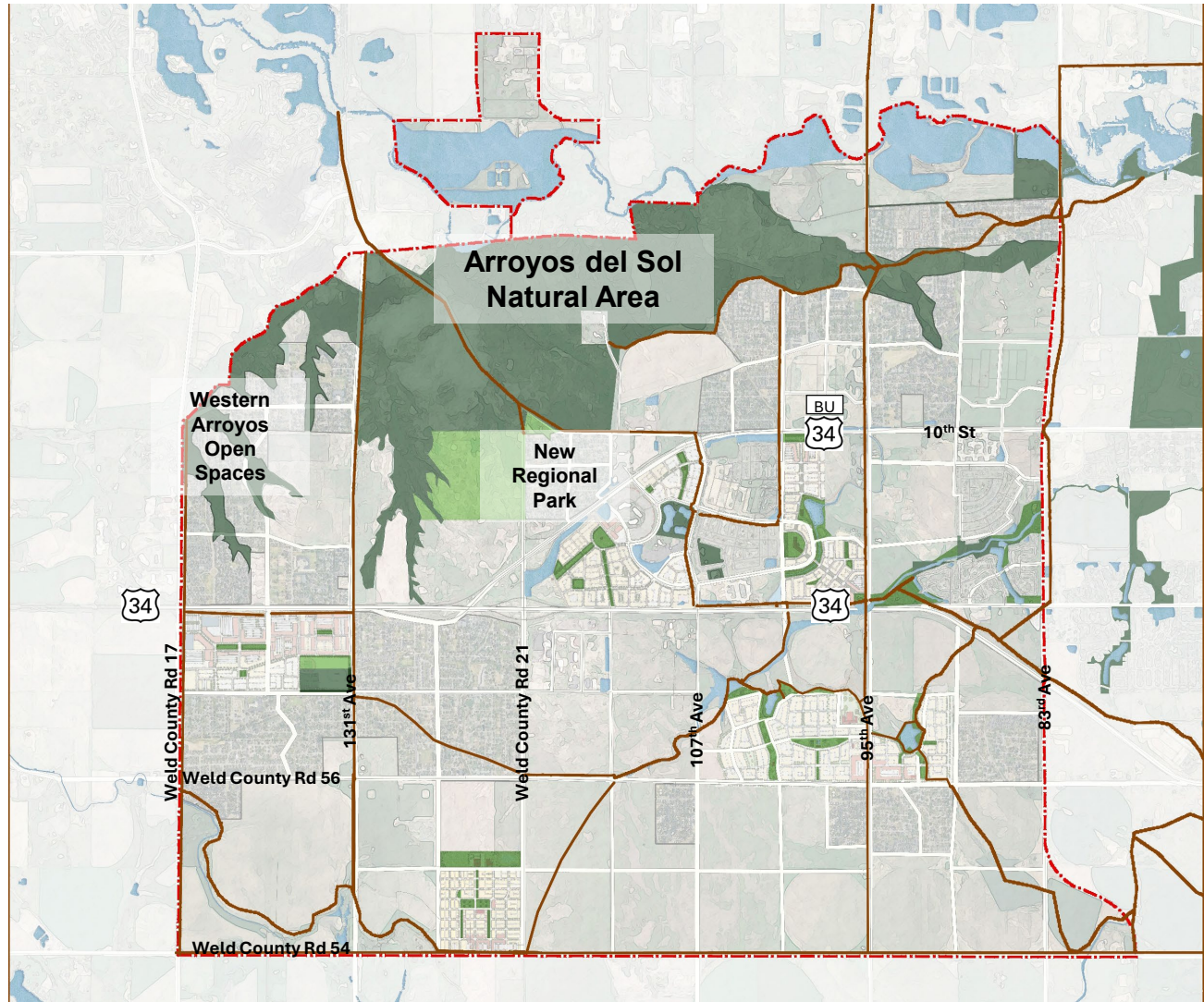


WEST GREELEY REGIONAL PLAN 2050

GREENPRINT

Legend

-  Westside Study Area
-  Existing Waterways
-  Existing Parks and Natural Areas
-  Proposed Regional Park
-  Proposed Neighborhood Parks
-  Existing and Proposed Trails



2.7 Map the Corridors

Identify major natural and man-made corridors.



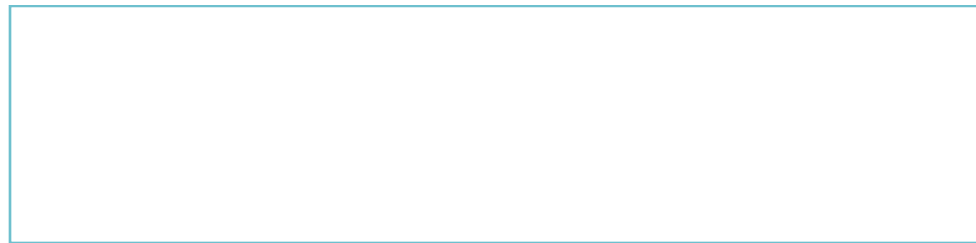
Boston, MA: This corridor plan includes mixed-income, mixed-use development around existing and proposed transit stations at regional centers.

7.3 Connections Beyond

Connect neighborhoods to adjacent roads and sites.

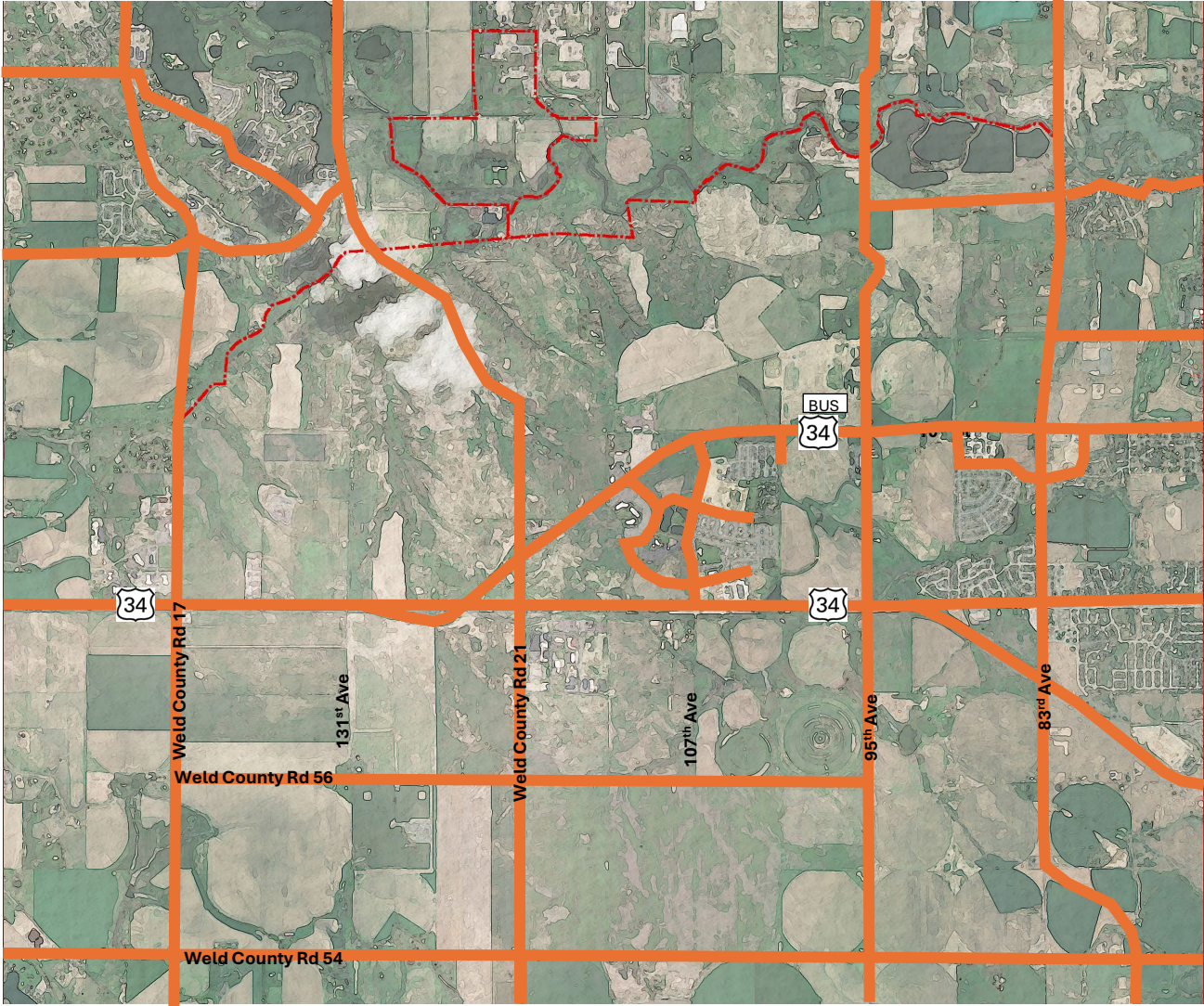


Gaithersburg, MD: When designed, Kentlands (left) kept connections open for future development. Lakelands (right) took the cue.



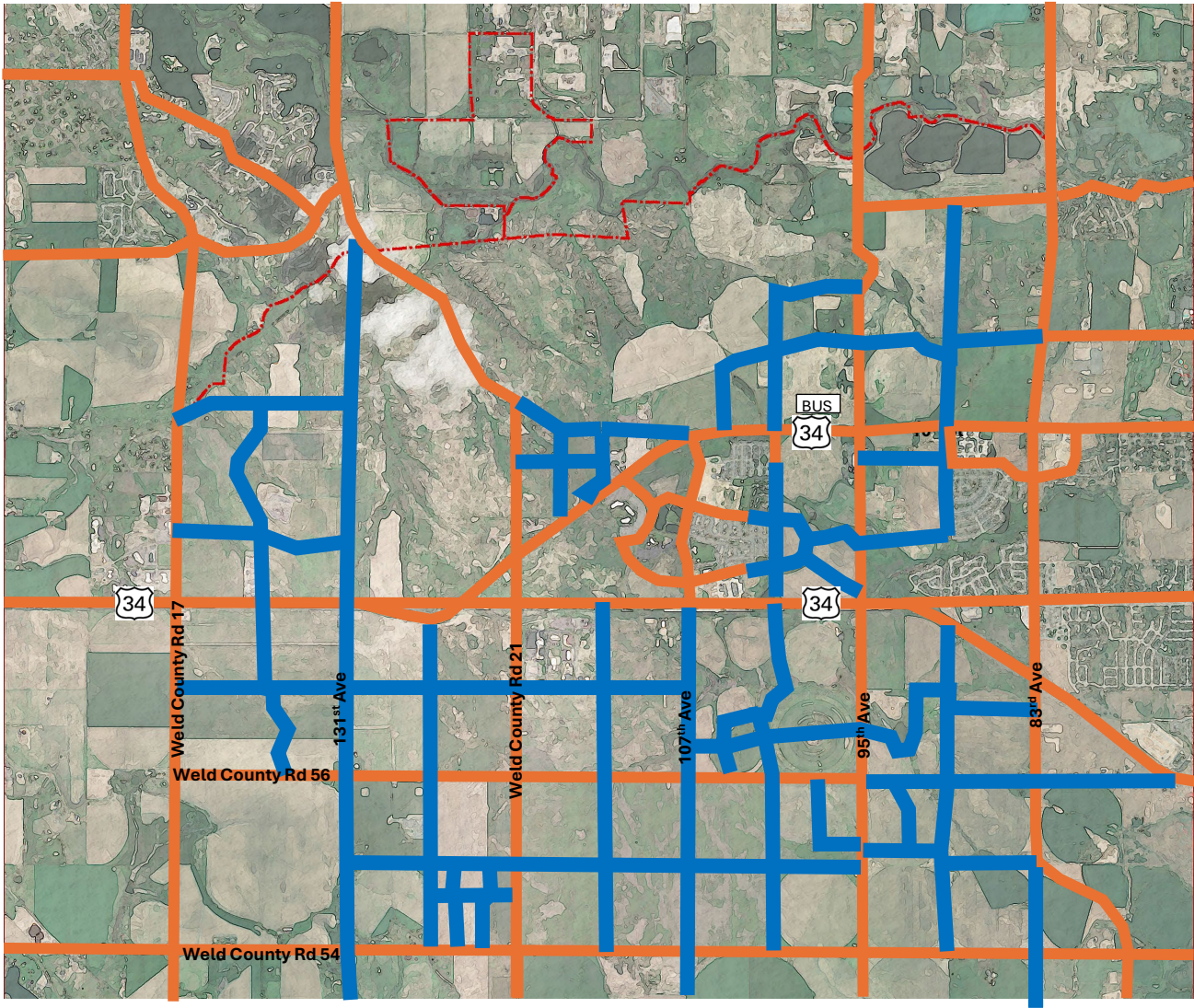
CONNECTING STREET NETWORK EXISTING

 Existing Arterial and
Collector Streets



CONNECTING STREET NETWORK PROPOSED

- Existing Arterial and Collector Streets
- Proposed Arterial and Collector Streets



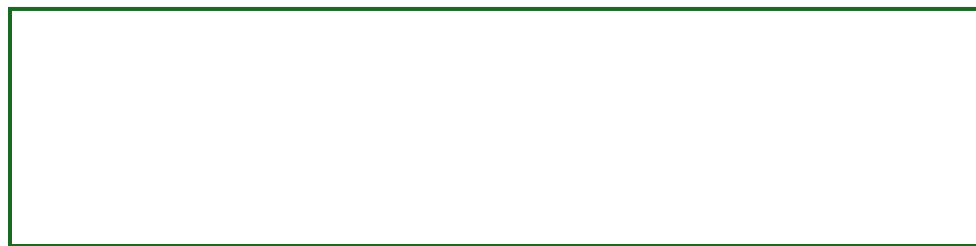
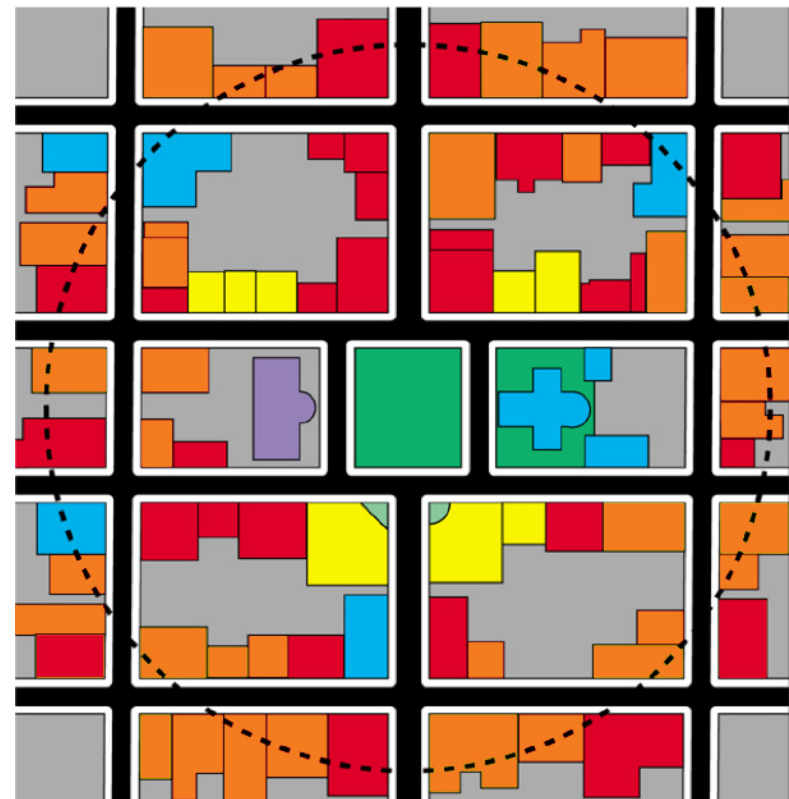
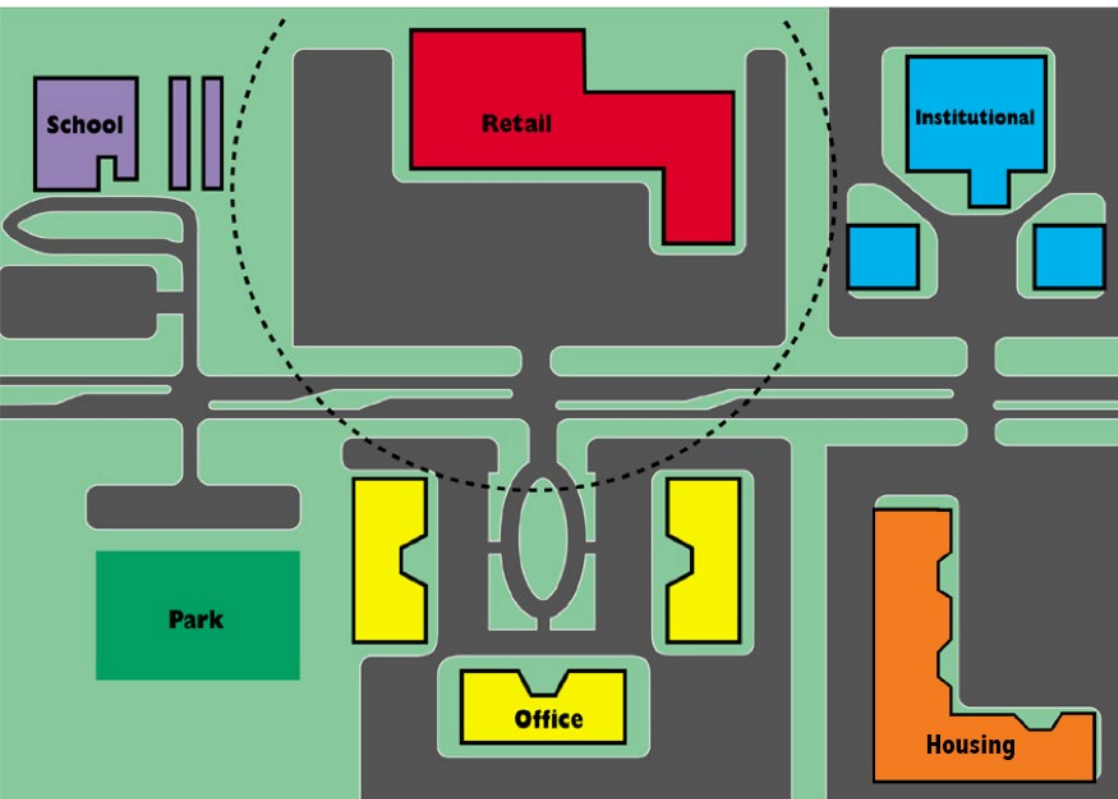
1.5 The Neighborhood

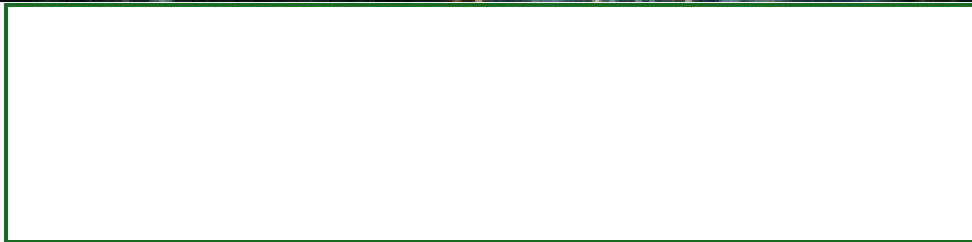
Plan in increments of complete neighborhoods.



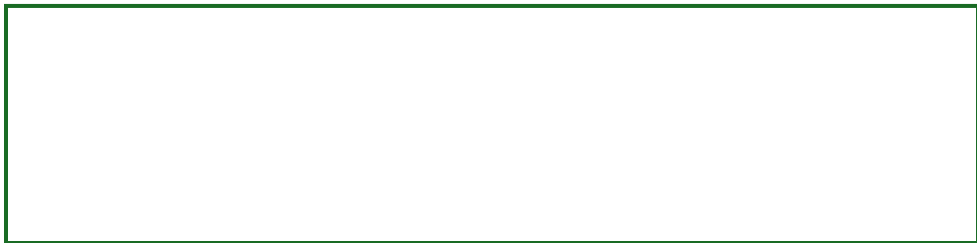
A neighborhood puts most daily needs, including transit, within a short walk.

















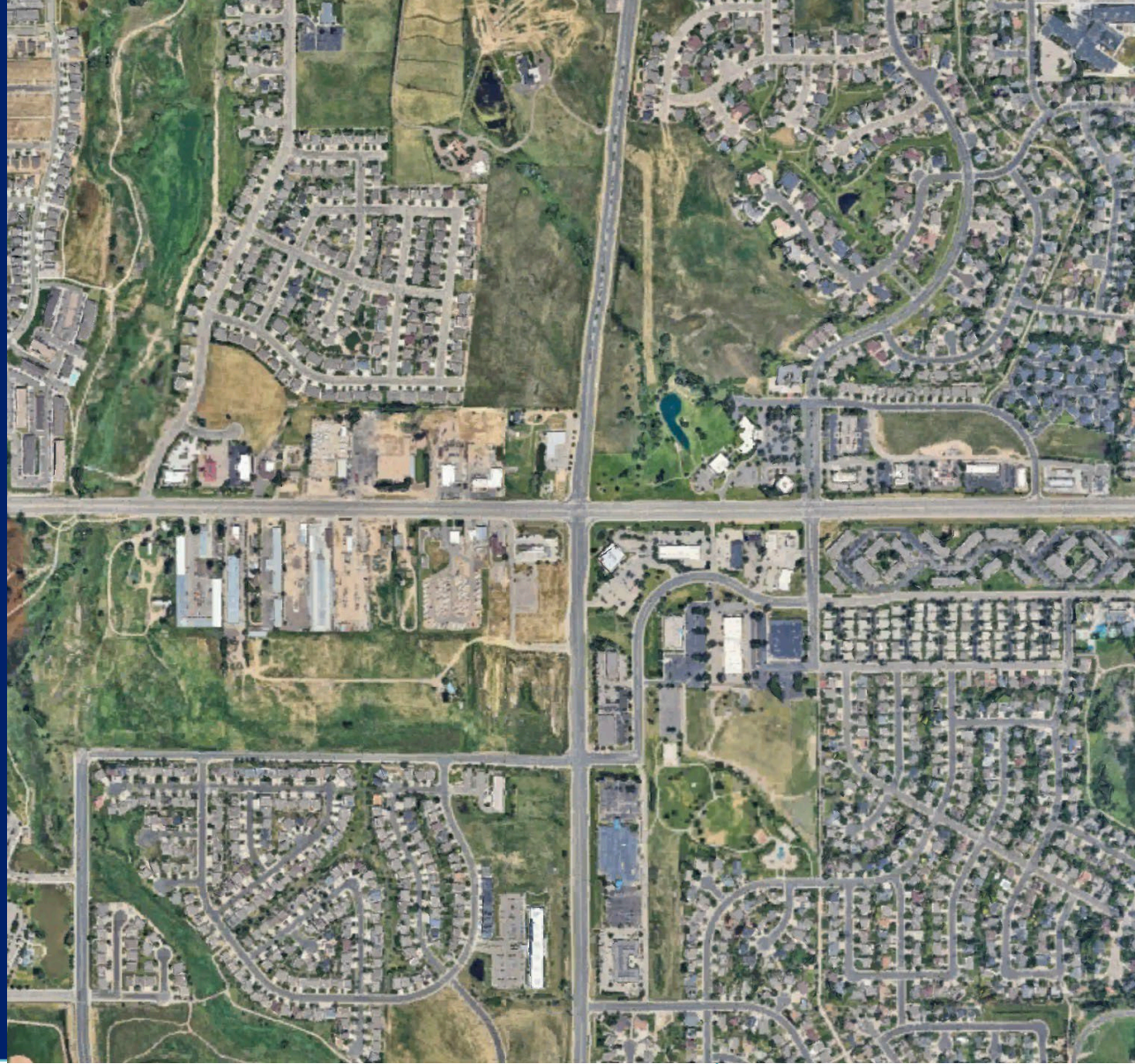


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GREELEY



GREELEY



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BIRDS EYE VIEW OF
GREELEY, COLO.
 COUNTY SEAT OF WELD CO.

1882.

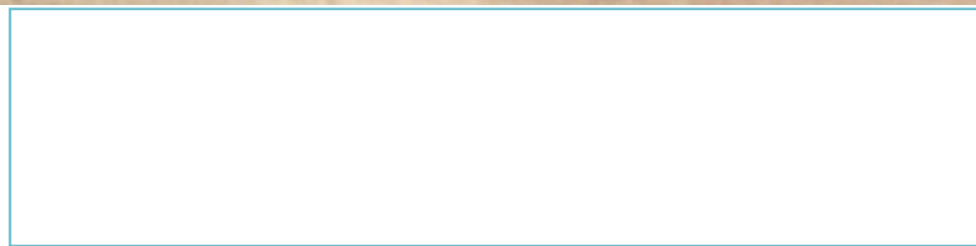
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- Smith & Fry, Blacksmith and Wagon Shop, Walnut St.
- Camp & Neuhann, Brick Contractors.

- L. Jackson House.
- M. Greeley City Mills, R. F. Johnson.
- N. Garden City Mills, J. E. Graham & Co.
- O. Elevator, J. L. Ewing & Co.
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- S. Lumber Yard, J. K. Thompson & Bro.
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- Joseph Moses, Coal Dealer, Maple St.
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- F. H. Groves, Contractor and Builder, Walnut St.
- Jos. Moss, Contractor and Builder, Grant Ave. Cor. Linden St.
- Theos. Robbins, House and Sign Painting, Kalsomining and Paper Hanging, Walnut St.
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8575. Opera House Block, Greeley, Colo.



BRADBURN VILLAGE

ESTABLISHED 2002

CONTINUUM PARTNERS, LLC
Neighborhood Founders

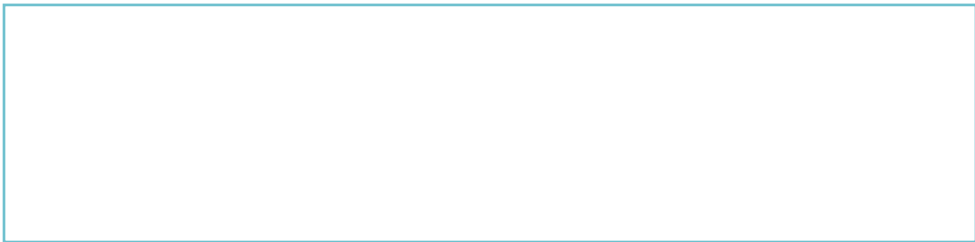
DUANY PLATER ZYBERK/CIVITAS
Planners

Bradburn Village was conceived and built by a team of individuals and enterprises committed to the belief that our built environment should reflect the highest civic aspirations we have for our society. The patterns of growth dominating our region at the time were both ecologically unsustainable and increasingly isolating people from the daily interactions around which community cohesion depends.

The Continuum Partners Team built Bradburn to serve as a new model for urban form in the Denver region.



m. Partners, LLC







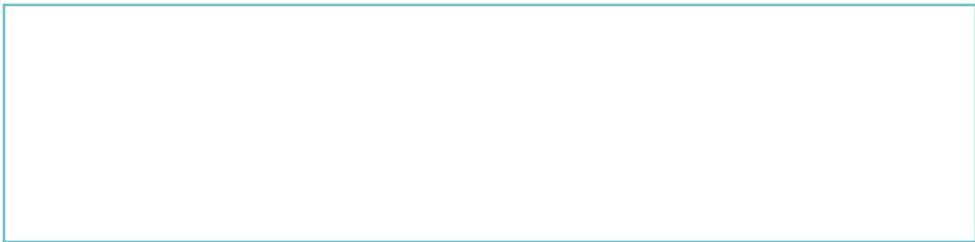








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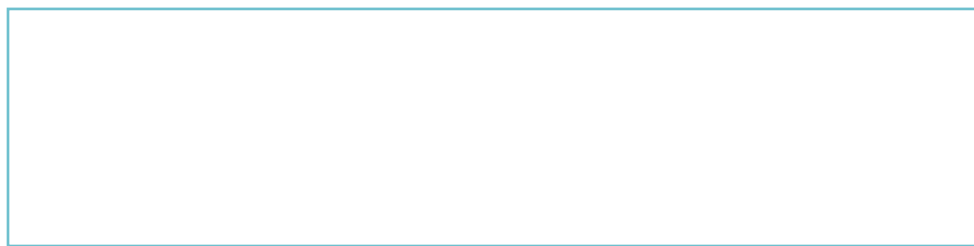
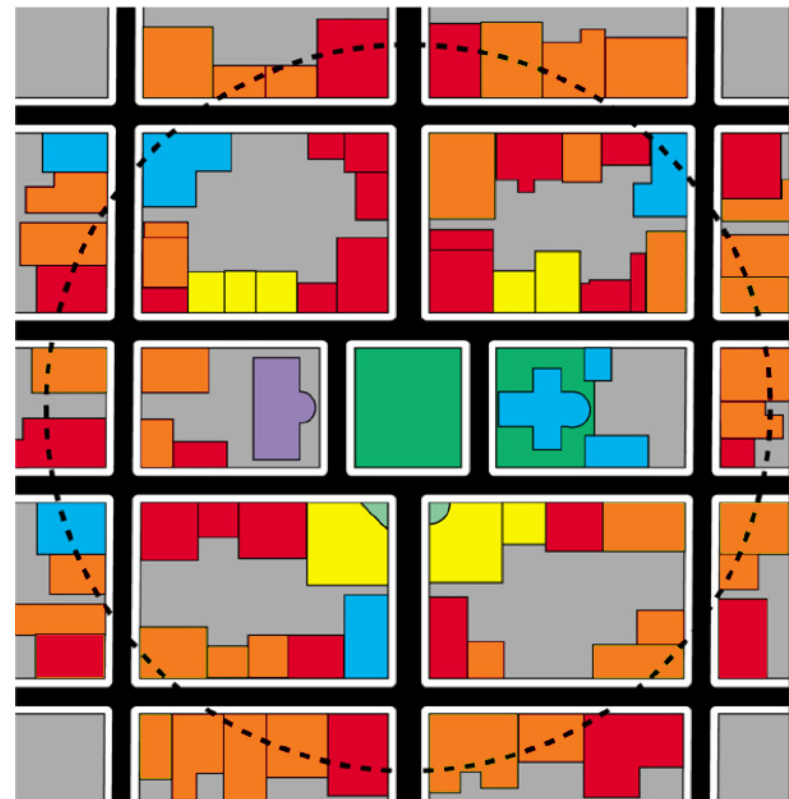
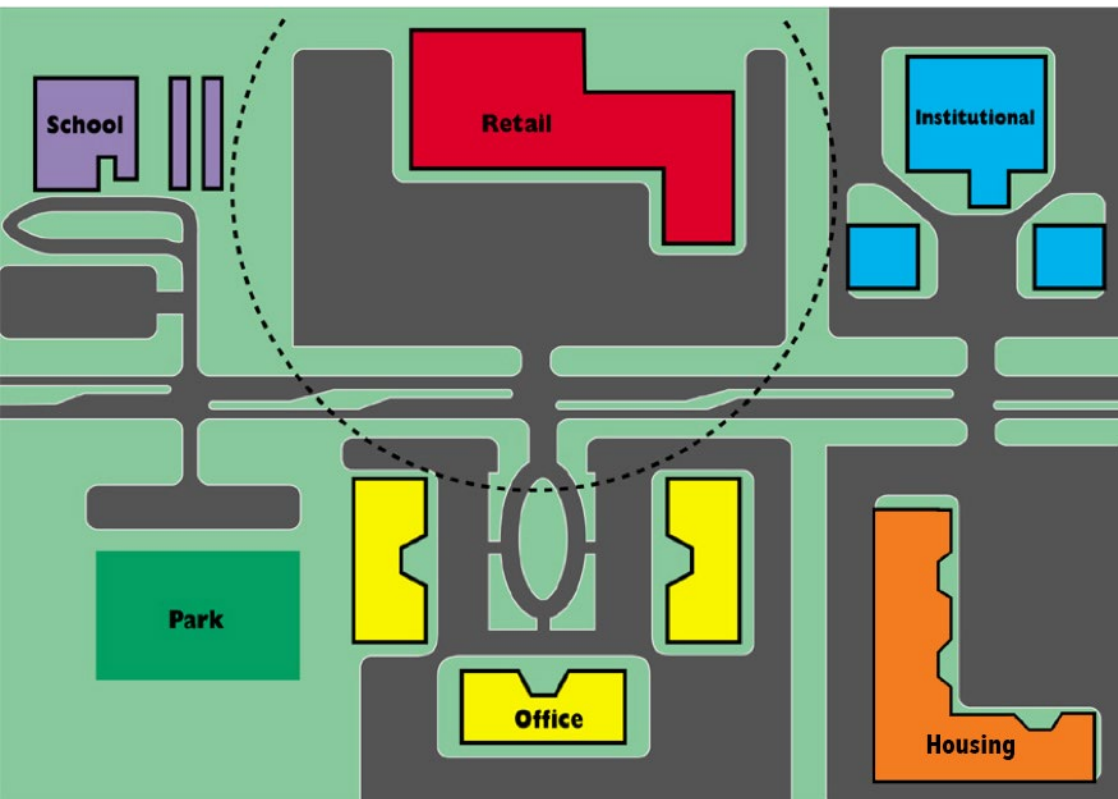












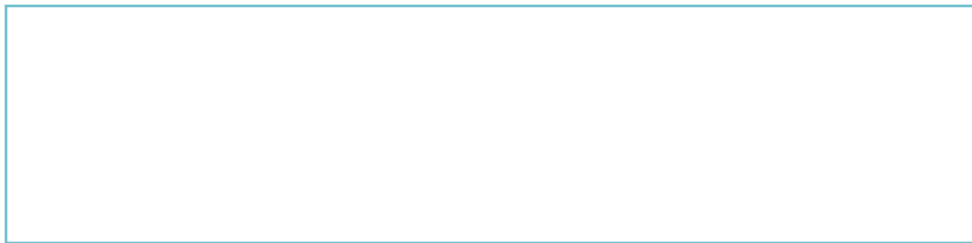


WESTMINSTER



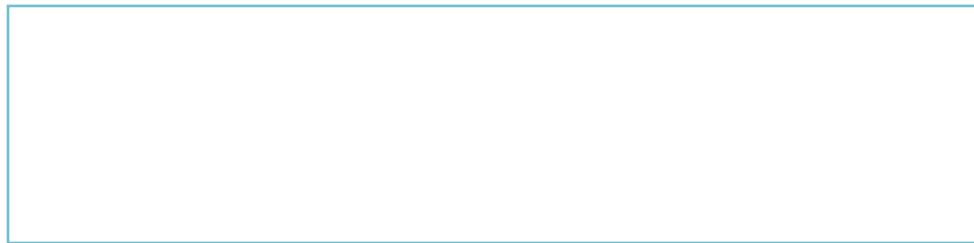


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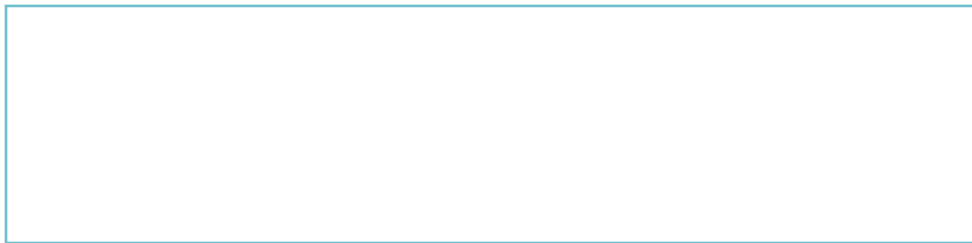


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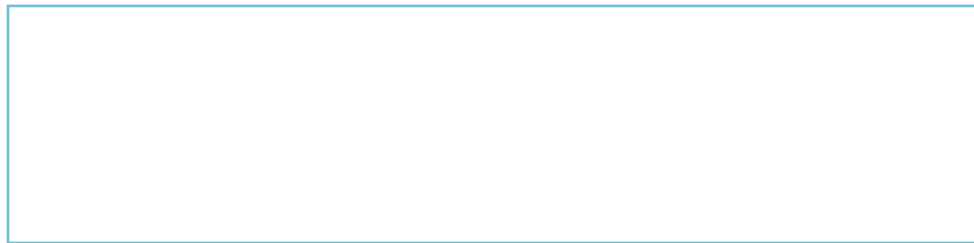


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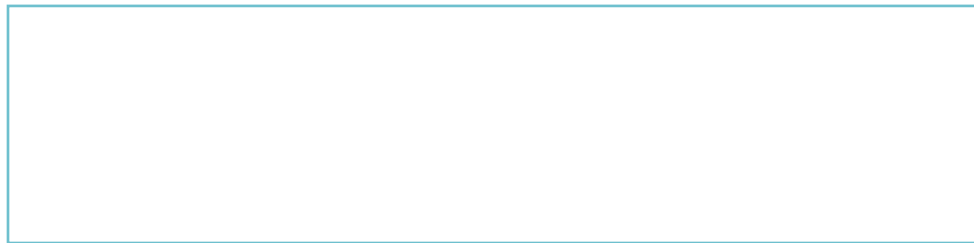


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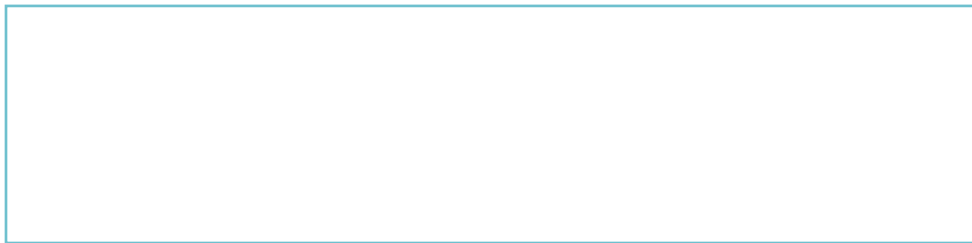


BRADBURN



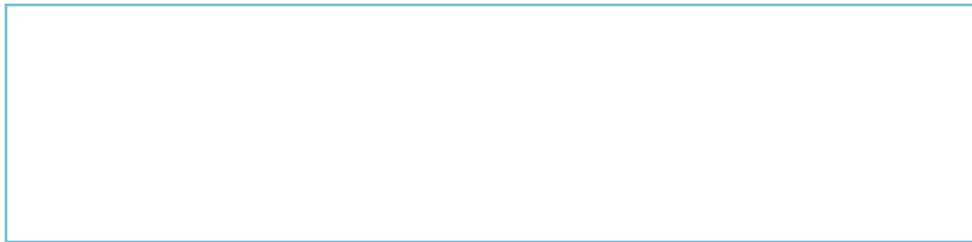


BROOMFIELD





BRADBURN





WESTMINSTER



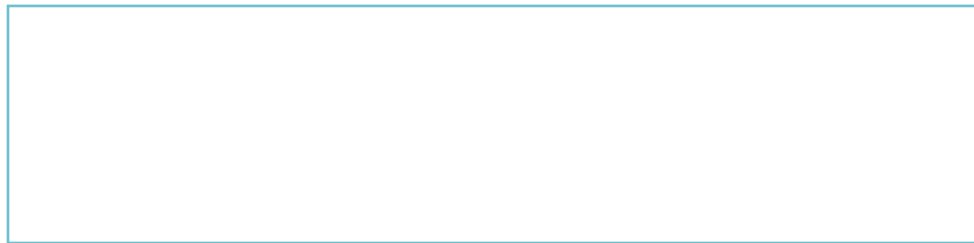


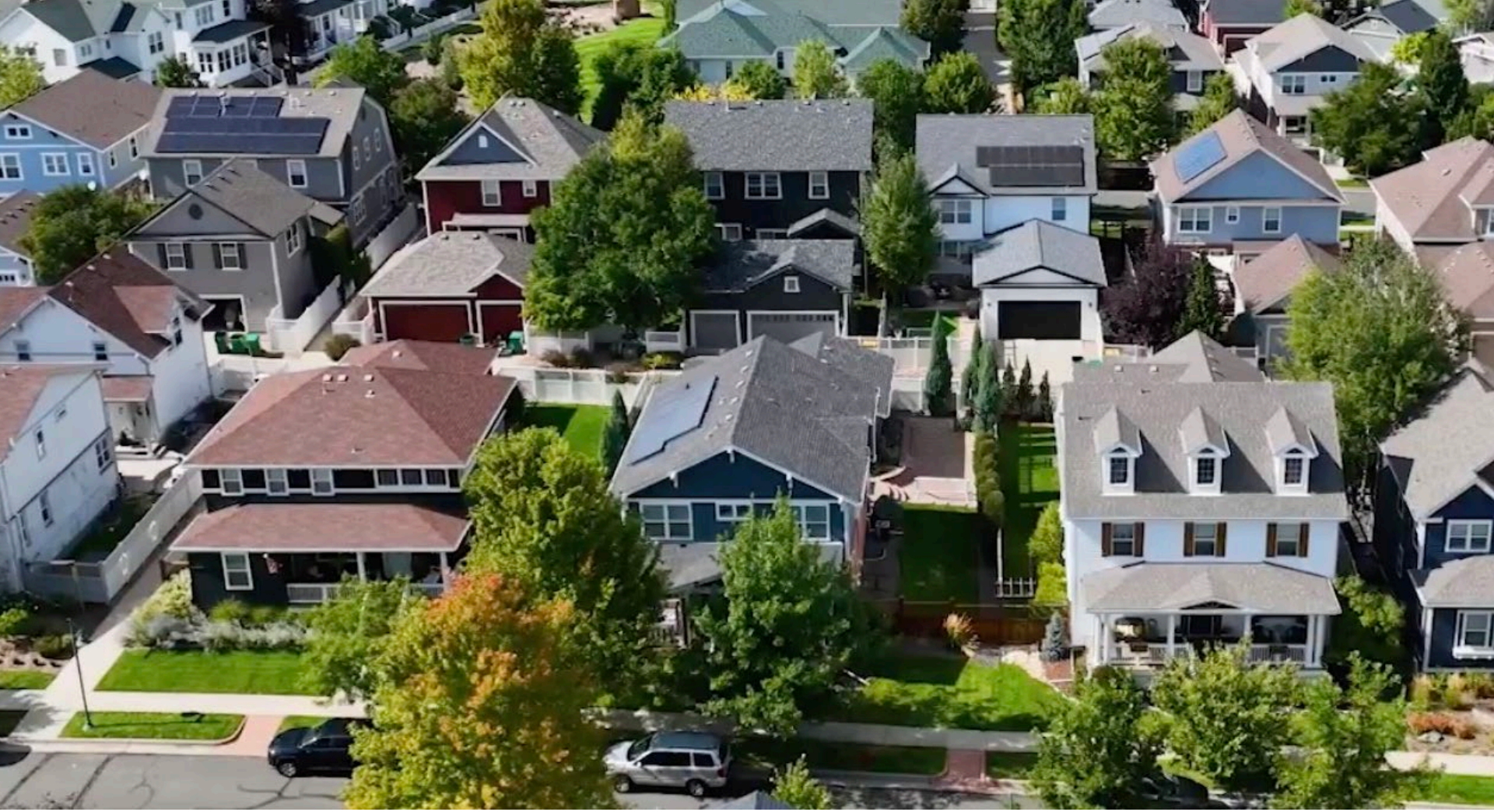
BRADBURN





WESTMINSTER





BRADBURN



1.5 The Neighborhood

Plan in increments of complete neighborhoods.



A neighborhood puts most daily needs, including transit, within a short walk.



Westside Area Plan
GREELEY, COLORADO



December 2025 DRAFT

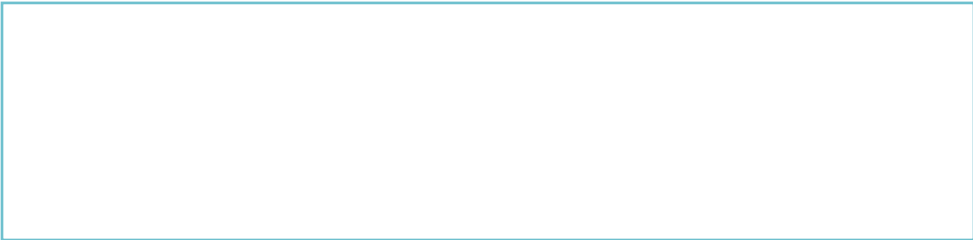


Greeley, CO
**GREELEY WESTSIDE
FORM-BASED CODE**

(Draft)



**AVAILABLE @
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WEST GREELEY

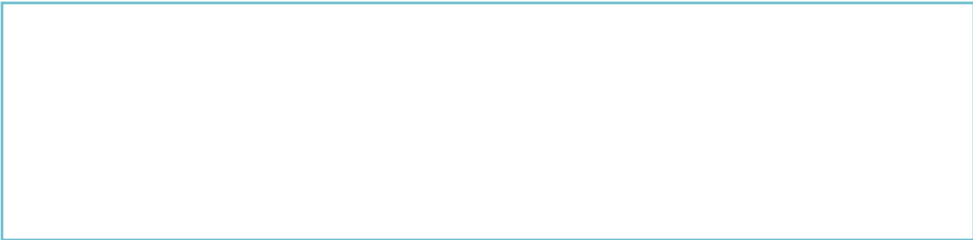
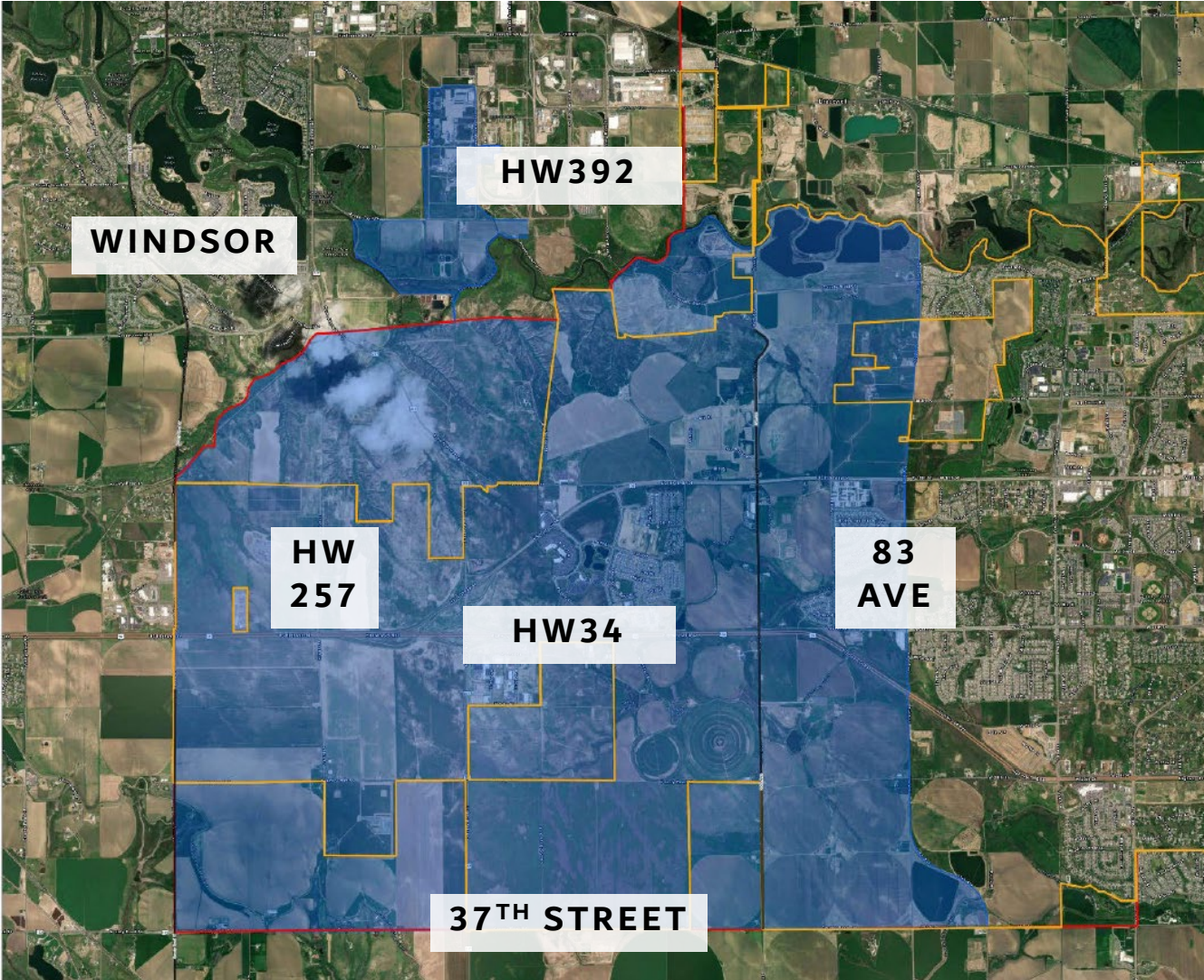
22 SQUARE MILES

5 MILES FROM
DOWNTOWN

- NEIGHBORHOODS
- INDUSTRIAL AREAS
- COMMERCIAL AREAS
- AGRICULTURAL LAND
- RIVER CORRIDORS
- NATURAL AREAS

Legend

-  Greeley City Limits
-  Long-range expected growth area (LREGA)



PROJECT TIMELINE

TASK 1: ANALYSIS

PROJECT
KICK-OFF
MAY 6 - 7

ANALYSIS
MAY-JUNE

TASKS 2-3: VISIONING

ORIENTATION
& VISIONING
WORKSHOP
JULY 7- 11

ALTERNATIVES
DEVELOPMENT
JULY - AUG

TASKS 4-6: PLAN & CODE REFINEMENT

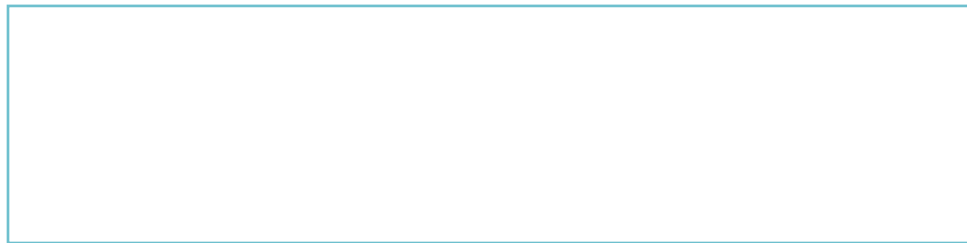
PRESENTATION OF
THE PLAN & CODE
DRAFT ELEMENTS
FALL

REFINE PLAN
& CODE
LATE FALL

TASKS 7-8: FINAL PLAN & CODE

PRESENTATION OF
COMPLETE DRAFT
PLAN & CODE
JAN/FEB 2026

FINAL PLAN & CODE
PRESENTATION
SPRING/SUMMER 2026



A Talk with Jeff Speck & Community Visioning for West Greeley

Wednesday, July 9, 2025, 6 – 8pm AIMS WELCOME CENTER

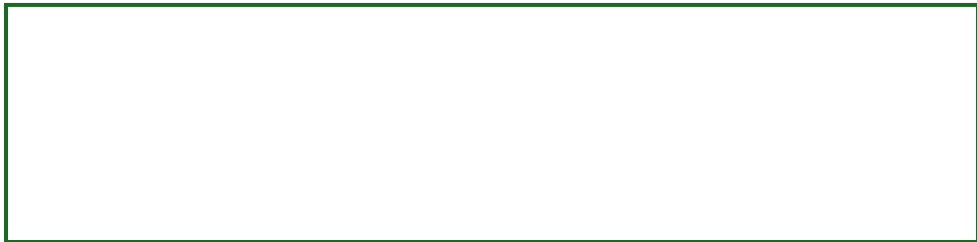


200+ ATTENDEES





200+ ATTENDEES





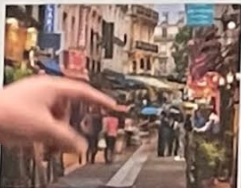
STREETS WHAT IS APPROPRIATE FOR YOUR AREA?



3 Green dots



3 Green dots



3 Green dots



1 Green dot



3 Green dots



1 Green dot

Please FOLK (YOU) vote on the photos you believe are appropriate for this part of the City.
You can vote ON or OFF the photos. You may vote as many times as you wish for the City.
You will see a tally of votes for each photo.

3 Green dots = More of this
1 Red dot = Not as much

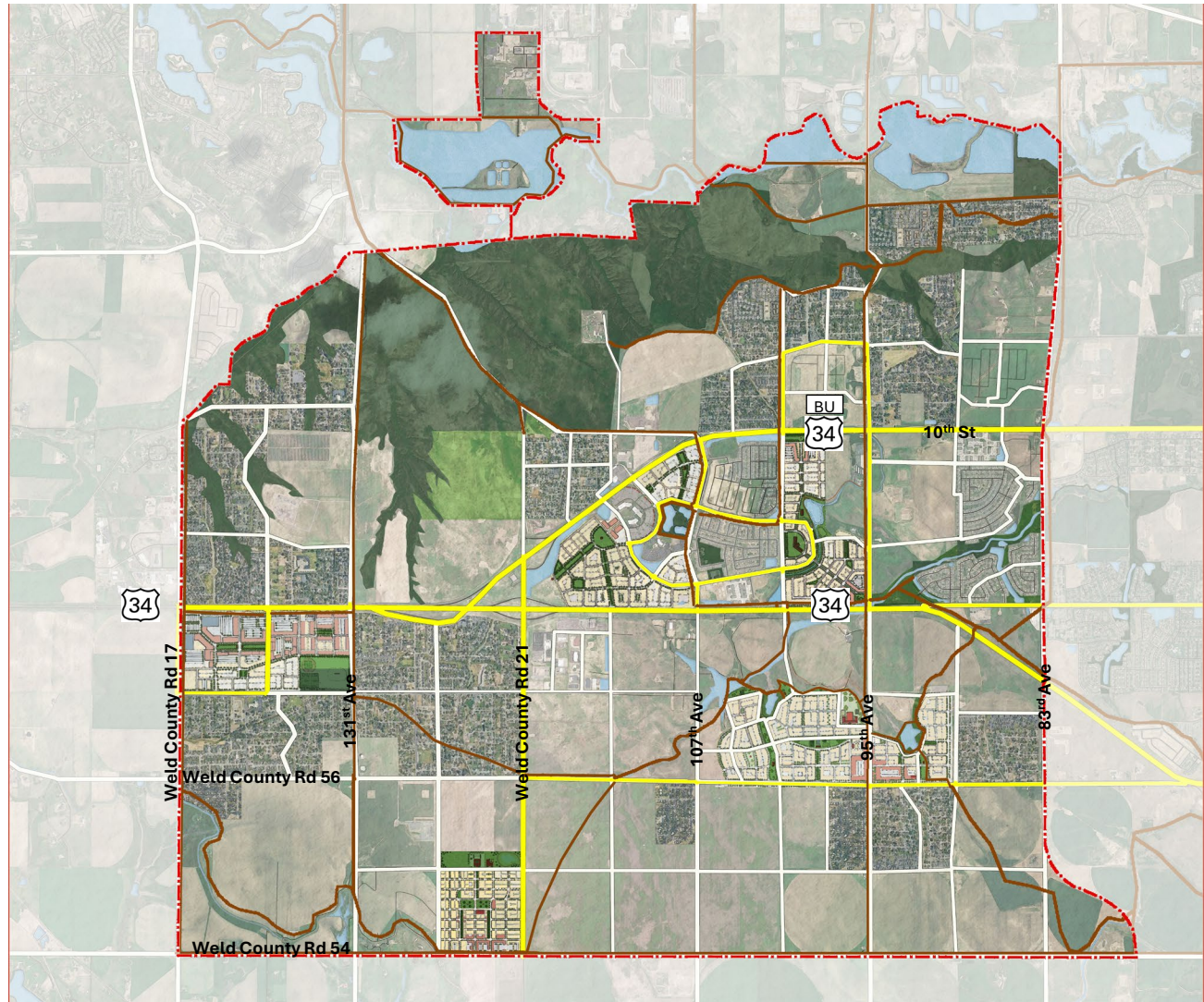
200+ ATTENDEES



WEST GREELEY REGIONAL PLAN TOMORROW (2050)

Legend









- Westside Study Area
- Existing Waterways
- Existing Parks and Natural Areas
- Proposed Regional Park
- Existing and Proposed Connecting Roads
- Existing and Proposed Trails
- Proposed Transit Corridors
- Proposed New Development Area
- Proposed Example Placetypes

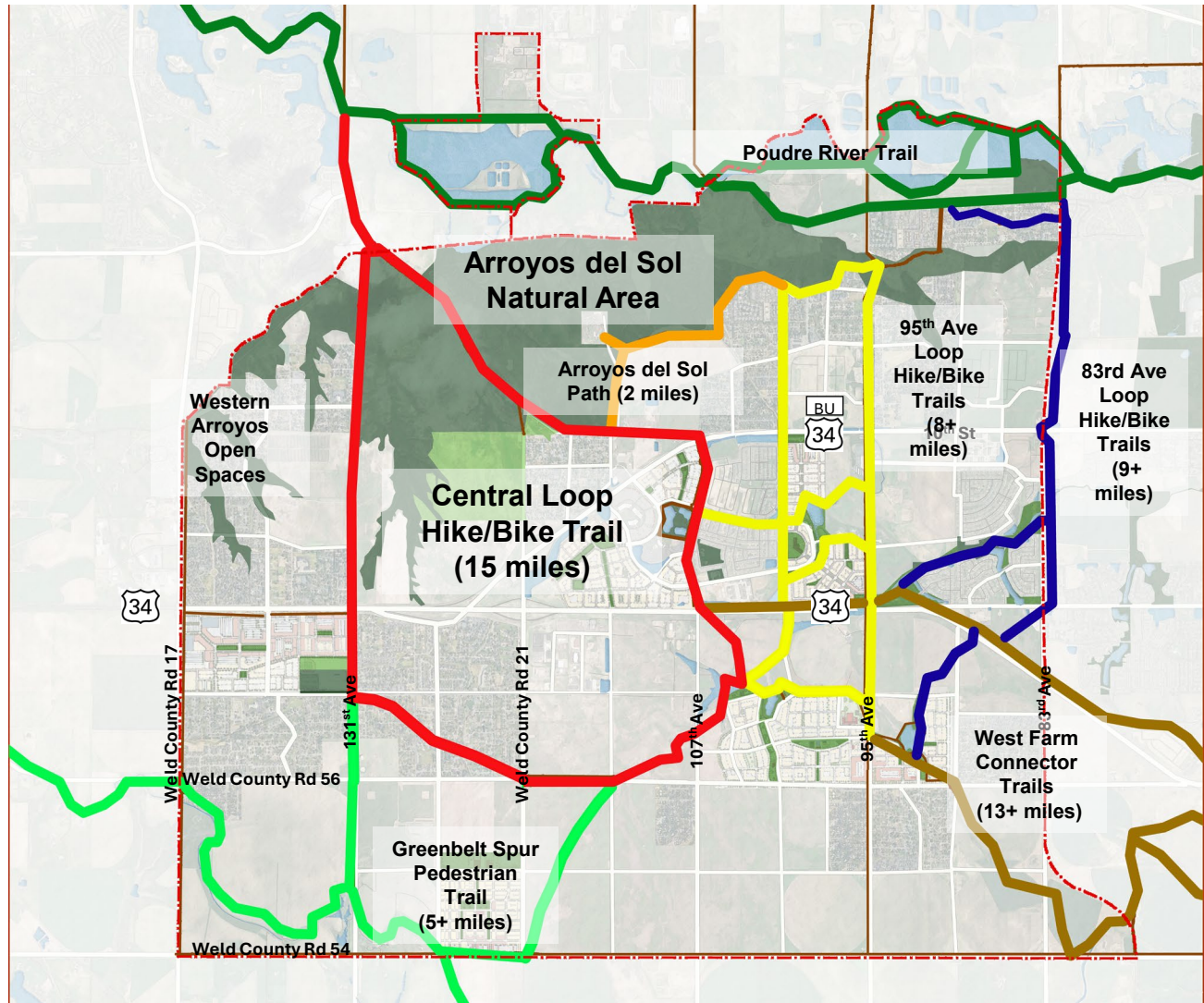


WEST GREELEY REGIONAL PLAN 2050

MAJOR PARKS AND TRAILS

Legend



-  Westside Study Area
-  Poudre River Trail (existing)
-  Central Loop (15 miles)
-  Arroyos del Sol Path (2 miles)
-  95th Ave Loop (8+ miles)
-  83rd Ave Trail (9+ miles)
-  West Farm Connector Trails (13+ miles)
-  Greenbelt Spur Pedestrian Trail (5+ miles)

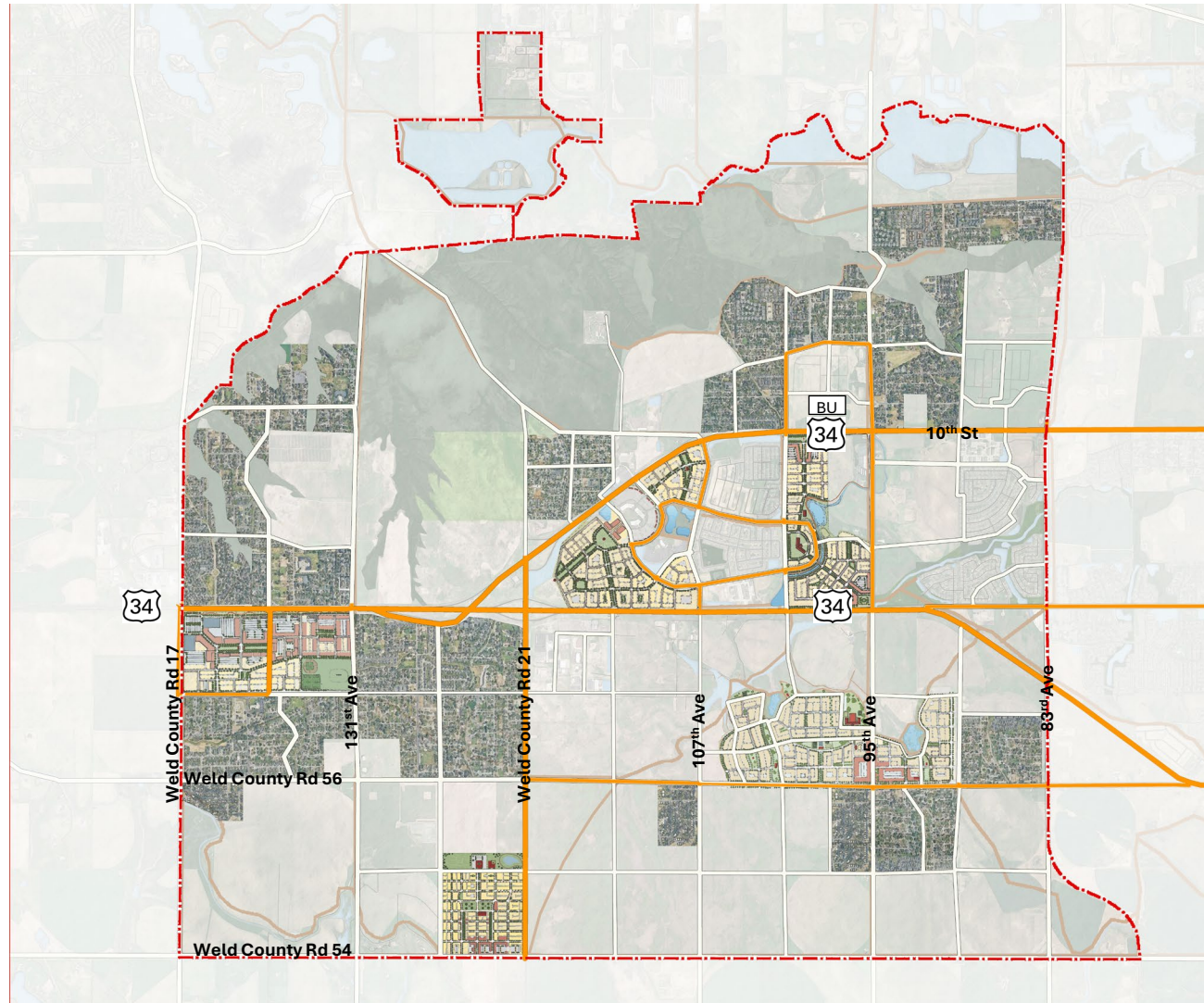


WEST GREELEY REGIONAL PLAN 2050

TRANSIT

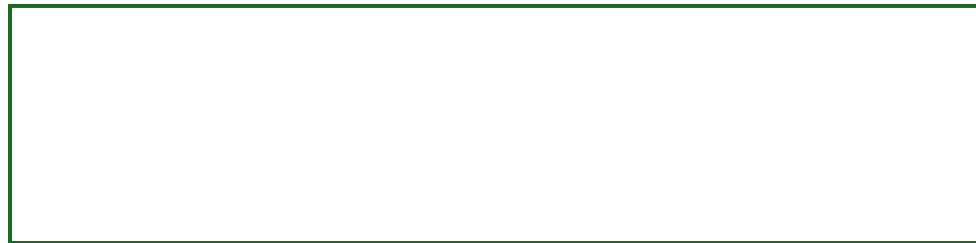
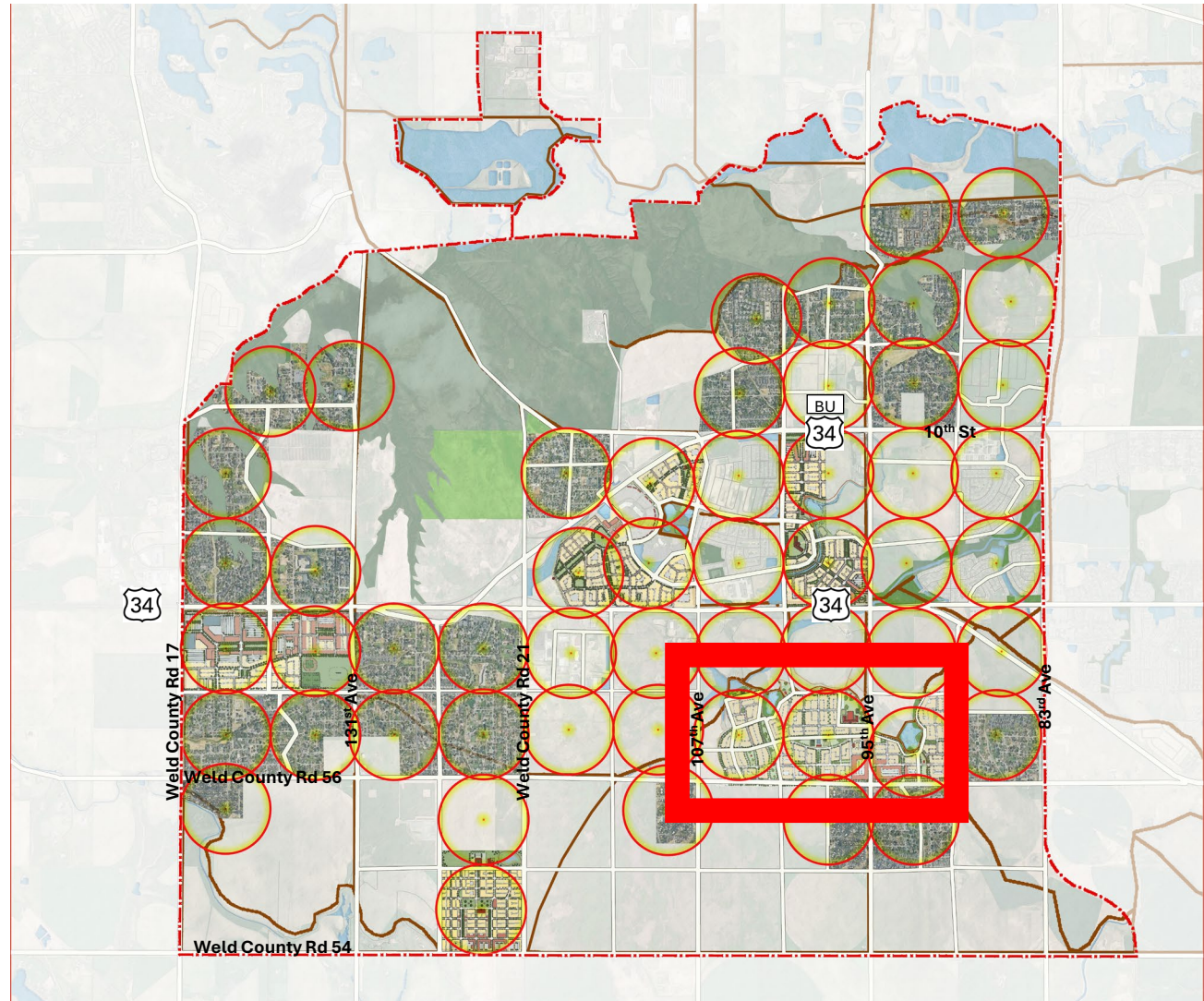
Legend

-  Westside Study Area
-  Proposed Transit Corridors



WEST GREELEY REGIONAL PLAN 2050

CITY OF NEIGHBORHOODS



TRANSECT PLANS

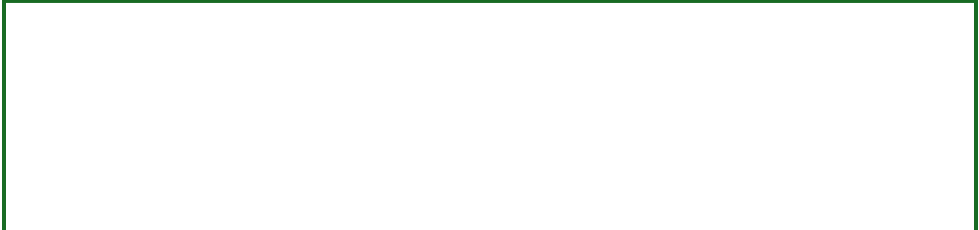
- T3: Suburban
- T4: General Urban
- T5: Urban Center
- T6: Urban Core
- Civic



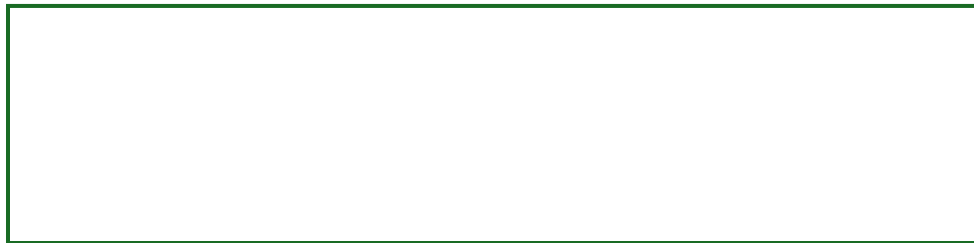
Weld County Road 56

95th Ave

LOCAL MAIN STREETS



WORKPLACES



NEIGHBORHOOD SCHOOLS



OUTDOOR DINING



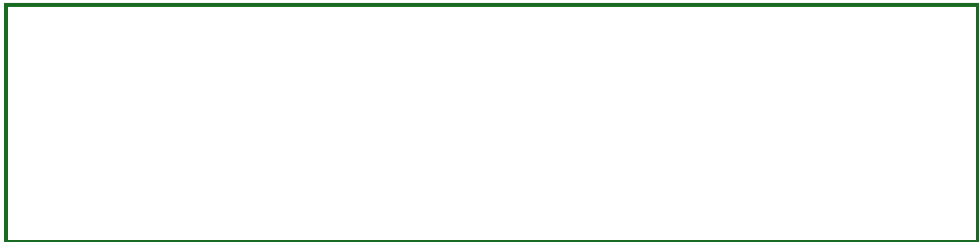
SMALL HOMES



TOWNHOMES



LARGE HOMES



REGIONAL CENTERS



TRANSIT-SUPPORTIVE DENSITIES





Westside Area Plan & Code



**SPECK
DEMPSEY**

able.city